

# HATFIELD BOROUGH COUNCIL

## REGULAR MEETING

**November 20, 2024**



**JASON FERGUSON, PRESIDENT**

**RICHARD GIRARD, VICE PRESIDENT**

**LARRY BURNS, COUNCILMEMBER**

**JAMES FAGAN, COUNCILMEMBER**

**MICHELLE KROESSER, COUNCILMEMBER**

**MARY ANNE GIRARD, MAYOR**

**MALACHI NISBETT, JUNIOR COUNCIL PERSON**

**JAIME E. SNYDER, BOROUGH MANAGER**

**CATHERINE M. HARPER, BOROUGH SOLICITOR**



# Borough of Hatfield

Montgomery County, Pennsylvania

## BOROUGH COUNCIL REGULAR MEETING

November 20, 2024

7:00PM

### AGENDA

CALL TO ORDER  
ROLL CALL  
PLEDGE OF ALLEGIANCE  
INVOCATION

1. APPROVAL OF MEETING AGENDA:

Motion to Approve the Agenda of the November 20, 2024 Regular Meeting

2. APPROVAL OF THE MINUTES:

Motion to Approve the Minutes of the October 2, 2024 Workshop Meeting and October 16, 2024 Regular Meeting

3. PUBLIC INPUT:

Please rise, state your name and address and the reason for addressing Council

4. ANNOUNCEMENTS:

- Next Council Meetings December 4<sup>th</sup> Workshop December 18<sup>th</sup> Regular Meeting at 7:00PM in Council Chambers
- Planning Commission is Scheduled to Meet on Monday, December 16, 2024, at 6:00PM in Council Chambers
- HEROC is Scheduled to Meet Wednesday, December 18, 2024, at 8:00AM in Council Chambers
- The Hatfield Borough Offices will be closed on Thursday and Friday November 28<sup>th</sup> and 29<sup>th</sup> in Observance of the Thanksgiving Day Holiday
- Leaf Bag Collection Starts Monday, October 28, 2024. Will Continue Every Monday Through December 2, 2024

401 S. Main Street  
P.O. Box 190  
Hatfield, PA 19440

**Phone:**  
215-855-0781

**Fax:**  
215-855-2075

**Email:**  
admin@  
hatfieldborough.com

**Website:**  
www.hatfieldborough.com

5. 23 N Main Street, Hatfield Walk, Land Development Presentation

6. NEW BUSINESS / DISCUSSION ITEMS:

- A. 2025 Preliminary Budget Update Discussion
- B. Ordinance No. 557 Rental Inspection Program
- C. Updating the Consolidated Fee Schedule
- D. Payment Request No. 1 Utility Replacement Project
- E. SEPTA Sub-License Agreement Bard & Jester Brewery

7. OLD BUSINESS:

- A. Ordinance No. 556 Updating the International Property Maintenance Code from 2003 to 2021
- B. Sitework Escrow Release No. 1 Lennar (Bennetts Court)
- C. 2025 Proposed Meeting Dates

8. ACTION ITEMS:

- A. Motion to Consider Sitework Escrow Release No. 1 Lennar (Bennetts Court) in the Amount of \$814,128.77 (eight hundred fourteen thousand one hundred twenty-eight dollars and seventy-seven cents)
- B. Motion to Consider Advertising Ordinance No. 557 Enacting a Rental Inspection Program in Hatfield Borough for a Public Hearing to be held on December 4, 2024 at 7:00PM in Council Chambers
- C. Motion to Consider Payment Request No. 1 for the Utility Replacement Project to KBC Construction Inc. in the Amount of \$291,685.50 (two hundred ninety-one thousand six hundred eighty-five dollars and fifty cents)

9. Motion to Approve Payment of the Bills

10. MOTION to ADJOURN: EXECUTIVE SESSION

## **2. APPROVAL OF THE MINUTES:**

**Motion to Approve the Minutes of the  
October 2, 2024 Workshop Meeting and  
October 16, 2024 Regular Meeting**



**HATFIELD BOROUGH COUNCIL  
WORKSHOP MEETING  
October 2, 2024**

**MINUTES**

**THIS MEETING WAS HELD IN-PERSON & LIVE STREAMED  
BOROUGH HALL 401 S. MAIN STREET, HATFIELD  
THIS MEETING WAS RECORDED**

**CALL TO ORDER AND ROLL CALL:**

**ROLL CALL**

(X) Jason Ferguson, President  
(X) Richard Girard, Vice President  
(X) Larry Burns  
(X) James Fagan  
(X) Michelle Kroesser

(X) Mayor Mary Anne Girard

The record shows that five members of Council were present at roll call, as well as, Solicitor; Catherine Harper, Timoney Knox, LLP, Borough Manager; Jaime E. Snyder, Public Works Director; Stephen S. Fickert, Jr, Junior Council Person; Malachi Nisbett and Assistant Manager; Kathryn Vlahos.

**1. Motion to Approve the October 2, 2024 Workshop Meeting Agenda.**

Motion: A motion was made by Councilmember Kroesser for Approval of Meeting Agenda, October 2, 2024 Workshop Meeting Agenda. The motion was seconded by Councilmember Burns and unanimously approved with a vote of 5-0.

**2. PUBLIC INPUT:** President Ferguson asked if there was any Public Input. There was no media present.

Robert Boyer from 462 Edgewood Drive wanted came to Borough to discuss the children that are riding around on the bikes in the borough that jumping in front of cars while they are riding. His other concern is that people are not stopping at the stop signs Towamencin and Butler. Mayor Girard responded that she did receive a complaint about this area and she talked to the police and they were going to patrol that area more, she will talk about this concern at the next public safety committee meeting.

**3. ANNOUNCEMENTS: Manager Jaime E. Snyder made the following announcements.**

- Next Council Meeting October 16<sup>th</sup> Regular Meeting at 7:00PM in Council Chambers
- Planning Commission is Scheduled to Meet on Monday, October 28, 2024, at 6:00PM in Council Chambers

- HEROC is Scheduled to Meet Wednesday, October 23, 2024, at 8:00AM in Council Chambers
- Touch a Truck Public Power Electric Event will be Friday, October 4, 2024, from 10:00AM-12:00PM at the Borough Office
- Curbside Chipping is Scheduled for Monday, October 7, 2024
- The Hatfield Borough Offices will be closed on Monday, October 14, 2024, for the Columbus Day Holiday
- Leaf Bag Collection Starts Monday, October 28, 2024

**4. Public Hearing for Ordinance No. 554 Non-Electoral General Obligation Debt: Jeffrey Calhoun, Calhoun Baker Inc.**

Councilman Ferguson suspended the Workshop Scheduled Public Meeting and opened for the Scheduled Public Hearing. A Court Reporter was present and the Public Hearing closed at 7:36PM.

**5. Public Hearing for Ordinance No. 555 Municipal Waste and Recycling**

Councilman Ferguson suspended the Workshop Scheduled Public Meeting and opened for the Scheduled Public Hearing. A Court Reporter was present and the Public Hearing closed at 7:45PM.

**6. REPORTS FROM STANDING COMMITTEES AND MAYOR:**

**Budget, Finance, and Labor Committee Report**

Councilmember Ferguson stated that the committee met and they started to discuss the 2025 budget.

**Planning, Building, and Zoning Committee Report**

Councilmember Burns stated that the committee did not meet but they are waiting on comments from the solicitor for the potential Rental Inspection Ordinance and they should have a draft copy of the ordinance soon.

**Public Safety Committee Report**

Councilmember Kroesser stated that the committee has not met and there is nothing new to report to council.

**Public Works & Property and Equipment Committee Report**

Councilmember Fagan stated that the committee has not met and there is nothing new to report to council.

**Utilities Committee Report**

Councilmember Girard stated that the committee has not met and there is nothing new to report to council.

**Hatfield Economic Revitalization Outreach Committee Report**

Councilmember Girard stated that the committee has not met and informed council that there was a nice turn out for fall festival.

**Mayor Mary Anne Girard's Report**

Mayor Mary Anne Girard reported that a girl scout built a free library outside of the borough office. Mayor Mary Anne Girard also reviewed the results of the Hatfield Township Garden Contest from this summer and the two borough residents placed first in two categories.

**7. REPORTS AND CORRESPONDENCE:**

- Monthly Investment Report
- Monthly EIT / LST Report
- Monthly Zoning Hearing Board Applications
- Police Department Report
- Fire Department Report
- EMS Report
- Public Works Department Report
- Engineering Report
- Zoning Officer, Building Code, Property Maintenance Report
- Fire Marshal / Fire Safety Inspection Report
- Pool Advisory Report

**8. MANAGERS REPORT****1. Land Use & Development Updates:**

- A. Edinburgh Square Subdivision
  - Maintenance Bond in place
- B. Bennetts Court Land Development
  - Paving & Final Improvements
  - Settlements Occurring
- C. 43 Roosevelt Land Development
  - Developers Agreement
  - Stormwater Management Agreement
- D. SEPTA Property
  - Long-Term Lease Agreement – Approved 6/14/2023
  - Working with Consultant
  - Working on a Lease Agreement with Tenant
- E. 200 N. Main Street (Biblical Seminary)
  - Sketch Plan Submitted
  - Applying for Tax Credits for Project
  - Received Grant for the Development
  - Looking at Zoning Extension – received 8/10/23
  - Updated Letter of Support for Tax Credits
  - Updated “Will Serve” Letters Issued
- F. 23 N. Main Street
  - ZHB Approved with Conditions 4/24/24
  - Spoke with Builder

- Tentatively looking at 10/28 PC and 11/6 or 11/20 Council
- H. George Didden Greenhouses
  - Went to Planning Commission – preliminary approval given
  - Needs to go to ZHB - waiting for application

**2. Utility Billing Update:**

- Staff continues to monitor Electric & Sewer Past Due accounts.
- Email billing is available for Electric & Sewer Accounts. Please contact the Utilities Department if you are interested in signing up. \*Details were in the Spring Borough Informer, on the Borough website, and on the back of all utility bills.
- The Electric Customer Portal has been updated. The Portal was restructured with customer input to make it more user-friendly. An updated user guide is available when opening the portal to assist with re-registration. The portal can be accessed from the Borough Website.
- <https://hatf-pa-web.amppartners.org/index.php>
- Please register exactly as it appears on your current billing.  
Example SMITH, JOHN E.

**3. 2021 Outstanding Project Updates:**

- A. The East Lincoln Avenue Bridge Replacement Project
  - All funds received; the project is closed out
  - Repayments made for debt borrowing

**4. 2024 Project Updates:**

- A. W. Broad Street, E. Broad Street, N. Market H2O / PA Small Water Storm and Sanitary Sewer Utility Replacement Project
  - Placed on PennBid week of June 10<sup>th</sup>
  - Bid Opened 7/10/24
  - Pre-Con Meeting 8/16/24
  - Project Start Date: Mid-October
  - Borrowing Advertisement 9/18 Ordinance 10/2
- B. 2024 Curb and ADA Project
  - Project Complete
- C. 2024 Roadway Resurfacing Project
  - Project Complete

**5. 2024 and Beyond Project Updates:**

- A. MTF / CTP Crosswalk Grants (after Utility Replacement Project)
  - HOP Application – realign crosswalk to the intersection
  - Coordination with Storm and Sanitary H2O / PA Small Water Grant Project - working with Engineer
- B. Stormwater Feasibility Study Grant with HT (Local Share Funds)
  - Meeting scheduled for October

**6. PMEA Update:**

**7. Public Information Officer Update:**

**8. Items of Interest:**

**9. NEW BUSINESS / DISCUSSION ITEMS:**

**A. Resolution 2024-20 Recognizing Public Power Week**

Manager Snyder stated that this is an annual resolution for Public Power Week that will be on for consideration tonight.

**B. Resolution 2024-21 Recognizing Fire Prevention Week**

Manager Snyder stated that this is an annual resolution for Fire Prevention Week that will be on for consideration tonight.

**10. OLD BUSINESS:**

**A. Northern Montgomery County Recycling Commission International Agreement**

Manager Snyder stated that this was on for review at the September 18, 2024 Workshop / Regular Meeting and is on for consideration tonight.

**11. ACTION ITEMS:**

**A. Motion to Consider Ordinance No. 554 Incurrence of Non-Electoral General Obligation Debt in the Amount of \$2,900,000.00 (two million nine hundred thousand dollars)**

Motion: A motion was made by Councilmember Girard to Approve Ordinance No. 554 Incurrence of Non-Electoral General Obligation Debt in the Amount of \$2,900,000.00 (two million nine hundred thousand dollars). The motion was seconded by Councilmember Fagan.

President Ferguson asked if there were any comments or questions. There were no comments or questions.

The motion was approved unanimously with a vote of 5-0.

**B. Motion to Consider Ordinance No. 555 Municipal Waste and Recycling**

Motion: A motion was made by Councilmember Kroesser to Approve Ordinance No. 555 Municipal Waste and Recycling. The motion was seconded by Councilmember Girard.

President Ferguson asked if there were any comments or questions. There were no comments or questions.



The motion was approved unanimously with a vote of 5-0.

**C. Motion to Consider the Northern Montgomery County Recycling Commission Intermunicipal Agreement**

Motion: A motion was made by Councilmember Fagan to Approve the Northern Montgomery County Recycling Commission Intermunicipal Agreement. The motion was seconded by Councilmember Girard.

President Ferguson asked if there were any comments or questions. There were no comments or questions.

The motion was approved unanimously with a vote of 5-0.

**D. Motion to Consider Resolution 2024-20 Recognizing Public Power Week**

Motion: A motion was made by Councilmember Fagan to Approve Resolution 2024-20 Recognizing Public Power Week. The motion was seconded by Councilmember Girard.

President Ferguson asked if there were any comments or questions. There were no comments or questions.

**E. Motion to Consider Resolution 2024-21 Recognizing Fire Prevention Week**

Motion: A motion was made by Councilmember Burns to Approve Resolution 2024-21 Recognizing Fire Prevention. The motion was seconded by Councilmember Girard.

President Ferguson asked if there were any comments or questions. There were no comments or questions.

**10. ADJOURNMENT:**

Motion: A motion was made by Councilmember Kroesser to adjourn the Workshop Meeting of October 2, 2024. The motion was seconded by Councilmember Girard and unanimously approved with a vote of 5-0. The meeting adjourned at 8:18 PM.

*Executive Session Litigation, Property and Personnel*

Respectfully Submitted,  
Kathryn Vlahos  
Assistant Manager

**HATFIELD BOROUGH COUNCIL  
REGULAR MEETING  
October 16, 2024**

**MINUTES**

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BOROUGH HALL 401 S. MAIN STREET, HATFIELD  
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**CALL TO ORDER AND ROLL CALL:**

**ROLL CALL**

(X) Jason Ferguson, President  
(X) Richard Girard, Vice President  
(X) Larry Burns  
(X) James Fagan  
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(X) Mayor Mary Anne Girard

The record shows that five members of Council were present at roll call, as well as, Solicitor; Catherine Harper, Timoney Knox, LLP, Borough Manager; Jaime E. Snyder, Public Works Director; Stephen S. Fickert, Jr, Junior Council Person; Malachi Nisbett and Assistant Manager; Kathryn Vlahos.

**1. Motion to Approve the October 16, 2024 Workshop Meeting Agenda.**

Motion: A motion was made by Councilmember Kroesser for Approval of the Meeting Agenda, October 16, 2024 Regular Meeting Agenda. The motion was seconded by Councilmember Burns and unanimously approved with a vote of 5-0.

**2. APPROVAL OF THE MINUTES: Motion to Approve the Minutes of the September 18, 2024 Workshop/ Regular Meeting**

Motion: A motion was made by Councilmember Girard to Approve the Minutes of the September 18, 2024 Workshop/Regular Meeting. The motion was seconded by Councilmember Fagan and unanimously approved with a vote of 5-0.

**3. PUBLIC INPUT:** President Ferguson asked if there was any Public Input. There was no media present. No Public Comment.

4. **ANNOUNCEMENTS:** **Manager Jaime E. Snyder made the following announcements.**
- Next Council Meetings November 6<sup>th</sup> Workshop November 20<sup>th</sup> Regular Meeting at 7:00PM in Council Chambers
  - Planning Commission is Scheduled to Meet on Monday, October 28, 2024, at 6:00PM in Council Chambers
  - HEROC is Scheduled to Meet Wednesday, October 23, 2024, at 8:00AM in Council Chambers
  - The Hatfield Borough Offices will be closed on Monday, October 14, 2024, for the Columbus Day Holiday
  - Leaf Bag Collection Starts Monday, October 28, 2024. Will Continue Every Monday Through December 2, 2024
  - Halloween Happy Event is Scheduled for Thursday, October 31<sup>st</sup> from 4:30PM to 6:30PM at the Borough Office
  - Fall Budget & Projects Town Hall Meeting November 13, 2024, at 7:00PM in Council Chambers

5. **NEW BUSINESS / DISCUSSION ITEMS:**

A. **Hatfield Christmas Tree Lighting Request**

Manager Snyder explained that included in the packet was Grace Lutherans Christmas Tree Lighting request and they will like to hold it at Memorial Park on December 7<sup>th</sup> at 6:30 PM. They will start at 5:00PM with refreshments at Grace and some activities and crafts for the kids then they will head down the tree at 6:00PM where they will be caroling and light the tree at 6:30PM. This is on for consideration tonight as an action item since this is an annual event that takes place each year in the borough.

6. **OLD BUSINESS:**

7. **ACTION ITEMS:**

A. **A motion to Consider Grace Lutheran Church Request to Hold the Annual Christmas Tree Lighting at Railroad Plaza / Memorial Park**

Motion:

A motion was made by Councilmember Kroesser to Grace Lutheran Church Request to Hold the Annual Christmas Tree Lighting at Railroad Plaza / Memorial Park . The motion was seconded by Councilmember Burns.

President Ferguson asked if there were any comments or questions. There were no comments or questions.

The motion was approved unanimously with a vote of 5-0.

**8. MOTION TO APPROVE PAYMENT OF THE BILLS**

President Ferguson and Manager Snyder reviewed and answered questions regarding the bill list.

Motion: A motion was made by Councilmember Girard to Approve the payment of the bills. The motion was seconded by Councilmember Fagan.

President Ferguson asked if there were any comments or questions. There were no comments or questions.

The motion was approved unanimously with a vote of 5-0.

**9. ADJOURNMENT:**

Motion: A motion was made by Councilmember Fagan to adjourn the Regular Meeting of October 16, 2024. The motion was seconded by Councilmember Burns and unanimously approved with a vote of 5-0. The meeting adjourned at 7:21 PM.

*Executive Session Litigation, Property and Personnel*

Respectfully Submitted,  
Kathryn Vlahos  
Assistant Manager

### **3. PUBLIC INPUT:**

**Please rise, state your name and  
address and the reason for addressing  
Council**



#### **4. ANNOUNCEMENTS:**

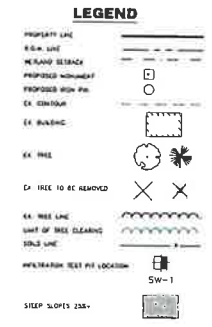
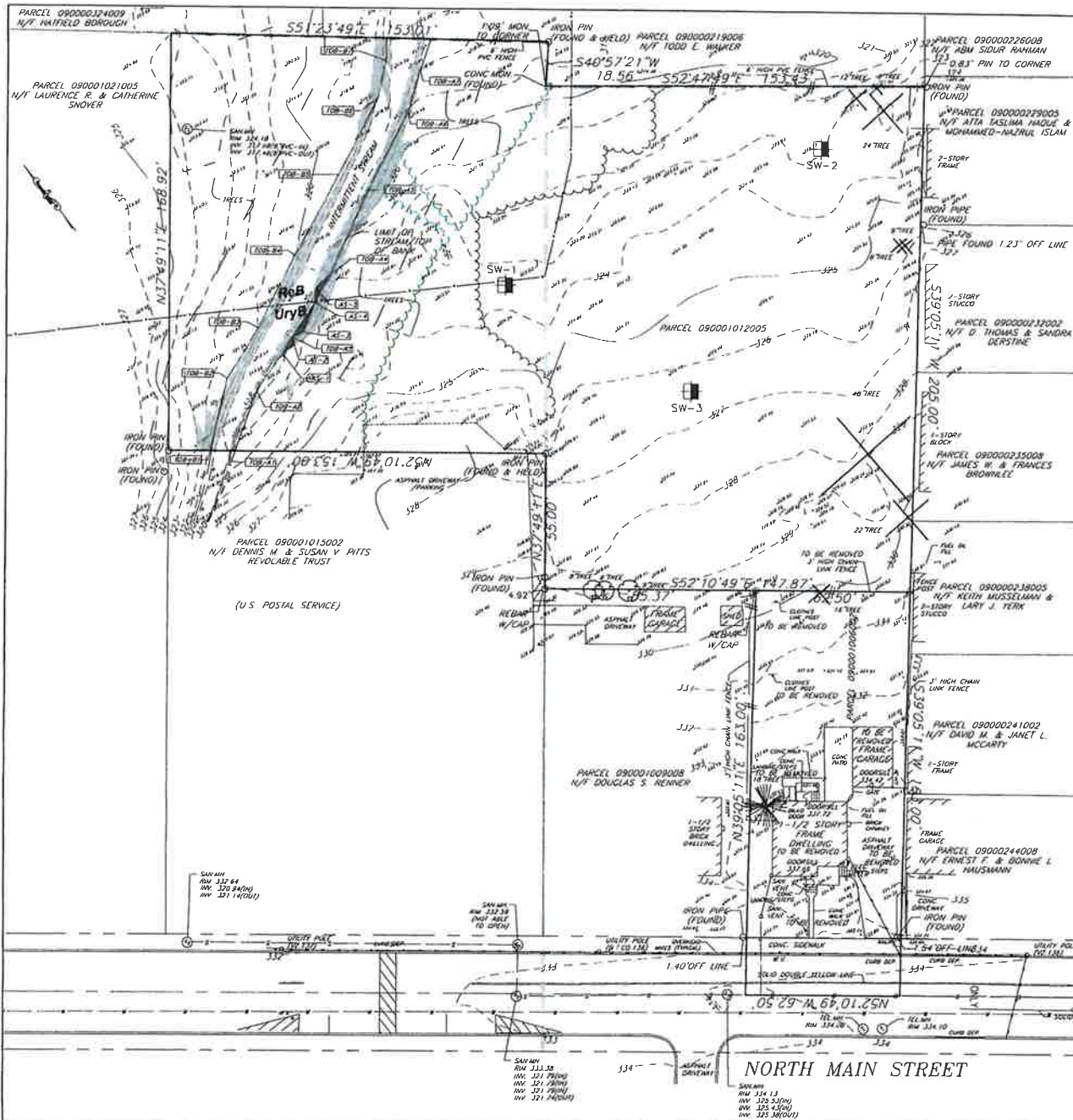
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and 29<sup>th</sup> in Observance of the  
Thanksgiving Day Holiday**
- **Leaf Bag Collection Starts Monday,  
October 28, 2024. Will Continue Every  
Monday Through December 2, 2024**

**5. 23 N Main Street, Hatfield Walk, Land  
Development Presentation**

**23 North Main Hatfield  
Walk Land Development  
Plans and  
Turning Template**










- NOTES**
- BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM A PLAN TITLED "EXISTING FEATURES" SURVEY PREPARED BY GANNETT'S SURVEYING SERVICE DATED 03-10-2013
  - ELEVATIONS ON THE PLANS ARE BASED ON PA STATE PLANE COORDINATE SYSTEM (EARTH TONES) HORIZONTAL DATUM: NAD-83 (1983 ADJUSTMENT) AND VERTICAL DATUM: NAVD-83 BASED ON A FIELD SURVEY SERVICES ON JULY 12, 2013.
  - SUBJECT PROPERTY CONSISTS OF TAX PARCEL NO. 09-00-0010-001
  - UTILIZATION TESTING HAS BEEN PERFORMED BY THE CONSULTANTS, LLC ON JULY 1, 2024.

<b>EXISTING FEATURES PLAN</b> HATFIELD WALK TMP # 0900001012005 & 090000106002 HATFIELD BOROUGH, MONTGOMERY COUNTY, PENNSYLVANIA		Holmes Consulting LLC 350 E. Butler Ave., Ste 108 New Britain, PA 18901 (215) 986-3330 www.holmesconsulting.net
REVISIONS DATE 10/17/2024	DIMENSION RECALCULATED PER BOROUGH, TRAFFIC & FIRE CODES	SCALE 1" = 100'
DRAWING NO. <b>C1.1</b>		





		<b>REVISED</b>	
Holmes Cunningham LLC 350 E. Butler Ave., Ste 106 New Britain, PA 18901 (215) 986-3330 www.hcengineering.net		Date 10/13/2024	Drawn by JES/EDS/BOB/THATK & JRE/CMTS
		Scale 1"=40'	
<b>HATFIELD WALK</b>		Drawing No. <b>C1.2</b>	
TMP # 09000102005 & 09000106002		Sheet 3 of 25	
HATFIELD BOROUGH, MONTGOMERY COUNTY, PENNSYLVANIA		Designed RC	
<b>AERIAL PHOTO PLAN</b>		Checked RC	
		Status RC	







#### NOTES

1. ALL POSTS SHALL BE OF ADEQUATE LENGTH TO MEET THE REQUIREMENTS FOR ERECTION AS STATED IN THE CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS".
2. ALL POSTS SHALL BE EMBEDDED 4'-2" MINIMUM BELOW GRADE.
3. ALL STEEL POSTS AND BRACKETS SHALL BE CUT, BENT, AND HOLES PUNCHED AND DRILLED BY THE MANUFACTURING MANUFACTURER. GALVANNEZING SHALL BE IN CONFORMANCE WITH CURRENT A.S.T.M. SPECIFICATION A133-78 (OR LATEST REVISED).
4. POSTS MAY BE STEEL, ALUMINUM, OR TWO-PIECE U-POST.
5. SIGN PANEL SIZES SHALL DETAILING POST TYPE AND DIMENSIONS AS SHOWN ON THIS DETAIL AND DIRECTIONAL SIGN SHEET.
6. BOLTS SHALL NOT PROTRUDE MORE THAN 3/4" BEYOND THE 1/2" INCH SCOT BUT SHALL EXCEED ALL THREADS IN THE NUT.
7. ALL TRAFFIC AND PEDESTRIAN SIGNS, AND LOCATION SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND ALL CURRENT REQUIREMENTS.
8. SIGNS SHOULD BE INSTALLED ON PENNDOT APPROVED BREAKAWAY POSTS.

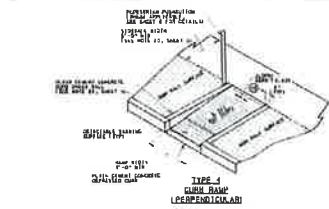
#### STOP SIGN



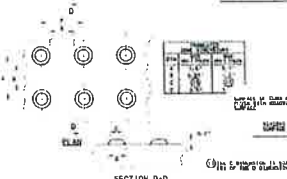
#### NOTES

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7. ALL TRAFFIC AND PEDESTRIAN SIGNS, AND LOCATION SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND ALL CURRENT REQUIREMENTS.

#### SIGNS DETAIL



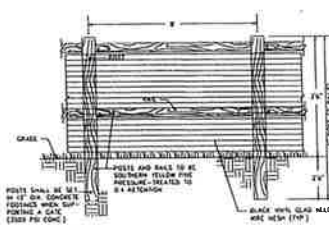
#### CURB RAMP DETAIL



#### DETECTABLE WARNING SURFACE (TYPICAL) DETAIL

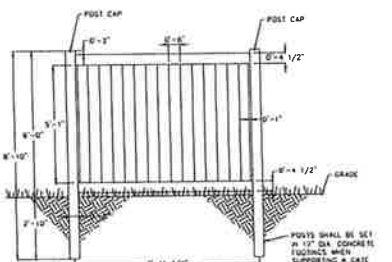
#### NOTES

1. DETECTABLE WARNING SURFACES SHALL BE INSTALLED AT ALL CURB RAMP AND CROSSINGS.



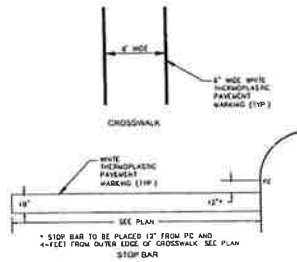
HEIGHT	28"	HEIGHT ON POST END OF GROUND
DEPTH	28"	DEPTH SET INTO GROUND
LENGTH	8'	POST SPACING CENTER TO CENTER
RAILS	2-1 1/2" x 4" x 1/8"	2 EACH, 8 FT. RAIL THICKNESS
SPACE	3-1 1/2"	BETWEEN RAILS
STEEL REINFORCEMENT	NO	
POST SIZE	4" SQUARE	3.16 MIN. THICKNESS

#### POST AND RAIL FENCE



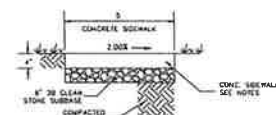
1. ALL FENCE POSTS ARE TO BE SET IN CONCRETE FOOTINGS.
2. FOR ADDITIONAL INFORMATION REFER TO MANUFACTURERS' SPECIFICATIONS.

#### SOLID VINYL PRIVACY FENCE



1. STOP BAR TO BE PLACED 1' FROM PE AND 4'-1' FROM OUTER EDGE OF CROSSWALK. SEE PLAN.
2. STOP BAR TO BE PLACED 1' FROM PE AND 4'-1' FROM OUTER EDGE OF CROSSWALK. SEE PLAN.

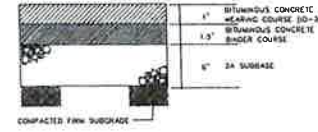
#### PAVEMENT MARKINGS



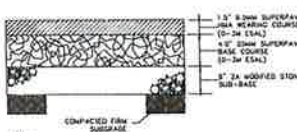
#### SIDEWALK SECTION

1. SIDEWALK IS TO BE PROVIDED, PLACED, CURED AND FINISHED TO PERFORM SPECIFICATIONS PLUS (CURRENT EDITION), SECTIONS 704 AND 1001. TYPE A CONCRETE (3,000 PSI, TWENTY-FOUR (24) STRENGTH) SHALL BE USED.
2. CONTRACTION JOINTS SPACED AT EQUAL INTERVALS BETWEEN EXPANSION JOINTS, NOT TO EXCEED 8 FEET FOR SIDEWALKS TO BE FORMED BY DIVISION PLATES OR CUTTING GROOVE INTO CONCRETE SURFACE NOT LESS THAN 1/3 CURVE DEPTH OF SLAB.
3. SIDEWALK THICKNESS 8 INCHES AT RESIDENTIAL DRIVEWAYS, FINISHED WITH WOOD (FLAT) BEVELLED FINISH FOR SLOPES IN EXCESS OF 1%. THE CONCRETE SIDEWALK SHALL BE PLACED UPON A SIX-INCH THICK COMPACTED STONE BASE, CONSISTING OF TYPE 2A STONE.
4. DRAINAGE SLOPE TO BE MAINTAINED AT 2% TOWARDS CURB.
5. EXPANSION JOINTS TO BE ONE-HALF-INCH PRECURED, BITUMINOUS, EXPANSION JOINT MATERIAL AT THIRTY-FOOT INTERVALS, NEXT TO BUILDINGS, WALLS, STRUCTURES AND CURBS, ARISING INTERSECTIONS OF TWO WALKS, DRIVEWAY APPROX JOINT TO WALK, CURB OR SIDEWALK, AND AT ENDS OF ALL WALKS AND CURBS INCLUDING RADIUS CURBS.
6. MINIMUM 102 8" x 16" x 1/4" MUST BE USED IN CONCRETE DRIVEWAY CONSTRUCTION.
7. CONTRACTION JOINTS ARE TO BE PLACED EVERY 8' AND EXPANSION JOINTS TO ARE TO BE PLACED EVERY 30'.
8. SIDEWALK BEDDING SHALL BE 4" OF 20 CLEAN STONE.
9. CLASS A AIR ENTRAINED CONCRETE MIX IS TO BE USED FOR SIDEWALK CONSTRUCTION.
10. SIDEWALK IN BOROUGH RIGHT-OF-WAY SHALL BE CURED WITH ABRASION 3000 OR APPROVED EQUAL.

#### SIDEWALK DETAIL

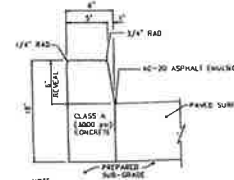


#### INDIVIDUAL UNIT DRIVEWAY PAVEMENT SECTION



1. ALL COURSES SHALL CONFORM TO PENNDOT REQUIREMENTS.
2. PAVEMENT SECTION TO BE USED FOR ALL RIGIDS.
3. PAVING MATERIAL SHALL BE 3/4" TO 1 1/2" MAX. DESIGN.

#### SHARED DRIVEWAY PAVEMENT SECTION



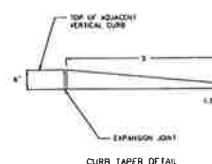
#### CONCRETE CURB

1. ALL CURBING TO BE CONSTRUCTED IN 10 FT. SECTIONS.
2. CONTRACTION JOINTS TO BE 2" DEEP & 3/16" WIDE, 100'-EDGED TO A 1/4" RADIUS.



#### BELGIAN BLOCK CURB

1. ON TRANSITIONS FROM AND TO PROPOSED CURB, USE ONE LARGE BLOCK, SLOPED AND CUT TO FIT.
2. CURB IMMEDIATELY ADJACENT TO INLET CASTINGS MUST HAVE PREPARED EXPANSION MATERIAL.
3. CONCRETE JOINTS USE MORTAR WITH COMPOSITION - 3 SAND / CEMENT JOINTS TO BE 1/2" WIDE.
4. 6" BELGIAN BLOCK CURB TO BE USED FOR ALL ON-SITE RESIDENTIAL ROADS AND RETAIL ROADS.
5. CONCRETE CURB TO BE USED IN LEGAL RIGHT-OF-WAY.



#### CURB TAPER DETAIL

Holmes Cunningham LLC  
3550 E. Butler Ave., Ste 100  
New Britain, PA 18901  
(215) 586-3330  
www.hcengineering.net



REVISIONS	Description	Date
1	REVISED FOR BIDDING, TRAFFIC & FIRE CENTS	10/11/2024

HATFIELD WALK  
TMP # 090001012005 & 090001006002  
HATFIELD BOROUGH, MONTCOMERY COUNTY, PENNSYLVANIA

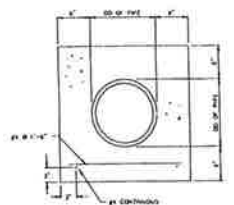


File No.	1727, C2.0, 2.1
Proj. No.	1727
Date	08/07/2024
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Drawn	ME
Checked	ME
Sheet	8 of 15

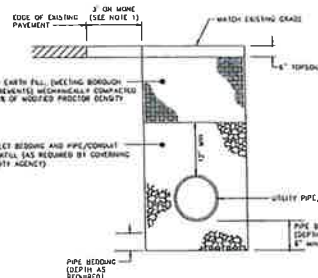
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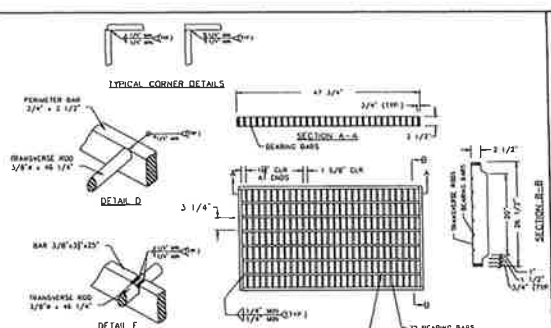


NOTE: PROVIDE REINFORCEMENT AT UTILITY CROSSINGS AS DIRECTED BY THE AUTHORITY'S ENGINEER.  
**CONCRETE ENCLOSURE DETAIL**

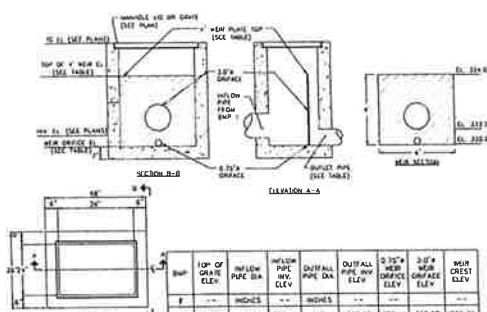


NOTES:  
1. TRENCH OPENINGS LESS THAN 3' FROM THE EDGE OF AN EXISTING PAVED SURFACE SHALL BE BACKFILLED WITH A 2A COARSE AGGREGATE MECHANICALLY TAMPED IN 6" LAYERS.

**TRENCH RESTORATION IN UNGRADED AREA**

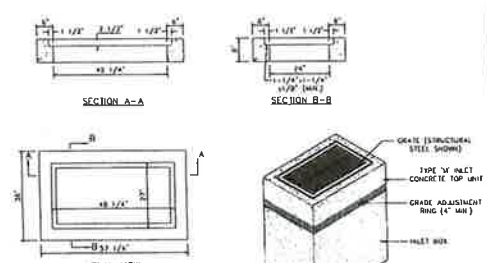


**STRUCTURAL STEEL BICYCLE SAFE INLET GRATE DETAIL**



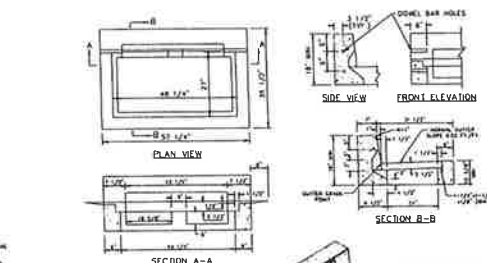
NOTES:  
1. OUTLET STRUCTURES SHALL BE AS DETAIL IN PENNSYLVANIA DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DESIGN STANDARDS FOR ROADWAY CONSTRUCTION, CURRENT EDITION (POT FOR 2004) RC-34A, INLET, CONCRETE TOP UNITS CAST-IN-PLACE AND PRECAST.  
2. STEPS SHALL BE PROVIDED WHENEVER STRUCTURE EXCEEDS 4 FEET IN DEPTH.  
3. THE CONTRACTOR SHALL PROVIDE OUT INLETS TO BE ORDERED TO BE ORDERED PRIOR TO CONSTRUCTION.  
4. REFER TO THE SUBSURFACE BASIN DETAIL FOR ADDITIONAL CONNECTION INFORMATION.

**SUBSURFACE STORMWATER FACILITY OUTLET STRUCTURE DETAIL**



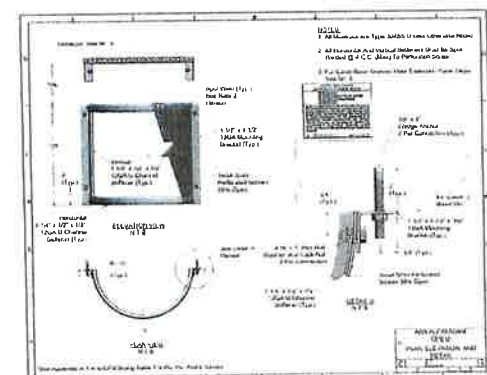
NOTES:  
1. REFER TO "CONCRETE HEALTH OF PENNSYLVANIA DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DESIGN STANDARDS FOR ROADWAY CONSTRUCTION, CURRENT EDITION (POT FOR 2004) RC-34A, INLET, CONCRETE TOP UNITS CAST-IN-PLACE AND PRECAST."  
2. ALL INLET TOP SHALL BE THE ENVIRONMENT TYPE.  
3. ALL INLETS IN AREAS TO BE PAVED ARE TO BE BACKFILLED WITH 2A MATERIAL.

**TYPE M INLET DETAIL**

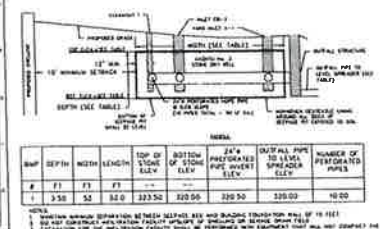


NOTES:  
1. REFER TO "CONCRETE HEALTH OF PENNSYLVANIA DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DESIGN STANDARDS FOR ROADWAY CONSTRUCTION, CURRENT EDITION (POT FOR 2004) RC-34A, INLET, CONCRETE TOP UNITS CAST-IN-PLACE AND PRECAST."  
2. ALL INLET TOP SHALL BE THE ENVIRONMENT TYPE.  
3. ALL INLETS IN AREAS TO BE PAVED ARE TO BE BACKFILLED WITH 2A MATERIAL.

**TYPE C INLET DETAIL**



**ORIFICE SCREEN DETAIL**



NOTES:  
1. THE CONTRACTOR SHALL PROVIDE A 3' ON MORE DIMENSION AT THE TOP OF THE STRUCTURE.  
2. THE CONTRACTOR SHALL PROVIDE A 3' ON MORE DIMENSION AT THE TOP OF THE STRUCTURE.  
3. THE CONTRACTOR SHALL PROVIDE A 3' ON MORE DIMENSION AT THE TOP OF THE STRUCTURE.

**SUBSURFACE BASIN DETAIL**

**CONSTRUCTION STANDARDS FOR PUMP BASINS**

1. PROVIDE CONSTRUCTION STANDARDS FOR PUMP BASINS.
2. REFER TO THIS SHEET FOR CONSTRUCTION STANDARDS.
3. UNLESS OTHERWISE SPECIFIED, THE INDIVIDUAL BASIN SHALL BE CONSTRUCTED FOR THE SEQUENCE IDENTIFIED BELOW.
4. PROTECT THE AREA FROM CONSTRUCTION FROM TO INSTALLATION USING DRAINAGE CONSTRUCTION FENCE AROUND THE PERIMETER OF THE BASIN.
5. IF POSSIBLE, INSTALL DIRT MOUND/SEDIMENTATION PORT/SCREENS DURING LATER PHASES OF SITE CONSTRUCTION TO PREVENT SEDIMENTATION AND/OR DAMAGE FROM CONSTRUCTION ACTIVITY.
6. POST AND MAINTAIN PROPER EROSION AND SEDIMENT CONTROL MEASURES DURING CONSTRUCTION AS PER THE PENNSYLVANIA EROSION AND SEDIMENT POLLUTION CONTROL PROGRAM MANUAL (REVISED 06/01) PRIOR TO EXCAVATION OF THE BASIN.
7. INSTALL CONCRETE FILTER SOCK UPSTREAM OF THE PROPOSED BASIN TO PREVENT SEDIMENTATION OF THE BASIN.
8. EXCAVATE BASIN FLOOR TO A UNIFORM LEVEL UNDISTURBED SUBGRADE FREE FROM ROCKS AND DEBRIS OR USE COMPACT SUBGRADE TO THE GREATEST EXTENT POSSIBLE. EXCAVATION EQUIPMENT SHOULD BE PLACED OUTSIDE THE LIMITS OF THE FACILITY BASIN.
9. PRIOR TO INSTALLATION OF STORMWATER BASIN MATERIAL, STONE AND SYSTEM COMPONENTS, A MINIMUM OF TWO (2) INFORMATION TESTS SHALL BE PERFORMED ON THE SUBGRADE. THE RESULTS AND TESTS SHALL BE COORDINATED WITH AND SUBMITTED TO THE WISCONSIN COUNTY COMMISSIONER'S OFFICE.
10. COMPLETELY REINFORCE AND COVER WITH REINFORCING CONCRETE (REINFORCING CONCRETE) UNDER PUMP BASIN. REINFORCE PRIOR TO CASTING. REINFORCEMENT SHALL BE A MINIMUM OF 30 MESH. FOUR BACK PLACEMENT LAYER SHALL BE A MINIMUM OF 30 MESH. FOUR BACK PLACEMENT LAYER SHALL BE A MINIMUM OF 30 MESH. FOUR BACK PLACEMENT LAYER SHALL BE A MINIMUM OF 30 MESH.
11. PLACE FIRST LIFT OF CLEAN STONE AGGREGATE AND LEVEL BEFORE PLACEMENT OF REINFORCING CONCRETE.
12. INSTALL LIGHTWEIGHT PORTLAND CEMENT CONCRETE WITH CLEANAGENTS, UNITS, REINFORCING, AND ALL OTHER STRUCTURES. CONNECT OUTLETS AND PIPES TO STRUCTURES. AND ALL OTHER STRUCTURES. CONNECT OUTLETS AND PIPES TO STRUCTURES. AND ALL OTHER STRUCTURES. CONNECT OUTLETS AND PIPES TO STRUCTURES.
13. PLACE UNIFORM GRADE, CLEAN-WASHED AGGREGATE IN 6-INCH LIFTS, LIGHTLY COMPACTING BETWEEN LIFTS.
14. FILL AND SECURE NONPUMPING GUTTER OVER TRENCH, WITH MINIMUM OVERLAP OF 12 INCHES.
15. PLACE LIFT OF SURFACE BASE OVER CLOSED GUTTER ENVELOPE, AS INDICATED ON PLANS.
16. SEED AND STABILIZE TOPSOIL, IF NECESSARY SURFACES IS PROPOSED.
17. DO NOT REGRADE EROSION AND SEDIMENT CONTROL MEASURES UNTIL SITE IS FULLY STABILIZED.

**MAINTENANCE PLAN**

- NOTE: AN ANNUAL REPORT SHALL BE PREPARED AND RETAINED BY THE RESPONSIBLE PARTY STATING THE FOLLOWING MAINTENANCE HAS BEEN PERFORMED:
- 1. THE OWNER IS RESPONSIBLE FOR MAINTENANCE OF THE STORMWATER CONVEYANCE SYSTEM SUBSURFACE INFILTRATION AREA, WATER QUALITY INSERTS, AND ALL OTHER PROPOSED BASINS.
  - 2. STORMWATER CONVEYANCE SYSTEM
  - 3. CATCH BASIN, MANHOLE, HEADWALL, TRENCH, RACKS, AND PIPES TO BE INSPECTED FOR CLOGGING AND EXCESSIVE SEDIMENT AND SIGNIFICANT ACCUMULATION AT LEAST ANNUALLY AS WELL AS AFTER EVERY STORM EXCEEDING 1-INCH OF RAINFALL. SEDIMENT SHALL BE REMOVED FROM THE TOP AND/OR INSIDE OF THE STRUCTURE.
  - 4. ALL STRUCTURAL COMPONENTS MUST BE INSPECTED FOR CRACKING, SQUEEZING, BULGING, WEARING, AND CORROSION AT LEAST ANNUALLY.
  - 5. DAMPS BY INLET AND MANHOLE STRUCTURES SHALL BE INSPECTED FOR DEBRIS FOLLOWING THE SCHEDULE ABOVE. IF PRESENT, ANY DEBRIS SHALL BE REMOVED FROM THE BOTTOM OF THE STRUCTURE.
  - 6. TRENCH RACKS AT HEADWALLS SHALL BE INSPECTED FOR CLOGGING AND/OR DEBRIS FOLLOWING THE SCHEDULE ABOVE. IF PRESENT, ANY DEBRIS SHALL BE REMOVED FROM THE TRENCH RACK.
  - 7. STORMWATER INFILTRATION BASIN
  - 8. INSPECT SUBSURFACE BASIN AT LEAST FOUR TIMES A YEAR, AS WELL AS AFTER EVERY SIGNIFICANT STORM.
  - 9. ENSURE THAT RUNOFF DRAINS DOWN TO THE LOWEST OUTFALL ELEVATION WITHIN AT LEAST 15 MINUTES OF RAIN-FALL DOWN TIME AND EXCEEDING THE MAXIMUM DRAIN THE FACILITY WAS PUMPING AND CLEAN OUT PERFORMED PIPING.
  - 10. AT LEAST TWO TIMES PER YEAR, INSPECT FOR ACCUMULATION OF SEDIMENT AND/OR DEBRIS. DISPOSE OF SEDIMENT, DEBRIS, AND ANY OTHER WASTE MATERIAL REMOVED FROM A DRY WELL AT OUTFALL DISPOSAL/RECYCLING SITES AND IN COMPLIANCE WITH LOCAL, STATE, AND FEDERAL WASTE REGULATIONS.
  - 11. AS NECESSARY, REMOVE ACCUMULATION OF SEDIMENT AND/OR DEBRIS FROM THE SYSTEM USING STRUCTURES AND/OR CLEANOUTS AT THE CORNERS OF THE BASIN, IN ORDER TO MAINTAIN FLOW THROUGH THE BASIN AND TO MAINTAIN WATER QUALITY FUNCTIONALITY.
  - 12. REGULARLY CLEAN AND MAINTAIN AND INSURE PROPER CONNECTIONS TO MAINTAIN THE EFFECTIVENESS OF THE FACILITY.
  - 13. REPAIR AND REPLACE ANY COMPONENTS AS NECESSARY.
  - 14. AN INTERMITTENT DRAIN BASIN SHALL BE CLEANED OUT AT LEAST ONCE PER YEAR.
  - 15. INSPECT FOR CORROSION OF BASIN MATERIALS.
  - 16. THE FACILITY SHALL BE MAINTAINED AS INDICATED IN THE PAGE.
  - 17. STORMWATER BASIN MATERIALS

**HATFIELD WALK**  
TMP # 090001032005 & 09000106002  
HATFIELD BOROUGH, MONTEGOMERY COUNTY, PENNSYLVANIA

**PCSM DETAILS**

Drawing No. **C3.1**

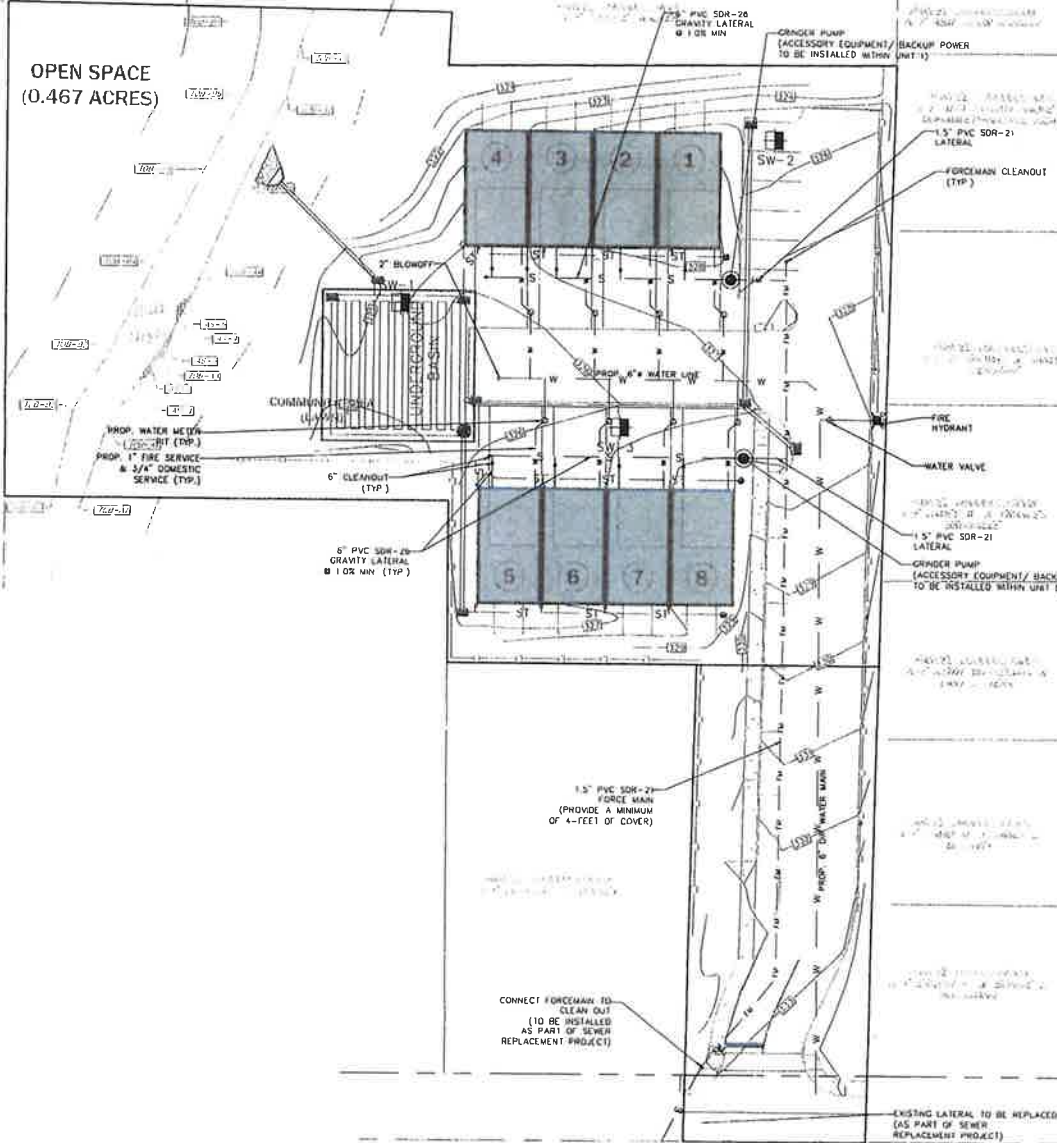
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OPEN SPACE  
(0.467 ACRES)



NORTH MAIN STREET

- NOTES**
1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE TOWN OF HATFIELD, PENNSYLVANIA AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL EXISTING UTILITIES TO BE LOCATED AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO BE UNDERTAKEN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF EXISTING UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF EXISTING UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF EXISTING UTILITIES.
  2. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS AND SPECIFICATIONS FOR ALL UTILITY LOCATIONS. CONTRACTOR SHALL COORDINATE WITH ALL AGENCIES INVOLVED IN THE PROJECT, INCLUDING BUT NOT LIMITED TO, THE TOWN OF HATFIELD, PENNSYLVANIA, AND THE PENNSYLVANIA DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT.
  3. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING THE EXISTING UTILITY SERVICE (WATER, SEWER, GAS AND ELECTRIC) TO THE PROPERTY AND LOCATING THE EXISTING UTILITY SERVICE (WATER, SEWER, GAS AND ELECTRIC) TO THE PROPERTY AND LOCATING THE EXISTING UTILITY SERVICE (WATER, SEWER, GAS AND ELECTRIC) TO THE PROPERTY.
  4. ALL GAS WORK AND OTHER ASSOCIATED APPLIANCES SHALL BE IN CONFORMANCE WITH APPLICABLE LOCAL, COUNTY, STATE AND FEDERAL REGULATIONS AND REQUIREMENTS.
  5. ALL ELECTRICAL WORK, INCLUDING PANELS AND ASSOCIATED APPLIANCES, SHALL BE IN CONFORMANCE WITH APPLICABLE LOCAL, COUNTY, STATE AND FEDERAL REGULATIONS AND REQUIREMENTS.
  6. THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFYING ALL EXISTING UTILITY LOCATIONS. CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DISCREPANCIES TO EXISTING UTILITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DISCREPANCIES TO EXISTING UTILITIES.
  7. THE CONTRACTOR SHALL COORDINATE THE WATER WORK WITH NORTH MAIN WATER AUTHORITY.
  8. THE CONTRACTOR SHALL COORDINATE THE SANITARY SEWER WORK WITH HATFIELD BOROUGH MUNICIPAL AUTHORITY.
  9. A MINIMUM TEN (10) FOOT HORIZONTAL AND EIGHTEEN (18) INCHES VERTICAL CLEARANCE MUST BE MAINTAINED BETWEEN WATER MAIN AND SEWER SEWER.
  10. EIGHTEEN (18) INCHES VERTICAL CLEARANCE SHOULD BE PROVIDED AT ALL UTILITY CROSSINGS WHERE THE CLEARANCE CANNOT BE MAINTAINED. THE WATER MAIN MUST BE ENCASED IN CONCRETE TO FEET EACH SIDE OF THE CROSSING IN CASES WHERE THE UTILITY IS A SANITARY OR STORM SEWER MAIN OF A LATERAL AND THE CLEARANCE CANNOT BE MAINTAINED. THE SEWER MAIN OR STORM SEWER SHALL ALSO BE ENCASED IN CONCRETE.
  11. ALL SANITARY SEWER FACILITIES SHOULD BE CONSTRUCTED IN ACCORDANCE WITH THE HATFIELD BOROUGH MUNICIPAL AUTHORITY SPECIFICATIONS.
  12. NO PLANTING OR STRUCTURES SHALL BE LOCATED WITHIN SANITARY SEWER EASEMENTS OR WITHIN 10 FEET OF THE SEWER OR LATERALS.
  13. A MINIMUM COVER OF FOUR FEET SHOULD BE MAINTAINED.
  14. ALL NEW FIRE HYDRANTS SHALL BE PROVIDED IN ACCORDANCE WITH THE 19TH REQUIREMENTS AND SHALL BE PROVIDED WITH APPROVED SIGN MARKERS.
  15. WATER MAINS AND FIRE HYDRANTS SHALL BE INSTALLED AND OPERATIONAL FROM THE START OF CONSTRUCTION ON ANY BUILDING.
  16. THE DEVELOPER AND THEIR CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF THE SEWER WITH UNIFORM SLOPE BETWEEN MANHOLES AND SHALL BE RESPONSIBLE FOR CORRECTING ANY DEVIATIONS FROM UNIFORM SLOPE THAT ARE DETECTED DURING THE POST-CONSTRUCTION SURVEY AND INSPECTION.
  17. THE SANITARY SEWER SYSTEM IN NORTH MAIN STREET IS IN THE PROCESS OF BEING REPLACED BY HATFIELD BOROUGH DURING THE DESIGN OF THESE PLANS. THE CONFIGURATION OF THE SANITARY LATERAL CONNECTION MAY BE DIFFERENT THAN WHAT IS ILLUSTRATED ON THESE PLANS BY THE TIME THE SITE IS BEING DEVELOPED.

**UTILITY LEGEND**



Holmes Consulting LLC  
350 E. Butler Ave., Ste 106  
New Britain, PA 18001  
(215) 586-3330  
www.holmesconsulting.net

REVISIONS	DATE	BY	DESCRIPTION
1	10/11/2024	REVISED PER HATFIELD TOWNSHIP & FIRE CODES	

FILE NO. 1727\_C4.0\_UTILITY.dwg  
DATE 08/07/2024  
SCALE 1"=20'  
DESIGNED BY RC  
SHEET 8 OF 15

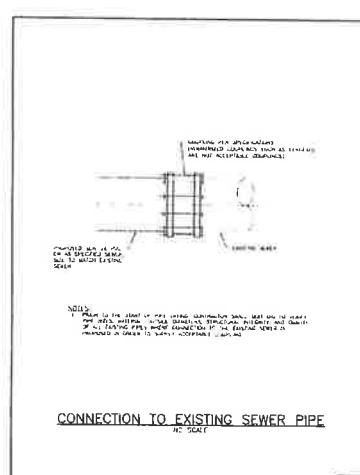
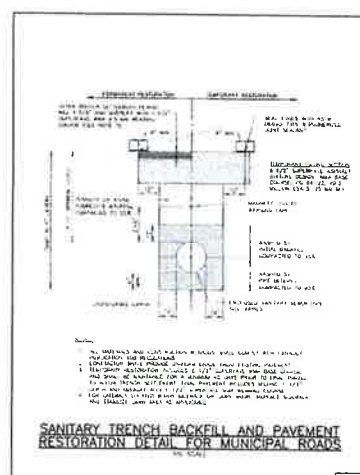
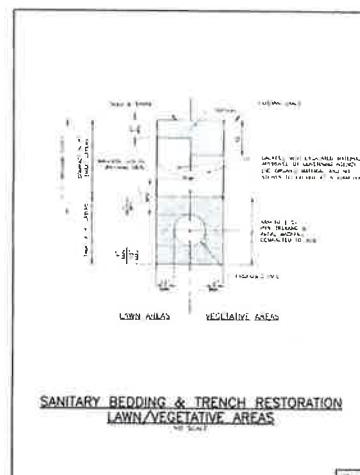
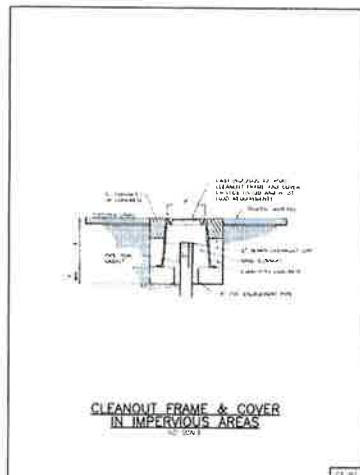
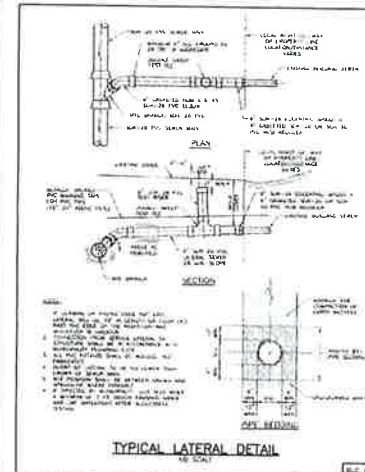
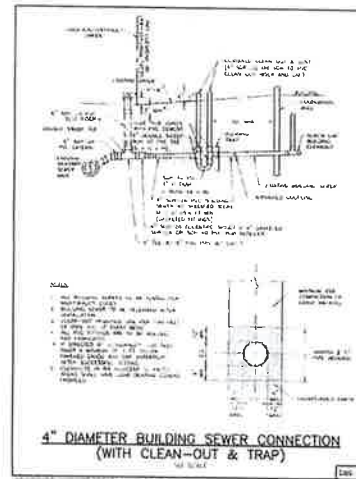
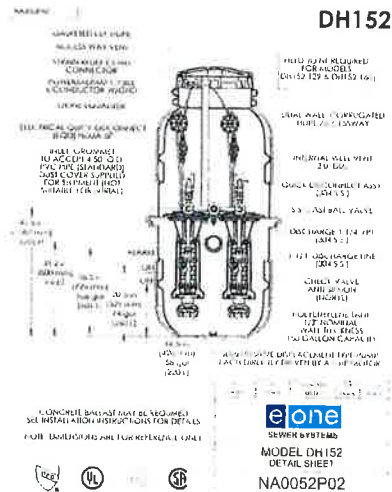
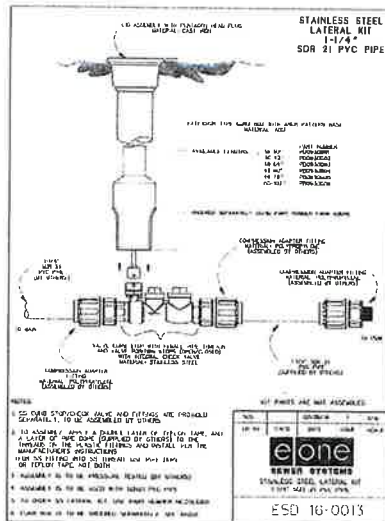
HATFIELD WALK  
TMP # 090001012005 & 09000106002  
HATFIELD BOROUGH, MONTGOMERY COUNTY, PENNSYLVANIA

UTILITY PLAN

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Date 08/07/2024  
Scale 1"=20'  
Designed BY RC  
Sheet 8 of 15

Drawing No. C4.0





REV	DATE	DESCRIPTION
1	10/12/2024	REVISED PER BIDDING, MATERIALS & PIPE SIZES

Holmes Cunningham LLC  
3340 E. Butler Ave., Ste 106  
New Britain, PA 18901  
(215) 986-3330  
www.hcengineering.net

HATFIELD WALK  
TMP # 090001042005 & 09000106002  
HATFIELD BOROUGH, MONTGOMERY COUNTY, PENNSYLVANIA

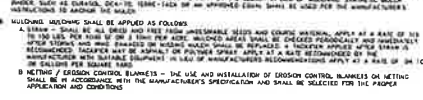
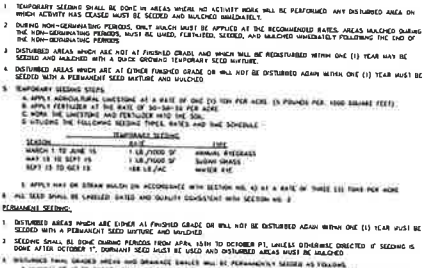
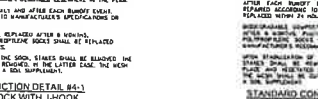
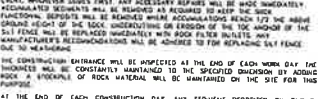
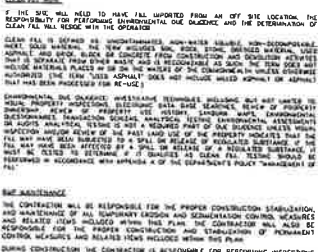
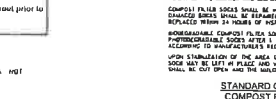
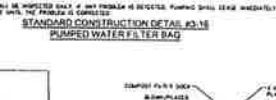
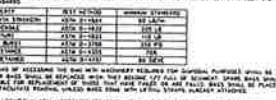
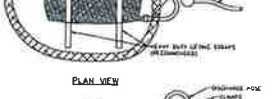
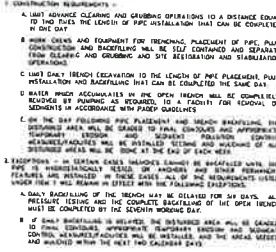
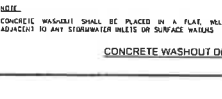
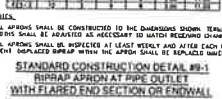
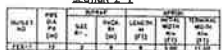
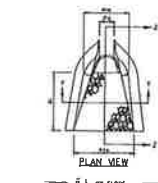
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DESIGNED	NC
CHECKED	ED

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**C4.2**







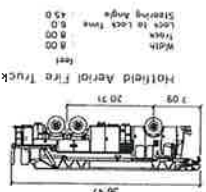












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Sheet: 1 of 1



**HATFIELD WALK**  
TMP # 0900010122005 & 090001006002  
HATFIELD BOROUGH, MONTGOMERY COUNTY, PENNSYLVANIA  
**FIRE TRUCK TURNING TEMPLATE**



REVISIONS

Date	Description



Hattfield Engineering, LLC  
350 E. Butler Ave., Ste. 106  
New Britain, PA 18901  
(215) 596-3330  
www.hattfieldeng.net

# **Updated Traffic Study**

## **10.18.2024**



**October 18, 2024**

TPD# PNPg,00002

[Info@TPDinc.com](mailto:Info@TPDinc.com)

## **PROPOSED HATFIELD HOMES RESIDENTIAL**

Transportation Impact Assessment

*Hatfield Borough, Montgomery County, PA*

### **For Submission To:**

Hatfield Borough

# PROPOSED HATFIELD HOMES RESIDENTIAL TRANSPORTATION IMPACT ASSESSMENT

FOR SUBMISSION TO:

Hatfield Borough, Montgomery County, PA

Prepared For:

**Pennington Property Group**

Ben Golthorp

P.O. Box 35

Chalfont, PA 18914

Phone: (267) 767-0876

October 18, 2024

TPD # PNPG.00002



Prepared By:

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Matthew I. Hammond, P.E.

*Executive Vice President*

Pennsylvania License Number 071037





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Appendix H:	Auxiliary Turn Lane Warrant Analyses

## EXECUTIVE SUMMARY

The purpose of this study is to examine the potential traffic impact associated with the proposed residential development in Hatfield Borough, Montgomery County, PA. Based on this evaluation, the following conclusions were reached:

1. The study area intersections included in this Transportation Impact Assessment (TIA) are listed below:
  - » Main Street (N/S) & Broad Street (E/W).
  - » N. Main Street & Proposed Site Driveway.
2. The project site is currently undeveloped and is located on the eastern (northbound) side of N. Main Street, approximately 200-feet north of the intersection of Main Street (N/S) & Broad Street (E/W). The proposed site will consist of eight (8) townhomes.
3. Access to the site will be served by one (1) full-access driveway to N. Main Street.
4. Traffic volumes for the study area intersections were determined based on a previous turning movement count conducted by TPD at the intersection of Main Street (N/S) & Broad Street (E/W) on Tuesday, March 29, 2022. Furthermore, TPD balanced the traffic volumes along N. Main Street at the proposed site driveway utilizing the count information.
5. A growth factor of 1.0042 (0.21% per year, compounded for two (2) years) was applied to the 2022 traffic volumes to produce 2024 existing condition traffic volumes.
6. The 2024 existing traffic volumes were then grown by applying a growth factor of 1.0042 (0.21% per year, compounded for two (2) years) to produce 2026 base condition traffic volumes.
7. Upon full build-out of the site, the proposed development is expected to generate approximately **4 new trips** during the weekday A.M. peak hour and **5 new trips** during the weekday P.M. peak hour.
8. The new trips generated by the proposed development were then added to the 2026 base condition traffic volumes to develop 2026 projected (build) conditions traffic volumes.
9. Turn lane warrants are not met for a left-turn or right-turn lane on N. Main Street at the Proposed Site Driveway under 2026 projected conditions.
10. Traffic Planning and Design, Inc. (TPD) recommends the following roadway improvements as outlined at the study area intersections:
  - N. Main Street & Proposed Site Driveway**
    - » Provide a stop sign (PennDOT designation R1-1) on the site driveway approach to control exiting traffic.
    - » Provide proper pavement markings and signage at the site driveway to facilitate safe and efficient ingress and egress movements to/from the proposed site.
11. Levels of Service (LOS) for the study area intersections have been summarized in matrix form. **Table I** details the overall intersection LOS for each study area intersection.

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**TABLE I**  
**LEVEL OF SERVICE (SECONDS) SUMMARY**

Intersection	Movement (Existing / Proposed)	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour		
		Existing Condition	Opening Year 2026		Existing Condition	Opening Year 2026	
			Base	Projected		Base	Projected
Main Street (N/S) & Broad Street (E/W)	EB L	B (15.7)	B (15.7)	B (15.8)	B (13.8)	B (13.9)	B (13.9)
	EB TR	C (24.6)	C (24.8)	C (24.9)	C (23.3)	C (23.5)	C (23.5)
	WB L	B (15.8)	B (15.9)	B (16.0)	B (14.3)	B (14.4)	B (14.4)
	WB TR	C (22.4)	C (22.5)	C (22.6)	B (19.6)	B (19.7)	B (19.8)
	NB L	B (17.4)	B (17.4)	B (17.5)	B (17.8)	B (17.8)	B (17.8)
	NB TR	B (18.9)	B (18.8)	B (18.8)	C (33.2)	C (33.4)	C (33.4)
	SB L	B (14.2)	B (14.2)	B (14.2)	B (18.4)	B (18.4)	B (18.4)
	SB TR	D (35.3)	D (35.5)	D (35.9)	C (29.4)	C (29.5)	C (29.5)
	ILOS	C (25.8)	C (26.0)	C (26.1)	C (25.5)	C (25.7)	C (25.7)
N. Main Street & Proposed Site Driveway	-- / WB LR	--	--	B (11.1)	--	--	B (12.8)
	-- / SB LT	--	--	A (0.0)	--	--	A (9.7)
	ILOS	--	--	A (0.0)	--	--	A (0.0)

Base = No-Build scenario

Projected = Build scenario

ILOS = Overall Intersection Level of Service; Unsignalized ILOS calculated in accordance with Figure 5 of Policies and Procedures for Transportation Impact Studies.

12. 95<sup>th</sup> percentile queue lengths for the study area intersection have been summarized in matrix form. **Table II** details the 95<sup>th</sup> percentile queue lengths at the study area intersection.

**TABLE II**  
**95<sup>TH</sup> PERCENTILE QUEUE ANALYSIS (FEET)**

Intersection	Movement (Existing/ Proposed)	Storage (Existing/ Proposed)	Weekday A.M. Peak Hour		Weekday P.M. Peak Hour	
			Opening Year 2026		Opening Year 2026	
			Base	Projected	Base	Projected
Main Street (N/S) & Broad Street (E/W)	EB L	280	<25	<25	25	25
	EB TR	--	205	205	233	235
	WB L	100	28	28	33	33
	WB TR	--	168	168	168	168
	NB L	100	25	25	43	43
	NB TR	--	163	163	328	330
	SB L	--	<25	<25	35	35
	SB TR	265	403	408	288	288
N. Main Street & Proposed Site Driveway	-- / WB LR	--	--	<25	--	<25
	-- / SB LT	--	--	<25	--	<25

Base = No-Build scenario

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## INTRODUCTION

Traffic Planning and Design, Inc. (TPD) has completed a Transportation Impact Assessment (TIA) for the proposed Hatfield Homes residential development in Hatfield Borough, Montgomery County, Pennsylvania. The project site is currently undeveloped and is located on the eastern (northbound) side of N. Main Street, approximately 200-feet north of the intersection of Main Street (N/S) & Broad Street (E/W), as shown in **Figure 1**. As shown in **Figure 2**, the proposed site will consist of eight (8) townhomes. All relevant correspondence pertaining to this project has been included in **Appendix A**.

### Site Access Location

Access to the site will be served by one (1) full-access driveway to N. Main Street.

## EXISTING ROADWAY NETWORK

A field review of the existing roadway system in the study area was conducted. The existing roadway characteristics within the study area are summarized in **Table 1**.

TABLE 1  
ROADWAY CHARACTERISTICS WITHIN STUDY AREA

Roadway	Ownership	Functional Classification/ Roadway Type	Predominant Directional Orientation	Average Daily Traffic <sup>1</sup>	Posted Speed Limit
S. Main Street (S.R. 0463) <sup>2</sup>	State	Minor Arterial	North-South	12,828	25 mph
N. Main Street <sup>3</sup>	Local	Major Collector	North-South	11,050	25 mph
E. Broad Street (S.R. 1003) <sup>4</sup>	State	Minor Arterial	East-West	8,784	25 mph
W. Broad Street (S.R. 0463) <sup>5</sup>	State	Minor Arterial	East-West	7,469	25 mph

<sup>1</sup> = AADT Data from PennDOT Traffic Information Repository (TIRe) website (Accessed October 2024)

<sup>2</sup> = South of Broad Street

<sup>3</sup> = North of Broad Street

<sup>4</sup> = East of Main Street

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### Land Use Context

In Section 1.1 of the Design Manual, Part 2, Contextual Roadway Design, there is guidance pertaining to defining the land use context(s) for a given area. Based upon review of this information, the land uses surrounding the proposed site best fits the Suburban designation, as described below:

**Suburban**, areas with low to medium density (where single-family structures predominate, along with some multi-family and multistory commercial structures); mixed residential neighborhood and commercial clusters (including town centers, commercial corridors, big box commercial, and light industrial); and varied setbacks with some sidewalks and mostly off-street parking.

### Roadway Type

In Section 1.2.1 of the Design Manual, Part 2, Contextual Roadway Design, there is guidance pertaining to defining the transportation context(s) for a given area. Comparing the existing condition roadway characteristics to the various options presented in Table 1.2, the study area roadways best fit the following categories, as described below:

**Minor Arterial**, corridors of regional or community importance connecting centers of activity.

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Intersection	Movement (Existing / Proposed)	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour		
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**Minor Arterial**, corridors of regional or community importance connecting centers of activity.

- » S. Main Street (S.R. 0463) – south of Broad Street.
- » E. Broad Street (S.R. 1003) – east of Main Street.
- » W. Broad Street (S.R. 0463) – west of Main Street.

**Collector**, roadways of lower community importance providing connections between arterials and local roads.

- » N. Main Street – north of Broad Street.

## EXISTING TRAFFIC CONDITIONS

### Intersection Turning Movement Counts

TPD conducted a turning movement count at the intersection of Main Street & Broad Street within the last three (3) years. Traffic counts at the signalized intersection were conducted on 15-minute intervals during the weekday morning (7:00 to 9:00 A.M.) and weekday evening (4:00 to 6:00 P.M.) peak periods. Peak hours and the count date for the signalized intersection are identified in **Table 2**.

TABLE 2  
TRAFFIC COUNT INFORMATION

Intersection	Date of Traffic Counts	Time Period	Intersection Peak Hour <sup>1</sup>
Main Street (N/S) & Broad Street (E/W)	Tuesday, March 29, 2022	Weekday A.M.	7:30 to 8:30 A.M.
		Weekday P.M.	4:30 to 5:30 P.M.

<sup>1</sup> = Peak Hour consists of the four consecutive 15 minute intervals where the highest traffic volumes occur.

In order to determine the through traffic volumes along N. Main Street in the vicinity of the proposed driveway, TPD balanced the traffic volumes along N. Main Street utilizing the above count information. **Table 3** provides a summary of the 2022 existing condition (raw) traffic volumes.

TABLE 3  
EXISTING COUNT INFORMATION

Time Period	2022 Raw Existing Traffic Volumes		
	NB volume	SB volume	Total
Weekday A.M. Peak Hour	277	498	775
Weekday P.M. Peak Hour	492	456	948

**Figure 3** shows the 2022 existing condition (raw) traffic volumes. Growth factors for August 2023 to July 2024 were obtained from the PennDOT Bureau of Planning and Research (BPR). The PennDOT BPR suggests using a background growth trend factor of 1.0042 (0.21% per year, compounded for two (2) years).

It should be noted that PennDOT BPR growth factors have recently been published for August 2024 to July 2025. The PennDOT BPR suggests using a background growth trend factor of 1.0034 (0.17% per year, compounded for two (2) years). As such, the growth factor for August 2023 to July 2024 was utilized to provide a more conservative analysis of background traffic growth. Therefore, TPD applied the 1.0042 growth trend factor to the 2022 raw traffic volumes to produce 2024 existing condition traffic volumes.

The 2024 existing condition traffic volumes for the weekday A.M. and weekday P.M. peak hours are shown in **Figure 4**. The turning movement traffic count is included in **Appendix B**.

## BASE (NO-BUILD) CONDITIONS

### Annual Background Growth

A background growth factor for the roadways in the study area was developed based on growth factors for August 2023 to July 2024 obtained from the PennDOT Bureau of Planning and Research (BPR). The PennDOT BPR suggests using a background growth trend factor of 0.21% per year in Montgomery County for urban non-interstate roadways.

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### Base (No-Build) Conditions Volume Development

The additional traffic volumes due to background growth were added to produce 2026 base (no-build) condition traffic volumes. The 2026 base condition traffic volumes for the weekday A.M. and weekday P.M. peak hours are illustrated in **Figure 5**.

## PROPOSED SITE ACCESS

Access to the site will be served by one (1) full-access driveway to N. Main Street.

### Sight Distance Analysis

A sight distance analysis was prepared for the proposed site driveway. In general, recommended safe sight distances depend upon the posted speed limit and roadway grades. The existing sight distances at the proposed driveways were measured in accordance with PennDOT Publication 282 Highway Occupancy Permit Operations Manual and compared to PennDOT's desirable sight distance standard, which is identified in 67 PA Code Chapter 441.8(h), "Access to and Occupancy of Highways by Driveways and Local Roads." In addition, measured sight distances at the proposed driveways were compared to PennDOT's safe stopping sight distance standard, which is calculated by the following equation:

$$SSSD = 1.47VT + V^2/[30(f \pm g)]$$

SSSD = safe stopping sight distance (acceptable sight distance)

V = Vehicle Speed

T = Perception Reaction Time of Driver (2.5 seconds)

f = Coefficient of Friction for Wet Pavements

g = Percent of Roadway Grade Divided by 100

**Table 4** shows the measured, desirable, acceptable (SSSD), and required sight distances at the site driveway for vehicles entering and exiting the site.

- » S. Main Street (S.R. 0463) – south of Broad Street.
- » E. Broad Street (S.R. 1003) – east of Main Street.
- » W. Broad Street (S.R. 0463) – west of Main Street.

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The 2024 existing condition traffic volumes for the weekday A.M. and weekday P.M. peak hours are shown in **Figure 4**. The turning movement traffic count is included in **Appendix B**.

## BASE (NO-BUILD) CONDITIONS

### Annual Background Growth

A background growth factor for the roadways in the study area was developed based on growth factors for August 2023 to July 2024 obtained from the PennDOT Bureau of Planning and Research (BPR). The PennDOT BPR suggests using a background growth trend factor of 0.21% per year in Montgomery County for urban non-interstate roadways.

It should be noted that PennDOT BPR growth factors have recently been published for August 2024 to July 2025. The PennDOT BPR suggests using a background growth trend factor of 1.0034 (0.17% per year, compounded for two (2) years). As such, the growth factor for August 2023 to July 2024 was utilized to provide a more conservative analysis of background traffic growth. The background growth factor was applied annually to yield overall growth percentages of 0.42% (0.21% per year, compounded over two (2) years) for the 2026 opening year.

### Base (No-Build) Conditions Volume Development

The additional traffic volumes due to background growth were added to produce 2026 base (no-build) condition traffic volumes. The 2026 base condition traffic volumes for the weekday A.M. and weekday P.M. peak hours are illustrated in **Figure 5**.

## PROPOSED SITE ACCESS

Access to the site will be served by one (1) full-access driveway to N. Main Street.

### Sight Distance Analysis

A sight distance analysis was prepared for the proposed site driveway. In general, recommended safe sight distances depend upon the posted speed limit and roadway grades. The existing sight distances at the proposed driveways were measured in accordance with PennDOT Publication 282 Highway Occupancy Permit Operations Manual and compared to PennDOT's desirable sight distance standard, which is identified in 67 PA Code Chapter 441.8(h), "Access to and Occupancy of Highways by Driveways and Local Roads." In addition, measured sight distances at the proposed driveways were compared to PennDOT's safe stopping sight distance standard, which is calculated by the following equation:

$$SSSD = 1.47VT + V^2/[30(f \pm g)]$$

SSSD = safe stopping sight distance (acceptable sight distance)

V = Vehicle Speed

T = Perception Reaction Time of Driver (2.5 seconds)

f = Coefficient of Friction for Wet Pavements

g = Percent of Roadway Grade Divided by 100

**Table 4** shows the measured, desirable, acceptable (SSSD), and required sight distances at the site driveway for vehicles entering and exiting the site.

**TABLE 4**  
**SIGHT DISTANCE ANALYSIS**  
**SITE DRIVEWAY TO N. MAIN STREET**

	Direction	Speed	Grade <sup>1</sup>	Sight Distances (feet)		
				DES	SSSD	EXIST
<b>Exiting Movements</b>	To the left	25 mph	-1%	250	148	<b>385</b>
	To the right	25 mph	+1%	195	145	<b>750+</b>
<b>Entering Left Turns</b>	Approaching same direction	25 mph	+1%	--	145	<b>800+</b>
	Approaching opposite direction	25 mph	-1%	190	148	<b>700+</b>

DES = PennDOT Desirable Sight Distance

<sup>1</sup> = Roadway Grade Approaching Driveway

SSSD = PennDOT Acceptable Sight Distance

EXIST = Existing (measured) Sight

As shown in **Table 4** above, the measured sight distances at the site driveway exceed PennDOT's desirable sight distance requirements.

## TRIP GENERATION

The trip generation rates for the proposed development were obtained from the *Trip Generation Manual*, Eleventh Edition, 2021, an Institute of Transportation Engineers (ITE) Informational Report. The data are categorized by Land Use Codes, with total vehicular trips for a given land use estimated using an independent variable and statistically generated rates or equations.

For the proposed residential development, Land Use Code 215 (Single-Family Attached Housing) from Trip Generation was used to calculate the number of vehicular trips the development will generate during the following time periods: (1) average weekday; (2) weekday A.M. peak hour; and (3) weekday P.M. peak hour. **Table 5** shows the rates/equations and directional percentages for the analyzed time periods.

**TABLE 5**  
**ITE TRIP GENERATION DATA – 8 TOWNHOMES**

Land Use	ITE #	Time Period	Equations/Rates	Entering %	Exiting %
Single-Family Attached Housing	215	Weekday	$T = 7.20*(X)$	50%	50%
		Weekday A.M. Peak Hour	$T = 0.48*(X)$	25%	75%
		Weekday P.M. Peak Hour	$T = 0.57*(X)$	59%	41%

$T$  = number of site-generated vehicular trips;

$X$  = Independent Variable (Dwelling Units)

The calculated trip generation for the proposed development for the opening year is shown in **Table 6**.

**TABLE 6**  
**TRIP GENERATION**

Time Period	Residential Development – 8 Single Family Homes		
	Total	Enter	Exit
Average Weekday	58	29	29
Weekday A.M. Peak Hour	4	1	3
Weekday P.M. Peak Hour	5	3	2

Based on the trip generation analysis summarized in **Table 6**, the proposed development will generate approximately **4 new trips** during the weekday A.M. peak hour and **5 new trips** during the weekday P.M. peak hour.

## TRIP DISTRIBUTION

The distribution of trips generated by the proposed development was based on the local road network, the existing traffic patterns, the proposed use of the site, and the site driveway location. The new trips for the proposed development were distributed to the local roadway network based on the percentages shown in **Table 7**.

TABLE 7  
TRIP DISTRIBUTION PERCENTAGES

Direction - To/From	Assignment (To/From)	Distribution Percentage
North	via N. Main Street	29%
South	via S. Main Street (S.R. 0463)	29%
East	via E. Broad Street (S.R. 1003)	20%
West	via W. Broad Street (S.R. 0463)	22%

The assignment of site-generated trips for the proposed development during the weekday A.M. and weekday P.M. peak hours are shown in **Figure 6**.

## PROJECTED (BUILD) CONDITION TRAFFIC VOLUMES

The site-generated trips for the proposed residential development were added to the 2026 base (no-build) condition traffic volumes to develop 2026 projected (build) condition traffic volumes.

Projected condition traffic volumes for the opening year of 2026 for the weekday A.M. and weekday P.M. peak hours are shown in **Figure 7**. Traffic volume development worksheets are contained in **Appendix C**.

## LEVELS OF SERVICE FOR AN INTERSECTION

For analysis of intersections, level of service is defined in terms of delay, which is a measure of driver discomfort and frustration, fuel consumption, and lost travel time. LOS criteria is stated in terms of control delay per vehicle for a one-hour analysis period. Control delay includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. The criteria are shown in **Table 8**. Delay, as it relates to level of service, is a complex measure and is dependent upon a number of variables. For signalized intersections, these variables include the quality of vehicle progression, the cycle length, the green time ratio, and the volume/capacity ratio for the lane group in question. For unsignalized intersections, delay is related to the availability of gaps in the flow of traffic on the major street and the driver's discretion in selecting an appropriate gap for a particular movement from the minor street (straight across, left or right turn).

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As shown in **Table 4** above, the measured sight distances at the site driveway exceed PennDOT's desirable sight distance requirements.

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$T$  = number of site-generated vehicular trips

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Based on the trip generation analysis summarized in **Table 6**, the proposed development will generate approximately **4 new trips** during the weekday A.M. peak hour and **5 new trips** during the weekday P.M. peak hour.

## TRIP DISTRIBUTION

The distribution of trips generated by the proposed development was based on the local road network, the existing traffic patterns, the proposed use of the site, and the site driveway location. The new trips for the proposed development were distributed to the local roadway network based on the percentages shown in **Table 7**.

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Projected condition traffic volumes for the opening year of 2026 for the weekday A.M. and weekday P.M. peak hours are shown in **Figure 7**. Traffic volume development worksheets are contained in **Appendix C**.

## LEVELS OF SERVICE FOR AN INTERSECTION

For analysis of intersections, level of service is defined in terms of delay, which is a measure of driver discomfort and frustration, fuel consumption, and lost travel time. LOS criteria is stated in terms of control delay per vehicle for a one-hour analysis period. Control delay includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. The criteria are shown in **Table 8**. Delay, as it relates to level of service, is a complex measure and is dependent upon a number of variables. For signalized intersections, these variables include the quality of vehicle progression, the cycle length, the green time ratio, and the volume/capacity ratio for the lane group in question. For unsignalized intersections, delay is related to the availability of gaps in the flow of traffic on the major street and the driver's discretion in selecting an appropriate gap for a particular movement from the minor street (straight across, left or right turn).

**TABLE 8**  
**LEVEL OF SERVICE CRITERIA**  
**UNSIGNALIZED AND SIGNALIZED INTERSECTIONS<sup>1</sup>**

Level of Service	Control Delay Per Vehicle (Seconds)	
	Signalized	Unsignalized
A	< 10	< 10
B	> 10 and < 20	> 10 and < 15
C	> 20 and < 35	> 15 and < 25
D	> 35 and < 55	> 25 and < 35
E	> 55 and < 80	> 35 and < 50
F	> 80 or v/c > 1.0	> 50 or v/c > 1.0

<sup>1</sup> Obtained from Exhibits 19-8 and 20-2 of the Transportation Research Board's Highway Capacity Manual 5<sup>th</sup> Edition

## CAPACITY ANALYSIS METHODOLOGY

Capacity analyses were conducted for the weekday A.M. and weekday P.M. peak hours at the study area intersections. These analyses were conducted according to the methodologies contained in the *Highway Capacity Manual 6<sup>th</sup> Edition* (HCM) using *Synchro 11* software, a Trafficware product.

The following conditions were analyzed, as applicable:

- » Existing conditions;
- » 2026 Base conditions (Build-out year without development);
- » 2026 Projected conditions (Build-out year with development).

The following items should be noted with respect to the capacity analyses:

- » The Pennsylvania default values for two-way stop-controlled intersections in a suburban land use context contained in Chapter 10 of PennDOT's Publication 46 were utilized for the base critical headway and base follow-up headways. The critical and follow-up headway calculation worksheet is included in **Appendix D**.
- » Per PennDOT standards, a peak hour factor of 0.90 was utilized for the intersection of N. Main Street & Proposed Site Driveway.
- » Per PennDOT standards, a heavy vehicle percentage of 2% was utilized for all turning movements to/from the proposed site driveway.

The capacity analysis worksheets are included in **Appendix E**. The PennDOT-approved existing signal plan is included in **Appendix F**.

### PennDOT Standards

The capacity analyses were conducted in accordance with the below noted standards contained in Appendix A - Policies and Procedures for Transportation Impact Studies Related to Highway Occupancy Permits of PennDOT Publication 282, dated February 2024:

- » Page 32 of the Guidelines state that if evaluation of the With Development Horizon Year Scenario to the Without Development Horizon Year Scenario indicates that the overall intersection level of service has dropped, the applicant will be required to mitigate the level of service if the increase in overall intersection delay is greater than 10-seconds. If the overall intersection delay increase is less than or equal to 10-seconds, mitigation of the intersection will not be required. If the intersection level of service meets the level of service requirements, applicants may still be required

to provide mitigation to address critical lanes or approaches. For locations where the level of service of the design horizon year without the development is LOS F and with development, the delay increases more than 10 seconds, the remedies shall provide an estimated delay which will be no worse than the delay for the design year without the development.

- » Page 33 of the Guidelines state that for mitigation scenarios, applicants are expected to mitigate the overall intersection LOS to the original Without Development LOS; the 10-second delay variance is not applied to mitigation scenarios. Applicants may be required to address available storage and queue lengths at critical movements or approaches even if the overall LOS requirements are met.
- » Page 34 of the Guidelines state that if signalization is the preferred alternative for mitigation, overall intersection LOS C in rural areas and LOS D in urban areas is acceptable.
- » Page 35 of the Guidelines states new signalized or unsignalized intersections established to serve as access to the development shall be designed to operate at minimum LOS C for rural areas, and minimum LOS D for urban areas.

## LEVELS OF SERVICE IN THE STUDY AREA

Level of service (LOS) matrices for the study area intersections are shown in **Table 9** for the weekday A.M. and weekday P.M. peak hours.

**TABLE 9**  
**LEVEL OF SERVICE (SECONDS) SUMMARY**

Intersection	Movement (Existing / Proposed)	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour		
		Existing Condition	Opening Year 2026		Existing Condition	Opening Year 2026	
			Base	Projected		Base	Projected
Main Street (N/S) & Broad Street (E/W)	EB L	B (15.7)	B (15.7)	B (15.8)	B (13.8)	B (13.9)	B (13.9)
	EB TR	C (24.6)	C (24.8)	C (24.9)	C (23.3)	C (23.5)	C (23.5)
	WB L	B (15.8)	B (15.9)	B (16.0)	B (14.3)	B (14.4)	B (14.4)
	WB TR	C (22.4)	C (22.5)	C (22.6)	B (19.6)	B (19.7)	B (19.8)
	NB L	B (17.4)	B (17.4)	B (17.5)	B (17.8)	B (17.8)	B (17.8)
	NB TR	B (18.9)	B (18.8)	B (18.8)	C (33.2)	C (33.4)	C (33.4)
	SB L	B (14.2)	B (14.2)	B (14.2)	B (18.4)	B (18.4)	B (18.4)
	SB TR	D (35.3)	D (35.5)	D (35.9)	C (29.4)	C (29.5)	C (29.5)
	ILOS	C (25.8)	C (26.0)	C (26.1)	C (25.5)	C (25.7)	C (25.7)
N. Main Street & Proposed Site Driveway	-- / WB LR	--	--	B (11.1)	--	--	B (12.8)
	-- / SB LT	--	--	A (0.0)	--	--	A (9.7)
	ILOS	--	--	A (0.0)	--	--	A (0.0)

Base = No Build scenario

Projected = Build scenario

ILOS = Overall Intersection Level of Service, Unsignalized, LOS calculated in accordance with Figure 5 of Policies and Procedures for Transportation Impact Studies.

## QUEUE ANALYSIS

Queue analyses were conducted at the study area intersections using *Synchro 11* software. The queue analysis results are summarized in **Table 10** for the analyzed peak hours.

**TABLE 8**  
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**UNSIGNALIZED AND SIGNALIZED INTERSECTIONS<sup>1</sup>**

Level of Service	Control Delay Per Vehicle (Seconds)	
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The following items should be noted with respect to the capacity analyses:

- » The Pennsylvania default values for two-way stop-controlled intersections in a suburban land use context contained in Chapter 10 of PennDOT's Publication 46 were utilized for the base critical headway and base follow-up headways. The critical and follow-up headway calculation worksheet is included in **Appendix D**.
- » Per PennDOT standards, a peak hour factor of 0.90 was utilized for the intersection of N. Main Street & Proposed Site Driveway.
- » Per PennDOT standards, a heavy vehicle percentage of 2% was utilized for all turning movements to/from the proposed site driveway.

The capacity analysis worksheets are included in **Appendix E**. The PennDOT-approved existing signal plan is included in **Appendix F**.

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## LEVELS OF SERVICE IN THE STUDY AREA

Level of service (LOS) matrices for the study area intersections are shown in **Table 9** for the weekday A.M. and weekday P.M. peak hours.

**TABLE 9**  
**LEVEL OF SERVICE (SECONDS) SUMMARY**

Intersection	Movement (Existing / Proposed)	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour		
		Existing Condition	Opening Year 2026		Existing Condition	Opening Year 2026	
			Base	Projected		Base	Projected
Main Street (N/S) & Broad Street (E/W)	EB L	B (15.7)	B (15.7)	B (15.8)	B (13.8)	B (13.9)	B (13.9)
	EB TR	C (24.6)	C (24.8)	C (24.9)	C (23.3)	C (23.5)	C (23.5)
	WB L	B (15.8)	B (15.9)	B (16.0)	B (14.3)	B (14.4)	B (14.4)
	WB TR	C (22.4)	C (22.5)	C (22.6)	B (19.6)	B (19.7)	B (19.8)
	NB L	B (17.4)	B (17.4)	B (17.5)	B (17.8)	B (17.8)	B (17.8)
	NB TR	B (18.9)	B (18.8)	B (18.8)	C (33.2)	C (33.4)	C (33.4)
	SB L	B (14.2)	B (14.2)	B (14.2)	B (18.4)	B (18.4)	B (18.4)
	SB TR	D (35.3)	D (35.5)	D (35.9)	C (29.4)	C (29.5)	C (29.5)
	ILOS	C (25.8)	C (26.0)	C (26.1)	C (25.5)	C (25.7)	C (25.7)
N. Main Street & Proposed Site Driveway	-- / WB LR	--	--	B (11.1)	--	--	B (12.8)
	-- / SB LT	--	--	A (0.0)	--	--	A (9.7)
	ILOS	--	--	A (0.0)	--	--	A (0.0)

Base = No-Build scenario

Projected = Build scenario

ILOS = Overall Intersection Level of Service; Unsignalized ILOS calculated in accordance with Figure 5 of Policies and Procedures for Transportation Impact Studies.

## QUEUE ANALYSIS

Queue analyses were conducted at the study area intersections using *Synchro 11* software. The queue analysis results are summarized in **Table 10** for the analyzed peak hours.

**TABLE 10**  
**95<sup>TH</sup> PERCENTILE QUEUE ANALYSIS (FEET)**

Intersection	Movement (Existing / Proposed)	Storage (Existing/ Proposed)	Weekday A.M. Peak Hour		Weekday P.M. Peak Hour	
			Opening Year 2026		Opening Year 2026	
			Base	Projected	Base	Projected
Main Street (N/S) & Broad Street (E/W)	EB L	280	<25	<25	25	25
	EB TR	--	205	205	233	235
	WB L	100	28	28	33	33
	WB TR	--	168	168	168	168
	NB L	100	25	25	43	43
	NB TR	--	163	163	328	330
	SB L	--	<25	<25	35	35
	SB TR	265	403	408	288	288
N. Main Street & Proposed Site Driveway	-- / WB LR	--	--	<25	--	<25
	-- / SB LT	--	--	<25	--	<25

Base = No-Build scenario

Projected = Build scenario

Queue analysis worksheets are included with the capacity analysis worksheets provided in **Appendix E**.

## GAP ANALYSIS

As requested by Hatfield Borough, TPD performed a Gap Study at the proposed site driveway location on N. Main Street. The number and duration of gaps available for these movements were documented. The duration of gaps in traffic directly relates to the capacity (number of vehicles) that can make the identified movements. In order for a vehicle to make the identified movements at these locations, a large enough gap in traffic must be present for those movements to occur. TPD determined the necessary Critical Gap and Follow-Up Gap needed for the evaluated movements based on *HCM 6<sup>th</sup> Edition* Methodology and the PA Default Value Adjustments. Based on this, the following peak hours and gaps were utilized:

### Minor Left-Turn from Proposed Full-Access Driveway (Westbound) to Southbound N. Main Street:

- Weekday A.M.: 7:30-8:30 A.M. - Critical Gap of 6.4 seconds and Follow-Up Gap of 3.0 seconds.
- Weekday P.M.: 4:30-5:30 P.M. - Critical Gap of 6.4 seconds and Follow-Up Gap of 3.0 seconds.

The number and time duration of gaps counted during the weekday A.M. and weekday P.M. peak hours were compared to the standards outlined above, in order to determine the total number of vehicles that can be served during the peak hours.

TPD compared the total capacity calculated based on the field gap counts to the projected vehicle demand. **Table 11** shows this comparison.

**TABLE 11  
GAP ANALYSIS**

Intersection	Movement	Peak Hour	Available Capacity for Turns	Projected 2026 Turning Vehicle Demand
N. Main Street & Full-Access Driveway	WB L	Weekday A.M.	375	2
		Weekday P.M.	312	1

As shown in **Table 11**, the available capacity for minor left-turn vehicles (gaps) from the proposed Full-Access Driveway (westbound) to southbound N. Main Street exceeds the anticipated number of minor left-turn vehicles. Therefore, sufficient capacity is available for left turns onto southbound N. Main Street from the proposed Full-Access Driveway (westbound) under future conditions.

Gap analysis worksheets are contained in **Appendix G**.

## AUXILIARY TURN LANE ANALYSIS

### Methodology

TPD evaluated auxiliary turn lane warrants at the site access intersections. The warrant analysis methodology contained within Chapter 11 of PennDOT's *Publication 46*, Section 11.17 and Strike-Off Letter 470-08-07 was utilized for this evaluation.

### Findings

**Table 12** summarizes the results of the auxiliary turn lane analysis at the site access intersection.

**TABLE 12  
AUXILIARY TURN LANE ANALYSIS SUMMARY**

Intersection	Auxiliary Lane	Warrant Satisfied?		Required Lane Length	Proposed Lane Length
		A.M.	P.M.		
N. Main Street & Proposed Site Driveway	SB Left-Turn Lane	No	No	--	--
	NB Right-Turn Lane	No	No	--	--

As shown in **Table 12**, based on the criteria outlined above, under 2026 projected conditions, left-turn and right-turn lane warrants are not satisfied on N. Main Street at the proposed site driveway.

Auxiliary turn lane warrant analysis worksheets are included in **Appendix H**.

## RECOMMENDATIONS AND CONCLUSIONS

The recommendations and conclusions of this Transportation Impact Assessment are identified in the Executive Summary.

**TABLE 10**  
**95<sup>TH</sup> PERCENTILE QUEUE ANALYSIS (FEET)**

Intersection	Movement (Existing / Proposed)	Storage (Existing/ Proposed)	Weekday A.M. Peak Hour		Weekday P.M. Peak Hour	
			Opening Year 2026		Opening Year 2026	
			Base	Projected	Base	Projected
Main Street (N/S) & Broad Street (E/W)	EB L	280	<25	<25	25	25
	EB TR	--	205	205	233	235
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	SB TR	265	403	408	288	288
N. Main Street & Proposed Site Driveway	-- / WB LR	--	--	<25	--	<25
	-- / SB LT	--	--	<25	--	<25

*Base = No-Build scenario*

*Projected = Build scenario*

Queue analysis worksheets are included with the capacity analysis worksheets provided in **Appendix E**.

## GAP ANALYSIS

As requested by Hatfield Borough, TPD performed a Gap Study at the proposed site driveway location on N. Main Street. The number and duration of gaps available for these movements were documented. The duration of gaps in traffic directly relates to the capacity (number of vehicles) that can make the identified movements. In order for a vehicle to make the identified movements at these locations, a large enough gap in traffic must be present for those movements to occur. TPD determined the necessary Critical Gap and Follow-Up Gap needed for the evaluated movements based on *HCM 6<sup>th</sup> Edition* Methodology and the PA Default Value Adjustments. Based on this, the following peak hours and gaps were utilized:

### Minor Left-Turn from Proposed Full-Access Driveway (Westbound) to Southbound N. Main Street:

- Weekday A.M.: 7:30-8:30 A.M. - Critical Gap of 6.4 seconds and Follow-Up Gap of 3.0 seconds.
- Weekday P.M.: 4:30-5:30 P.M. - Critical Gap of 6.4 seconds and Follow-Up Gap of 3.0 seconds.

The number and time duration of gaps counted during the weekday A.M. and weekday P.M. peak hours were compared to the standards outlined above, in order to determine the total number of vehicles that can be served during the peak hours.

TPD compared the total capacity calculated based on the field gap counts to the projected vehicle demand. **Table 11** shows this comparison.



**TABLE 11  
GAP ANALYSIS**

Intersection	Movement	Peak Hour	Available Capacity for Turns	Projected 2026 Turning Vehicle Demand
N. Main Street & Full-Access Driveway	WB L	Weekday A.M.	375	2
		Weekday P.M.	312	1

As shown in **Table 11**, the available capacity for minor left-turn vehicles (gaps) from the proposed Full-Access Driveway (westbound) to southbound N. Main Street exceeds the anticipated number of minor left-turn vehicles. Therefore, sufficient capacity is available for left turns onto southbound N. Main Street from the proposed Full-Access Driveway (westbound) under future conditions.

Gap analysis worksheets are contained in **Appendix G**.

## AUXILIARY TURN LANE ANALYSIS

### Methodology

TPD evaluated auxiliary turn lane warrants at the site access intersections. The warrant analysis methodology contained within Chapter 11 of PennDOT's *Publication 46*, Section 11.17 and Strike-Off Letter 470-08-07 was utilized for this evaluation.

### Findings

**Table 12** summarizes the results of the auxiliary turn lane analysis at the site access intersection.

**TABLE 12  
AUXILIARY TURN LANE ANALYSIS SUMMARY**

Intersection	Auxiliary Lane	Warrant Satisfied?		Required Lane Length	Proposed Lane Length
		A.M.	P.M.		
N. Main Street & Proposed Site Driveway	SB Left-Turn Lane	No	No	--	--
	NB Right-Turn Lane	No	No	--	--

As shown in **Table 12**, based on the criteria outlined above, under 2026 projected conditions, left-turn and right-turn lane warrants are not satisfied on N. Main Street at the proposed site driveway.

Auxiliary turn lane warrant analysis worksheets are included in **Appendix H**.

## RECOMMENDATIONS AND CONCLUSIONS

The recommendations and conclusions of this Transportation Impact Assessment are identified in the Executive Summary.













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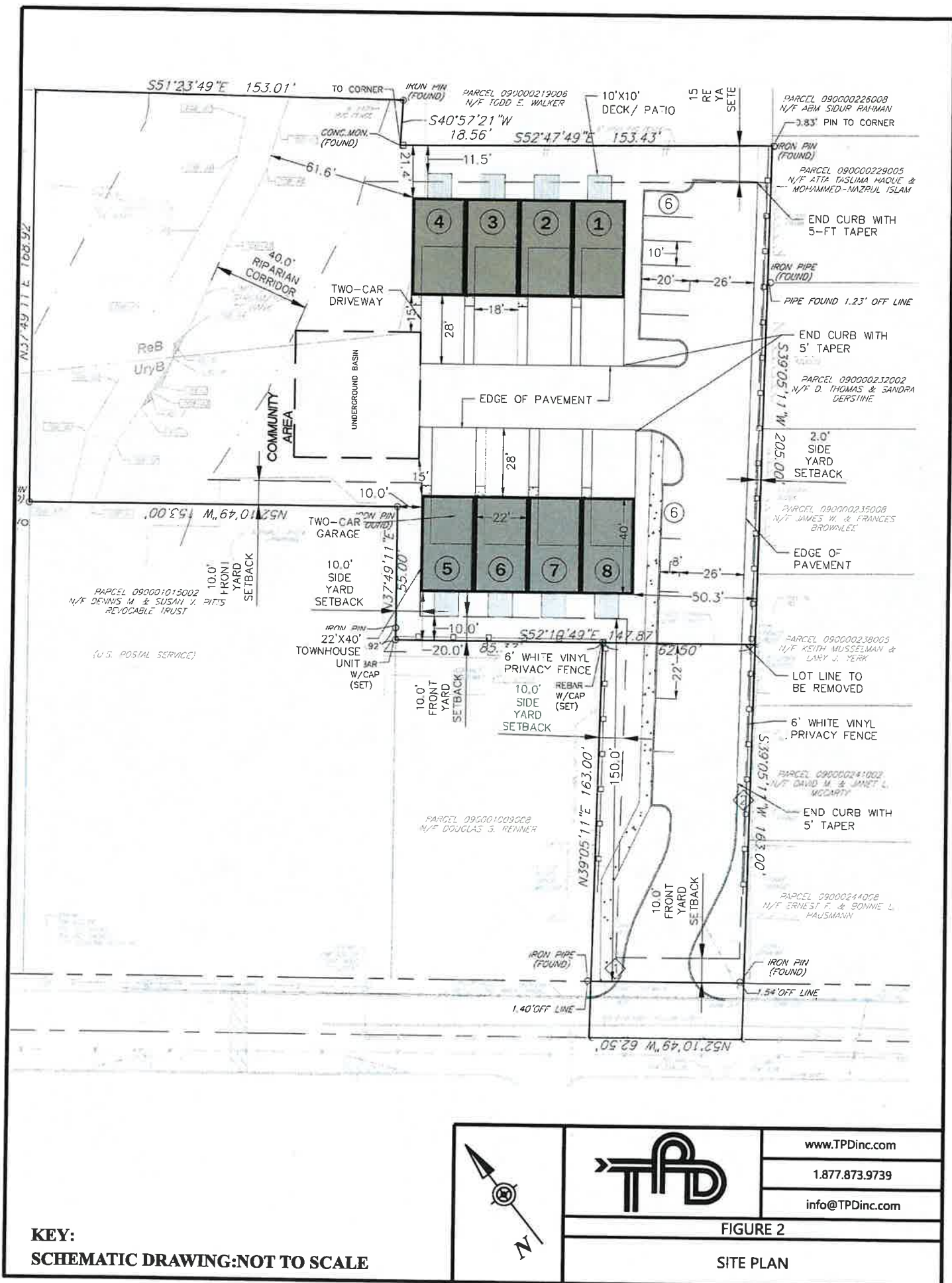
1.877.873.9739

[info@TPDinc.com](mailto:info@TPDinc.com)

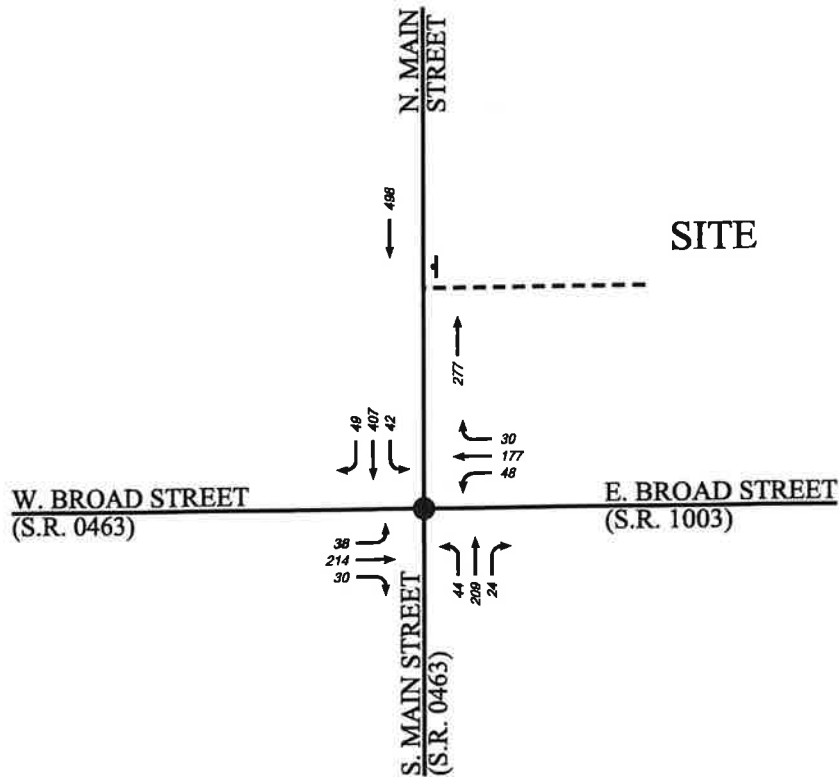
FIGURE 1

SITE LOCATION

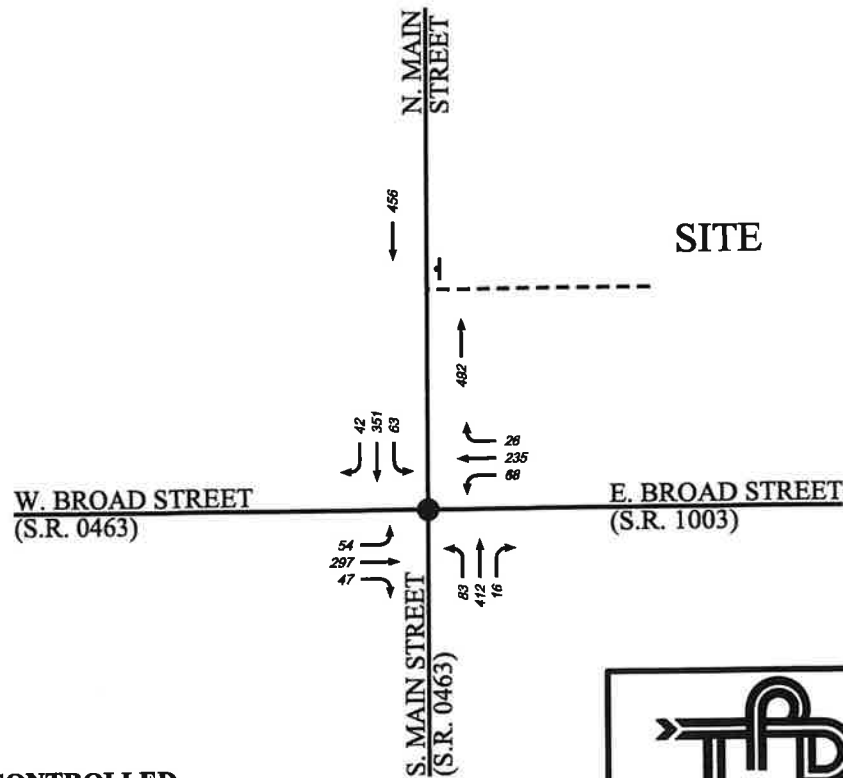




# WEEKDAY A.M. PEAK HOUR



# WEEKDAY P.M. PEAK HOUR



## KEY:

- STOP CONTROLLED
  - SIGNALIZED INTERSECTION
  - PROPOSED DRIVEWAY
- SCHEMATIC DRAWING: NOT TO SCALE**



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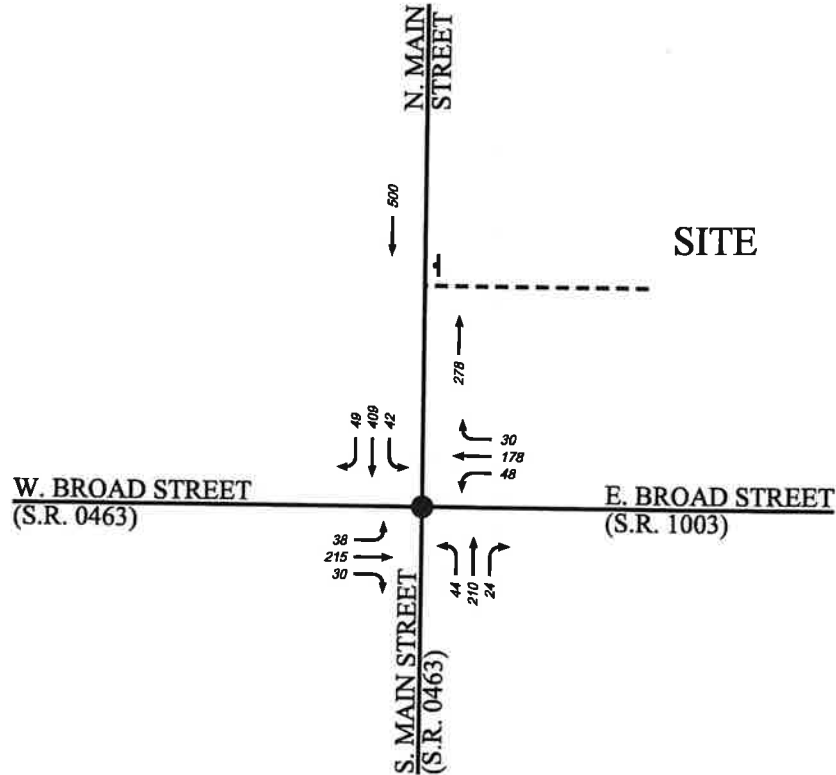
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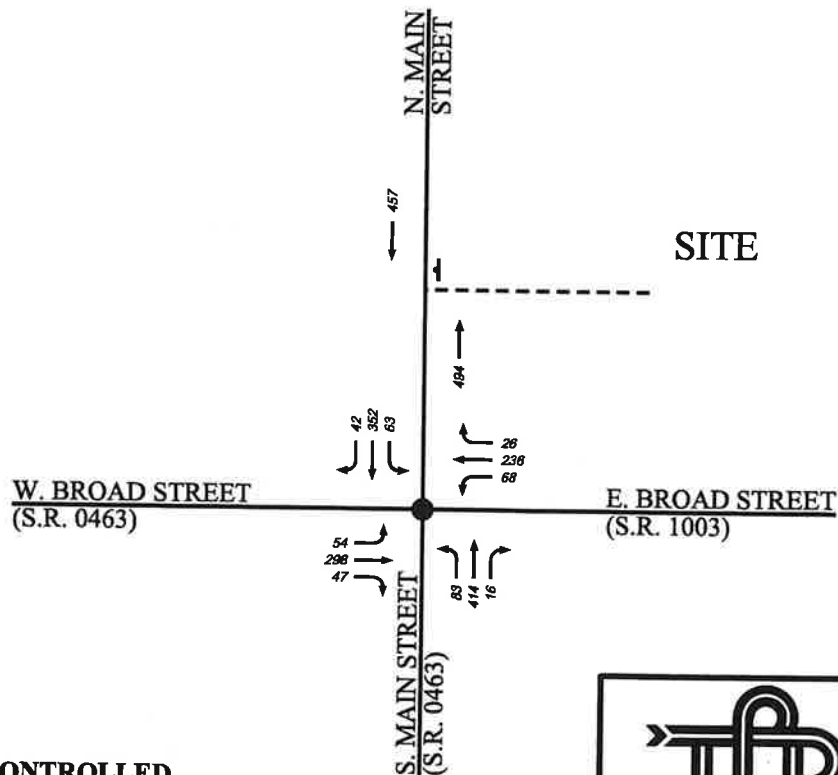
**FIGURE 3**

**2022 EXISTING CONDITIONS (RAW VOLUMES)  
PEAK HOUR  
TRAFFIC VOLUMES**

# WEEKDAY A.M. PEAK HOUR



# WEEKDAY P.M. PEAK HOUR



## KEY:

- STOP CONTROLLED
  - SIGNALIZED INTERSECTION
  - PROPOSED DRIVEWAY
- SCHEMATIC DRAWING: NOT TO SCALE



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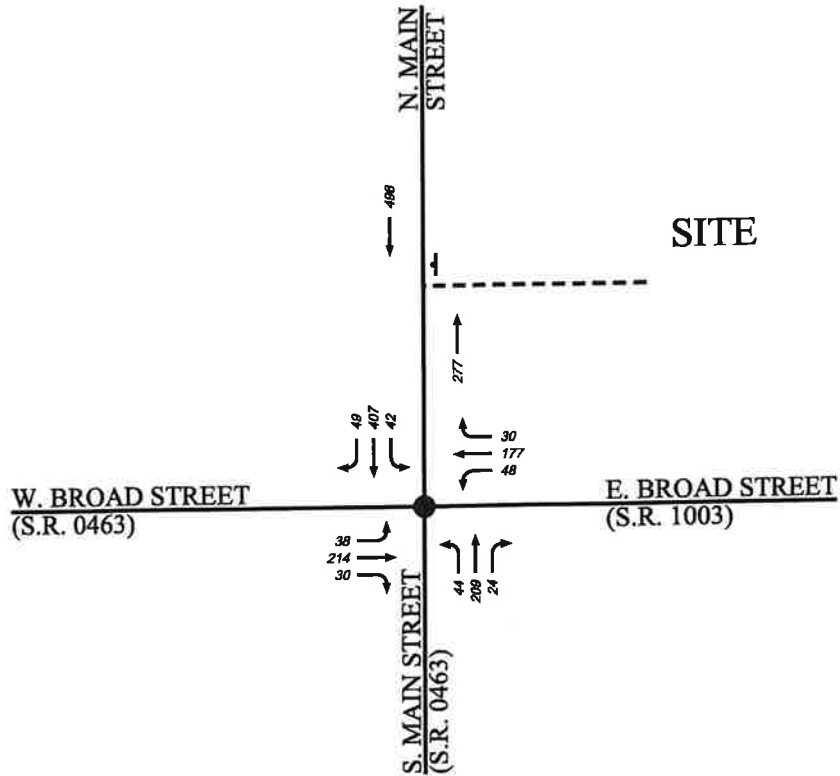
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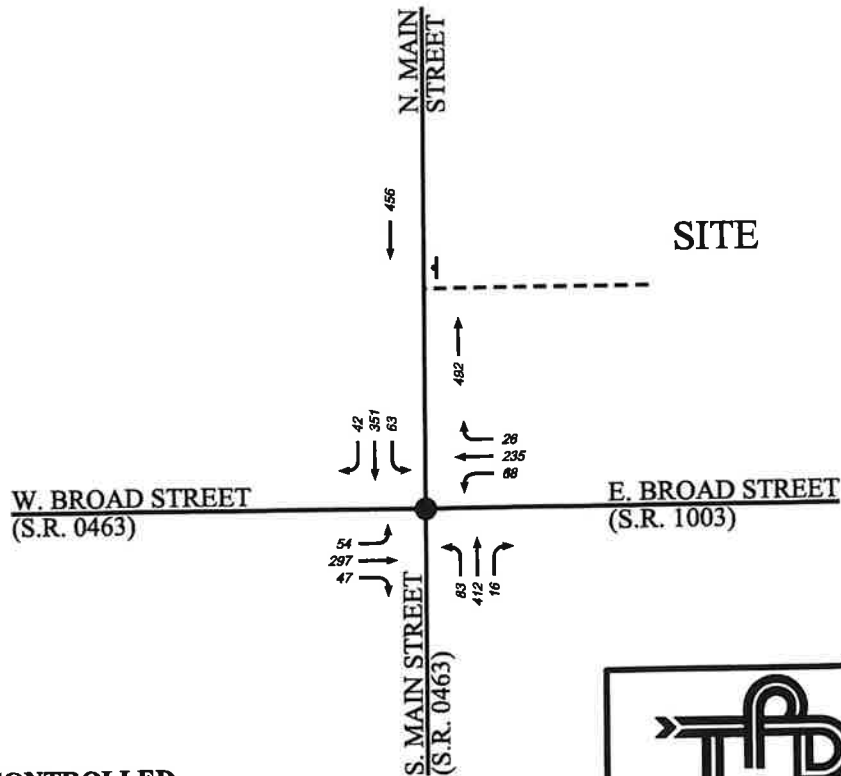
FIGURE 4

2024 EXISTING CONDITIONS  
PEAK HOUR  
TRAFFIC VOLUMES

# WEEKDAY A.M. PEAK HOUR



# WEEKDAY P.M. PEAK HOUR



## KEY:

- STOP CONTROLLED
  - SIGNALIZED INTERSECTION
  - PROPOSED DRIVEWAY
- SCHEMATIC DRAWING: NOT TO SCALE**



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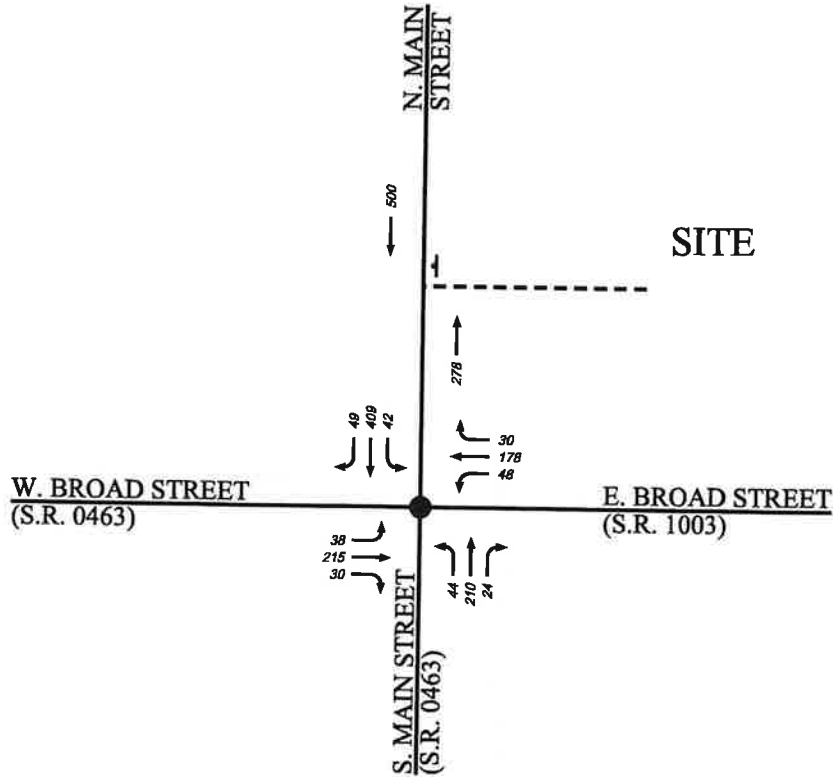
[info@TPDinc.com](mailto:info@TPDinc.com)

**FIGURE 3**

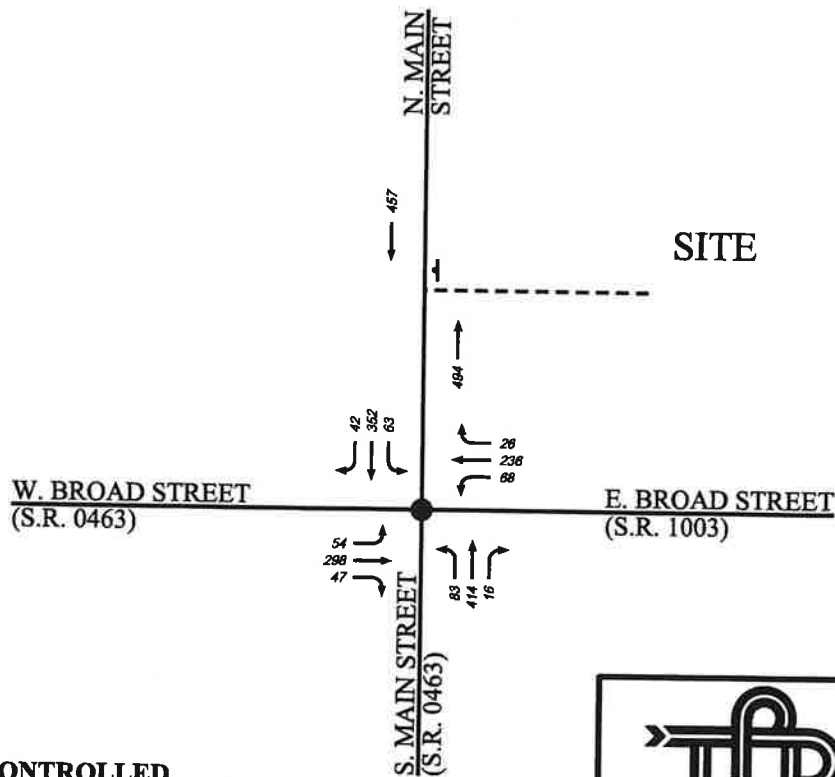
**2022 EXISTING CONDITIONS (RAW VOLUMES)  
PEAK HOUR  
TRAFFIC VOLUMES**



# WEEKDAY A.M. PEAK HOUR



# WEEKDAY P.M. PEAK HOUR



## KEY:



STOP CONTROLLED



SIGNALIZED INTERSECTION

----- PROPOSED DRIVEWAY

SCHEMATIC DRAWING: NOT TO SCALE



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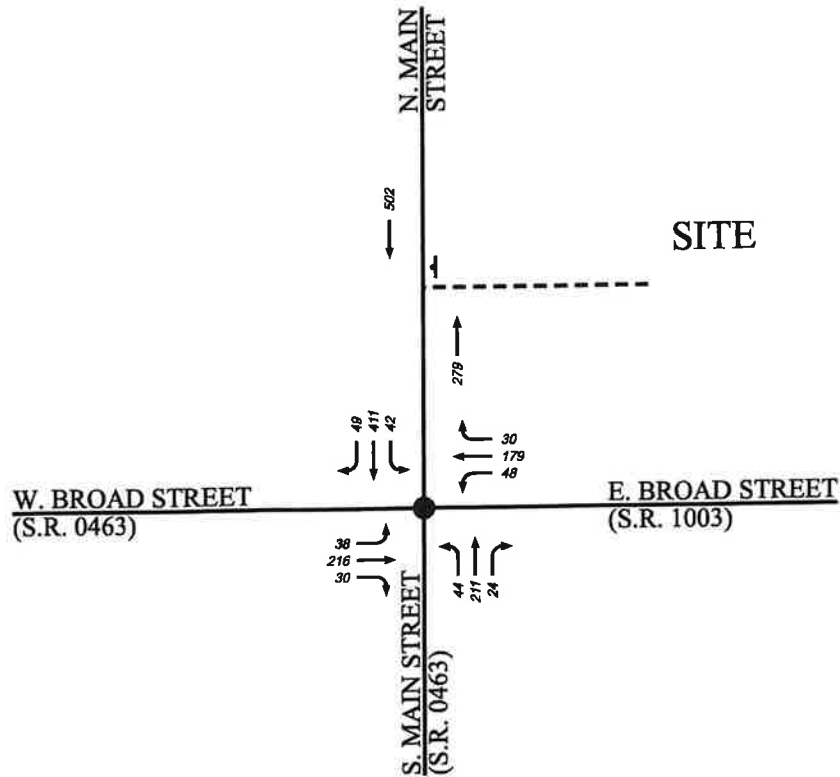
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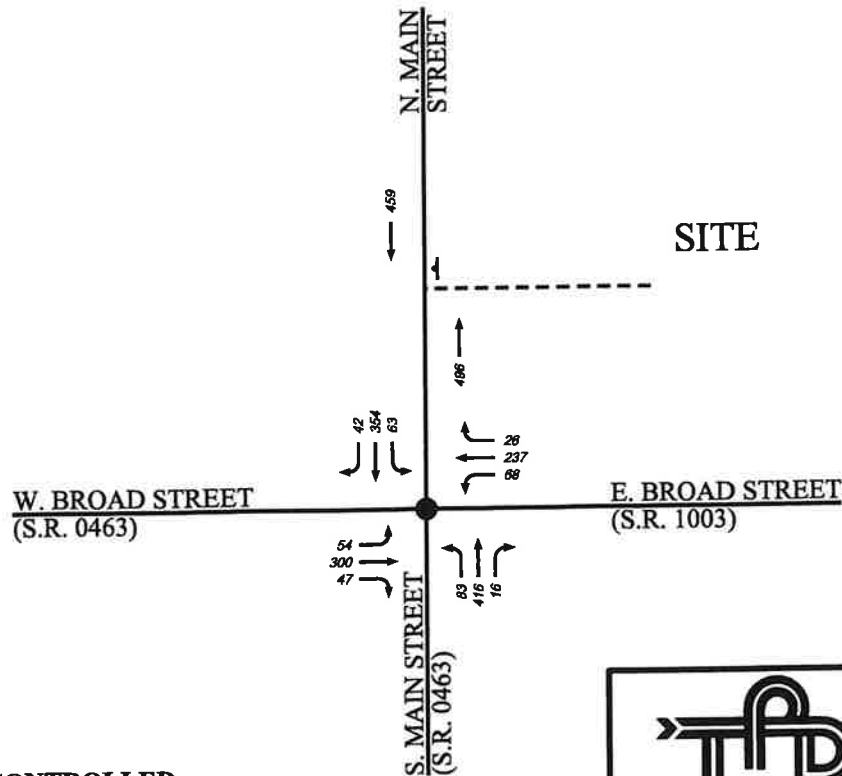
FIGURE 4

2024 EXISTING CONDITIONS  
PEAK HOUR  
TRAFFIC VOLUMES

# WEEKDAY A.M. PEAK HOUR



# WEEKDAY P.M. PEAK HOUR



## KEY:

- STOP CONTROLLED
  - SIGNALIZED INTERSECTION
  - PROPOSED DRIVEWAY
- SCHEMATIC DRAWING: NOT TO SCALE**



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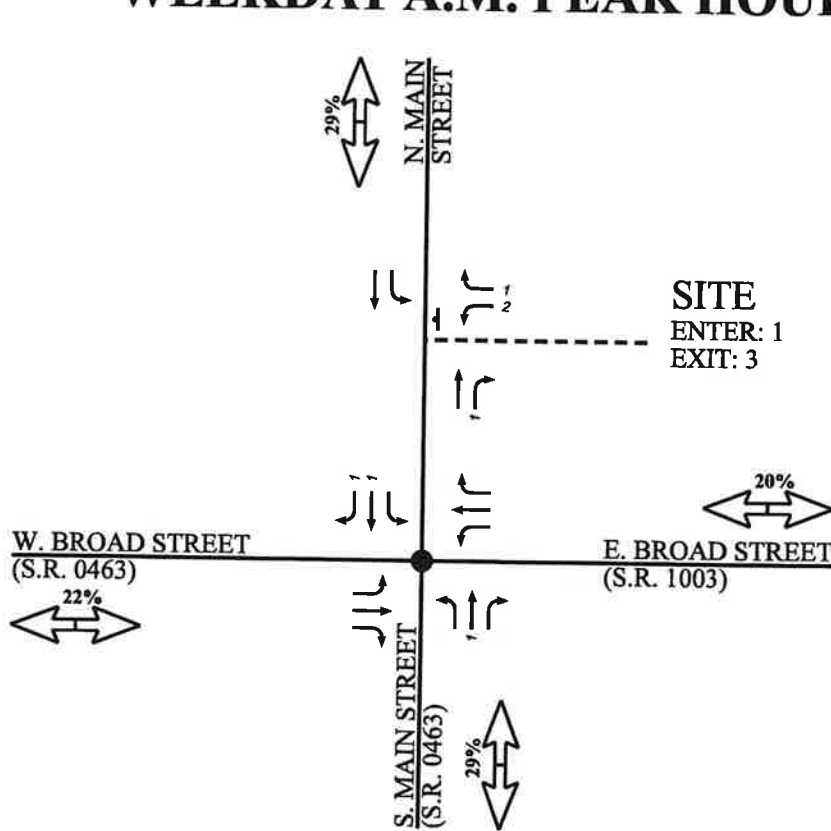
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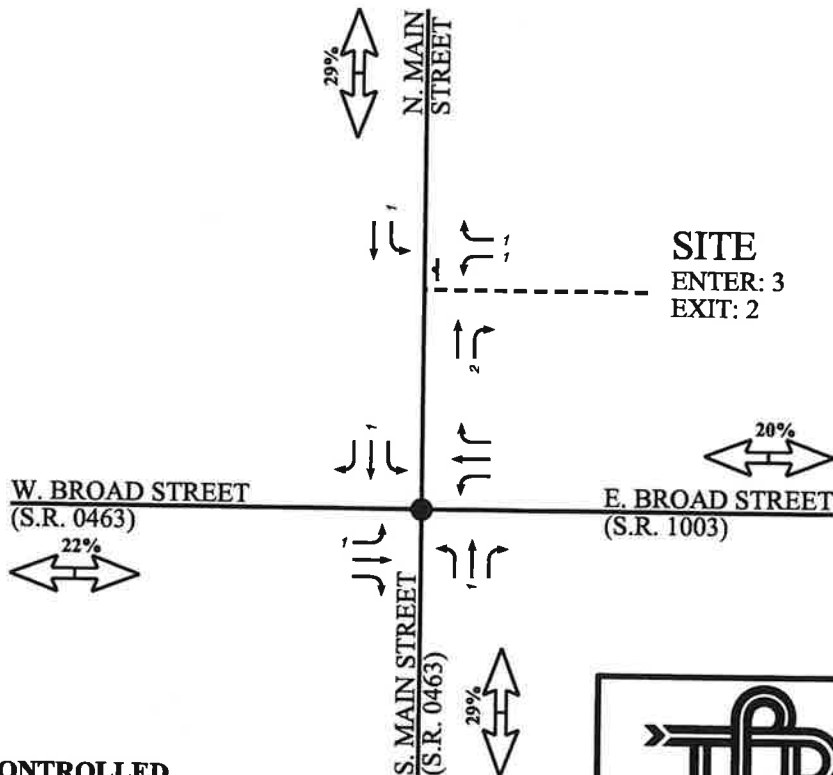
**FIGURE 5**

**2026 BASE (NO-BUILD) CONDITIONS  
PEAK HOUR  
TRAFFIC VOLUMES**

## WEEKDAY A.M. PEAK HOUR



## WEEKDAY P.M. PEAK HOUR



### KEY:

- STOP CONTROLLED
- SIGNALIZED INTERSECTION
- PROPOSED DRIVEWAY

SCHEMATIC DRAWING: NOT TO SCALE



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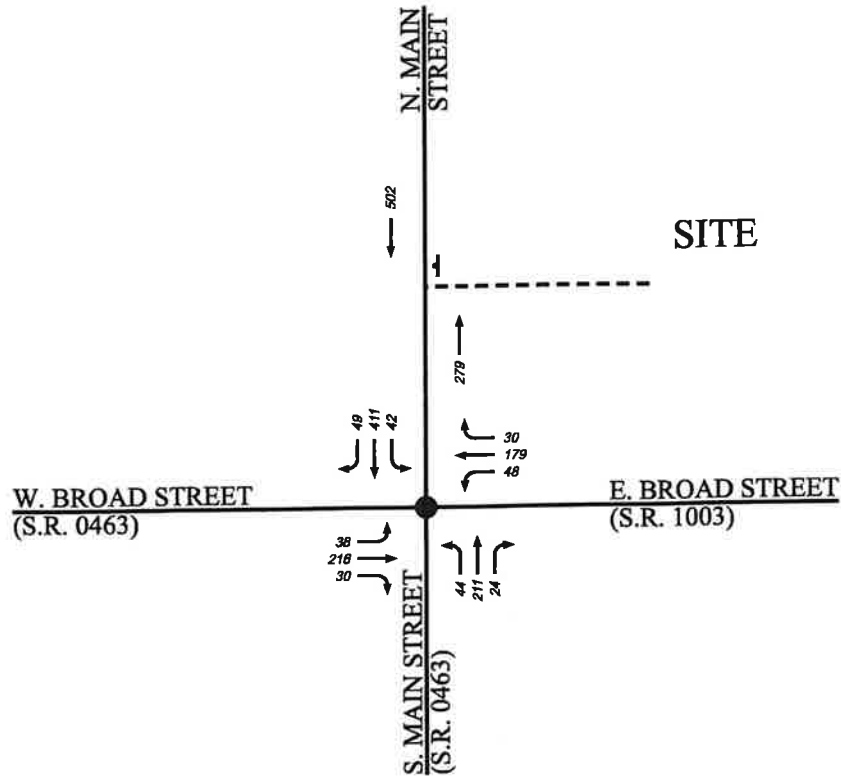
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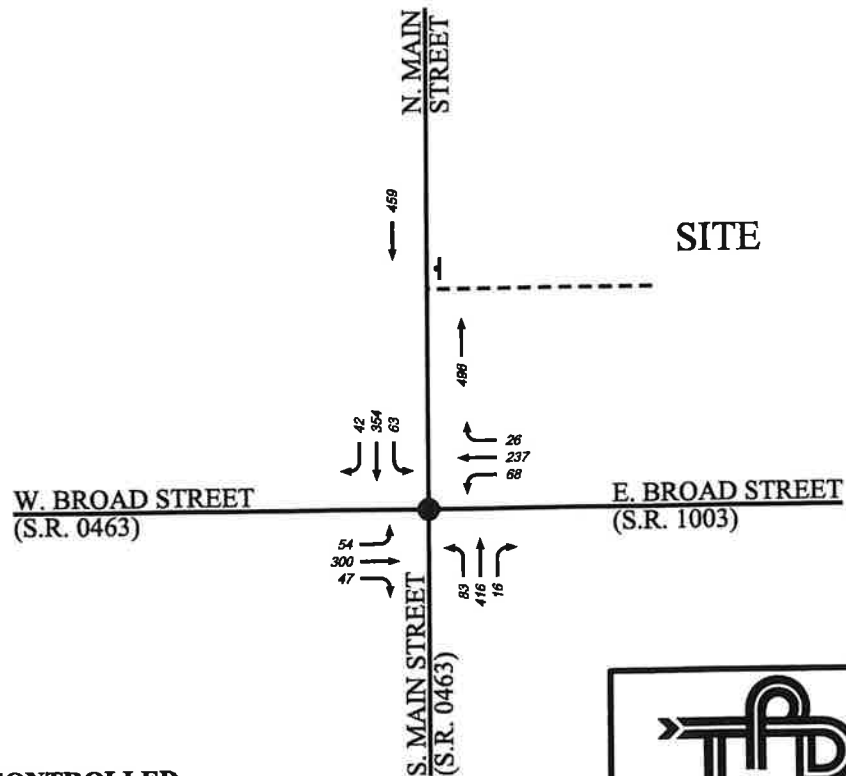
FIGURE 6

TRIP DISTRIBUTION  
PEAK HOUR  
SITE TRIPS

# WEEKDAY A.M. PEAK HOUR



# WEEKDAY P.M. PEAK HOUR



## KEY:

- STOP CONTROLLED
  - SIGNALIZED INTERSECTION
  - PROPOSED DRIVEWAY
- SCHEMATIC DRAWING: NOT TO SCALE**



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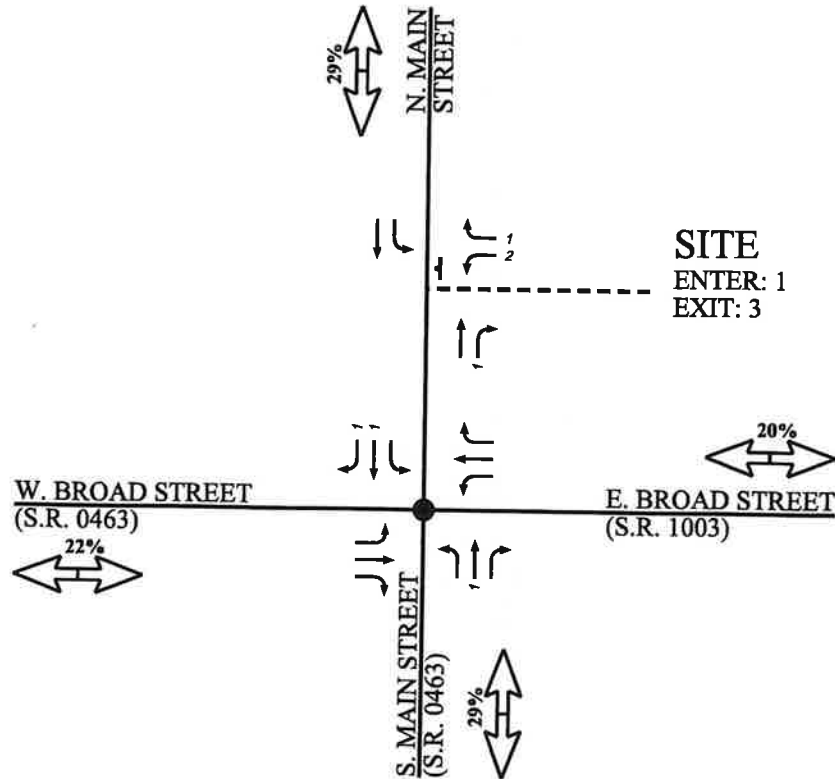
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**FIGURE 5**

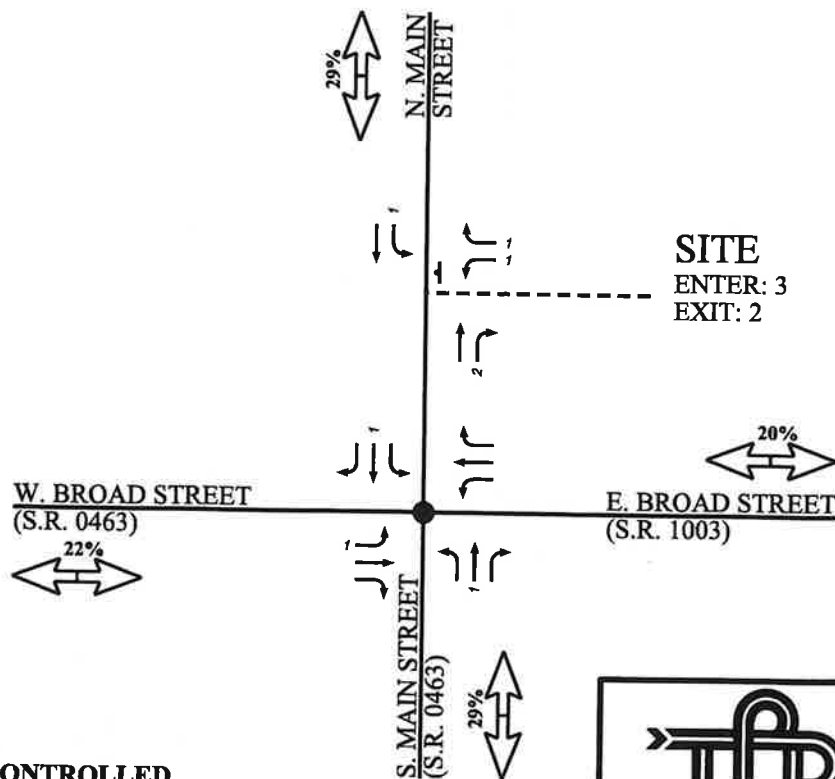
**2026 BASE (NO-BUILD) CONDITIONS  
PEAK HOUR  
TRAFFIC VOLUMES**



## WEEKDAY A.M. PEAK HOUR




## WEEKDAY P.M. PEAK HOUR

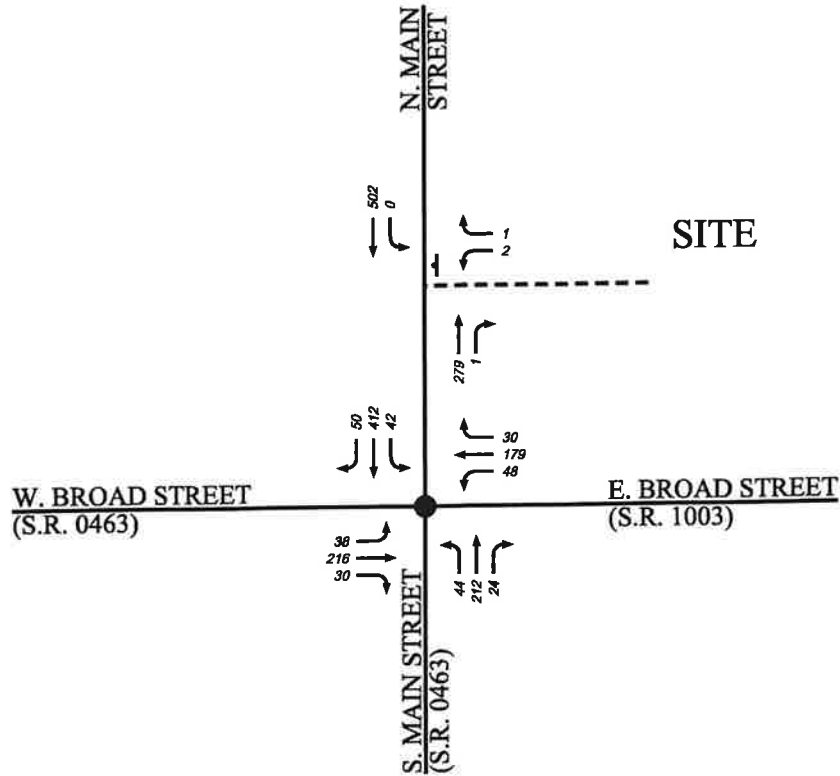


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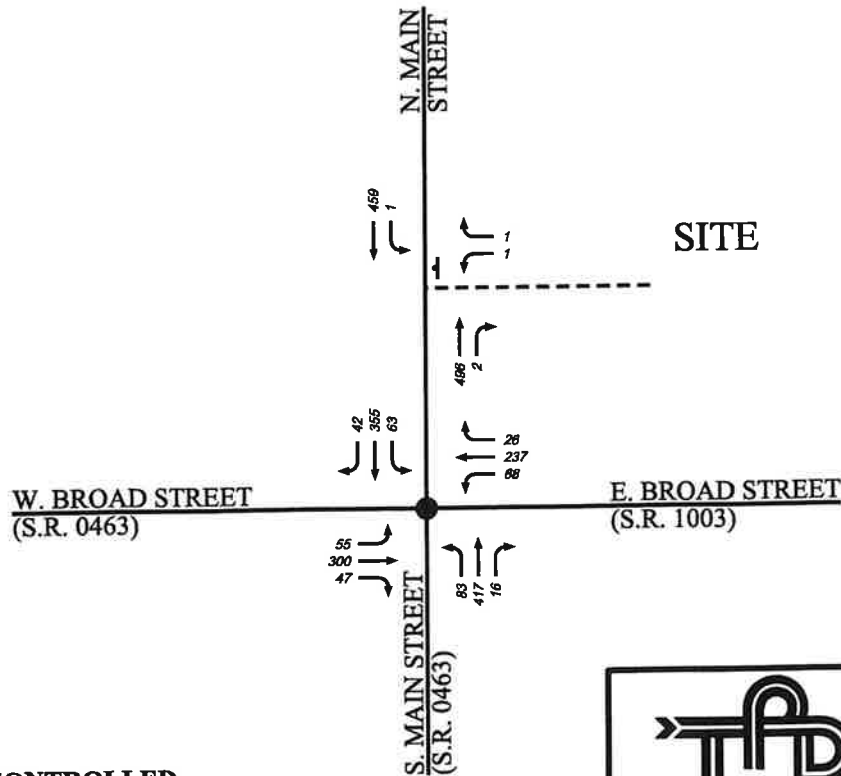
- STOP CONTROLLED
  - SIGNALIZED INTERSECTION
  - PROPOSED DRIVEWAY
- SCHEMATIC DRAWING: NOT TO SCALE

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	1.877.873.9739
	<a href="mailto:info@TPDinc.com">info@TPDinc.com</a>
<b>FIGURE 6</b>	
<b>TRIP DISTRIBUTION PEAK HOUR SITE TRIPS</b>	

# WEEKDAY A.M. PEAK HOUR



# WEEKDAY P.M. PEAK HOUR



## KEY:

- STOP CONTROLLED
  - SIGNALIZED INTERSECTION
  - PROPOSED DRIVEWAY
- SCHEMATIC DRAWING: NOT TO SCALE**



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**FIGURE 7**

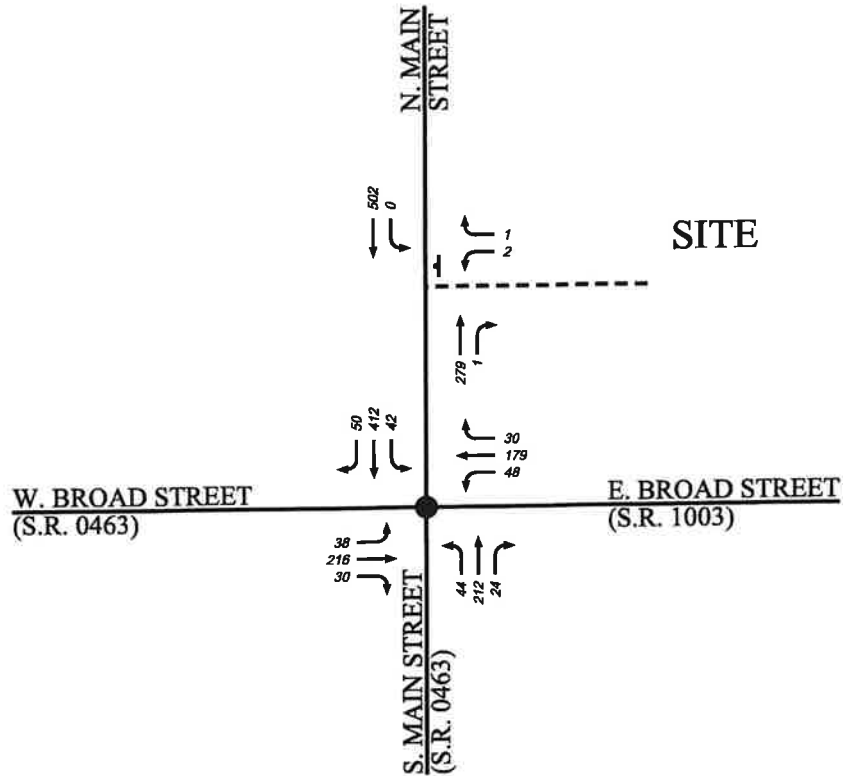
**2026 PROJECTED (BUILD) CONDITIONS  
PEAK HOUR  
TRAFFIC VOLUMES**

## **APPENDIX A:**

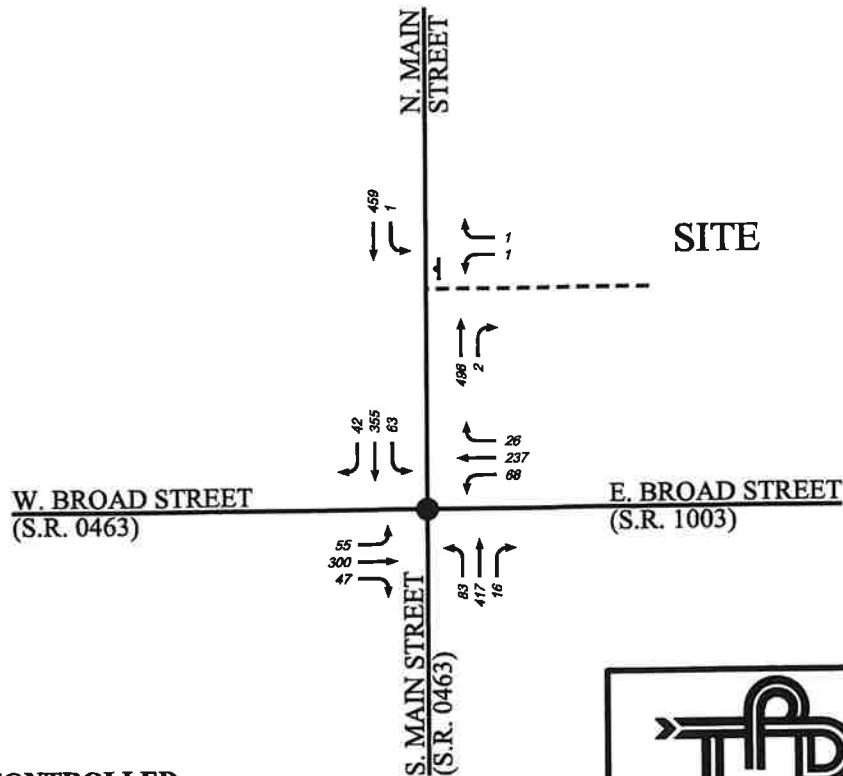
### **Project Correspondence**



# WEEKDAY A.M. PEAK HOUR



# WEEKDAY P.M. PEAK HOUR



## KEY:

- STOP CONTROLLED
  - SIGNALIZED INTERSECTION
  - PROPOSED DRIVEWAY
- SCHEMATIC DRAWING: NOT TO SCALE**



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**FIGURE 7**

**2026 PROJECTED (BUILD) CONDITIONS  
PEAK HOUR  
TRAFFIC VOLUMES**



## **APPENDIX A:**

### **Project Correspondence**





September 20, 2024

Ms. Jaime E. Snyder  
Borough of Hatfield  
401 South Main Street  
P.O. Box 190  
Hatfield, PA 19440

**RE: Traffic Engineering Review #3**  
Proposed Residential Development – Hatfield Walk  
23 North Main Street  
Hatfield, PA 19440  
Project No. 311304-01-001

Dear Jaime:

Per your request, Bowman Consulting Group (Bowman) has completed a traffic engineering review of the proposed residential development to be located at 23 North Main Street in the Borough of Hatfield, Montgomery County, PA. It is our understanding that the proposed development will consist of the development of eight (8) townhomes. Access to the proposed development will be provided via a full-movement driveway along North Main Street.

The following documents were reviewed and/or referenced in preparation of our comments:

- Site Access Study – Proposed Hatfield Homes Residential, prepared by Traffic Planning and Design, Inc., dated August 21, 2024.
- Preliminary/Final Land Development Plans – Hatfield Walk, prepared by Homes Cunningham, LLC, dated August 7, 2024.

Based on our review of the submitted documents noted above, Bowman offers the following comments for consideration by the Borough and action by the applicant.

#### **General**

1. A response letter must be provided with the resubmission detailing how each comment below has been addressed, and where each can be found in the resubmission materials (i.e., page number(s)) to assist in the re-review process. Additional comments may follow upon review of any resubmitted and more detailed plans during the land development process.

#### **Site Access Study**

2. The site access study should be revised to include a traffic analysis of the intersection of intersection of Main Street and Broad Street. The intersection currently experiences delay during the commuter peak hours and the queuing along Main Street may impact the operation of the site driveway during the commuter peak hours. A gap study along North Main Street at the proposed site driveway location should be conducted if necessary to confirm that there are an adequate number of gaps in the North Main Street traffic stream for vehicles to safely enter and exit the site.

425 Commerce Drive Suite 200, Fort Washington, PA 19034  
P: 215.283.9444  
**bowman.com**

3. The site access study should be updated to include capacity/levels-of-service analysis for the intersection of North Main Street and the site driveway for the weekday morning and weekday afternoon peak hours under 2029 future with-development conditions.
4. The study should be revised so that the entering and exiting site trips for the weekday morning peak hour shown in Table 6 and on Figure 6 match the distribution percentages shown in Table 5. In addition, the turn lane warrant analysis shown in Appendix C should be revised accordingly.

**Preliminary/Final Land Development Plans**

1. The pavement markings along Main Street at the site access should be reviewed. Modifications to the pavement markings may be required to properly manage the movements to \from the site, the left turn lane at the signalized intersection, and the existing pedestrian crossing and parking at the post office. It should be noted that the Borough has identified traffic calming\pedestrian improvements along North Main Street at the existing pedestrian crossing for the post office.
2. Sight distance measurements must be shown on the plans for the intersection of North Main Street and the site driveway as required by **Section 22-405.1** of the **Subdivision and Land Development Ordinance**.
3. Turning templates should be provided with future plan submissions demonstrating the ability of a trash truck, emergency vehicle, and the largest expected delivery truck to maneuver into and out of the driveway along North Main Street and entirely through the site. The Borough Fire Marshal should review the emergency vehicle turning template for accessibility and circulation needs of emergency apparatus.
4. A "Stop" sign and stop bar should be shown on the plans on the site driveway approach to North Main Street. "No Parking" signs should be shown on the plans along the eastern side of the site driveway from North Main Street to the northern end of the site driveway.
5. ADA ramps must be provided at the driveway along Main Street for the existing sidewalk. An ADA ramp should also be shown on the plans on the northern end of the sidewalk located on the western side of the site driveway at its intersection with the drive aisle leading to/from the townhomes.
6. A back-up area should be provided on the western end of the drive aisle leading to/from the townhomes so that vehicles backing out of the driveways for lots 4 and 5 have adequate space to complete this maneuver.



September 20, 2024

Ms. Jaime E. Snyder  
Borough of Hatfield  
401 South Main Street  
P.O. Box 190  
Hatfield, PA 19440

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Hatfield, PA 19440  
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P: 215.283.9444  
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We trust that this review letter responds to your request, and satisfactorily addresses the traffic issues related to the proposed development at this time. If the Borough has any questions, or requires further clarification, please contact me.

Sincerely,



Anton Kuhner, P.E.  
Senior Project Manager

AKK/BMJ

cc: Chad Camburn, P.E., Bursich Associates, Inc  
Catherine M. Harper, Borough Solicitor  
Bob Heil, Borough of Hatfield  
Rob Cunningham, P.E., Holmes Cunningham, LLC (Applicant's Engineer)  
Matt Hammond, P.E., Traffic Planning and Design, Inc. (Applicant's Traffic Engineer)

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## **APPENDIX B:**

### **Traffic Count Printouts**



We trust that this review letter responds to your request, and satisfactorily addresses the traffic issues related to the proposed development at this time. If the Borough has any questions, or requires further clarification, please contact me.

Sincerely,



Anton Kuhner, P.E.  
Senior Project Manager

AKK/BMJ

cc: Chad Camburn, P.E., Bursich Associates, Inc  
Catherine M. Harper, Borough Solicitor  
Bob Heil, Borough of Hatfield  
Rob Cunningham, P.E., Holmes Cunningham, LLC (Applicant's Engineer)  
Matt Hammond, P.E., Traffic Planning and Design, Inc. (Applicant's Traffic Engineer)

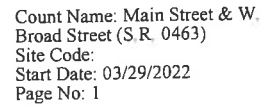
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## **APPENDIX B:**

### **Traffic Count Printouts**



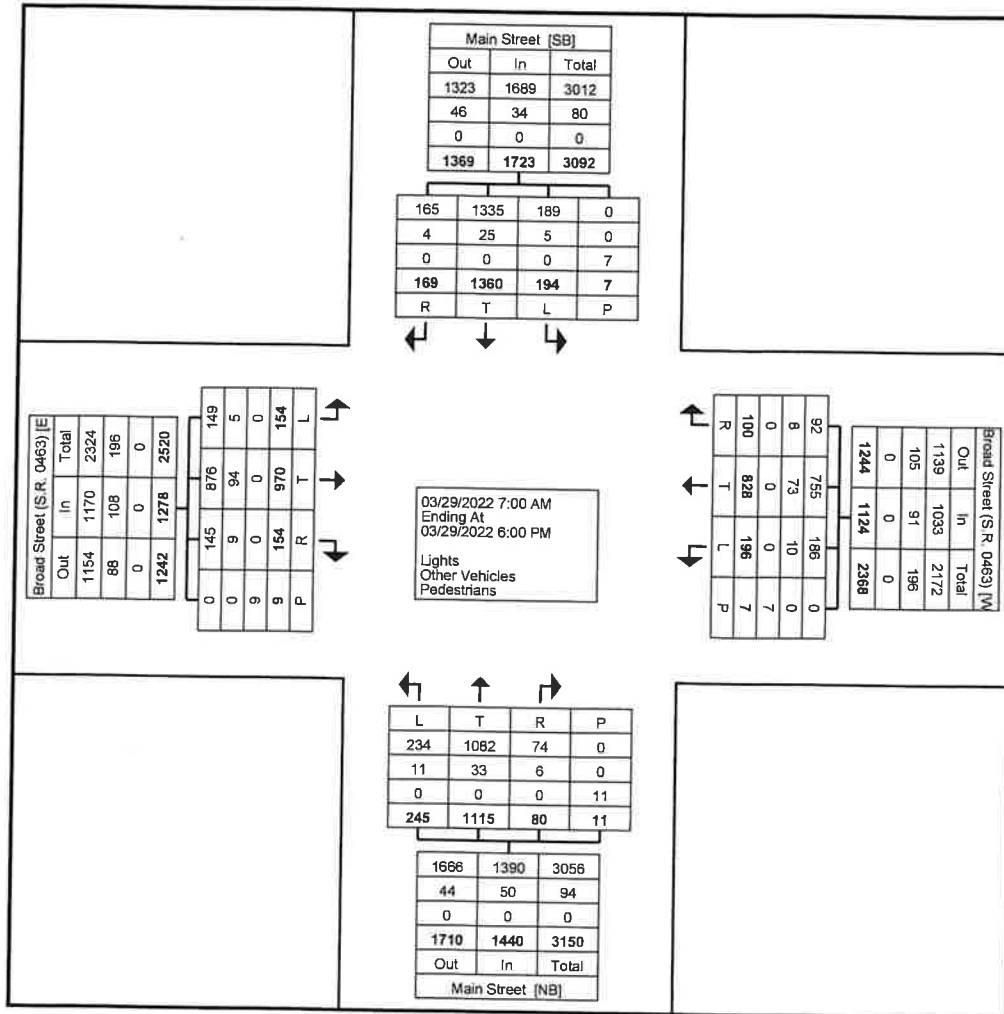




Traffic Planning and Design, Inc  
2500 East High Street  
Suite 650  
Pottstown, Pennsylvania, United States 19464  
610.326.3100

Counter: MIO:  
Set up By JH::

Count Name: Main Street & W.  
Broad Street (S.R. 0463)  
Site Code:  
Start Date: 03/29/2022  
Page No: 2



Turning Movement Data Plot



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Suite 650  
Pottstown, Pennsylvania, United States 19464  
610.326.3100

Count Name: Main Street & W.  
Broad Street (S.R. 0463)  
Site Code:  
Start Date: 03/29/2022  
Page No: 1

Counter: MIO:  
Set up By JH::

### Turning Movement Data

Start Time	Broad Street (S.R. 0463) Eastbound						Broad Street (S.R. 0463) Westbound						Main Street Northbound						Main Street Southbound						Int. Total	
	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total		
7:00 AM	2	55	7	0	0	64	6	45	2	0	0	53	16	35	4	0	0	55	3	85	9	0	0	97	269	
7:15 AM	2	54	2	0	0	58	4	46	4	0	0	54	12	37	0	0	0	49	12	97	7	0	0	116	277	
7:30 AM	2	50	2	0	2	54	16	48	9	0	2	73	9	40	6	0	1	55	6	96	14	0	1	116	298	
7:45 AM	10	66	6	0	0	82	12	59	6	0	2	77	15	66	5	0	0	86	17	111	16	0	1	144	389	
Hourly Total	16	225	17	0	2	258	38	198	21	0	4	257	52	178	15	0	1	245	38	389	46	0	2	473	1233	
8:00 AM	9	55	11	0	1	75	12	36	8	0	0	56	13	51	6	0	2	70	14	109	10	0	1	133	334	
8:15 AM	17	43	11	0	0	71	8	34	7	0	0	49	7	52	5	2	3	66	5	91	9	0	0	105	291	
8:30 AM	9	61	6	0	2	76	11	42	1	0	0	54	7	42	5	0	2	54	11	76	11	0	0	98	282	
8:45 AM	7	43	5	0	2	55	5	37	18	0	0	60	13	49	4	0	2	66	10	88	14	0	0	112	293	
Hourly Total	42	202	33	0	5	277	36	149	34	0	0	219	40	194	20	2	6	256	40	364	44	0	1	448	1200	
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
4:00 PM	10	63	16	0	1	89	15	68	5	0	1	88	13	78	6	0	0	97	18	71	14	0	1	103	377	
4:15 PM	12	70	12	0	0	94	15	61	11	0	0	87	24	100	8	0	0	132	17	74	4	0	0	95	408	
4:30 PM	14	77	14	0	0	105	15	61	7	0	0	83	21	91	1	0	0	113	22	102	15	0	0	139	440	
4:45 PM	11	57	8	0	0	76	15	65	7	0	0	87	26	98	2	0	1	126	17	80	13	0	0	110	399	
Hourly Total	47	267	50	0	1	364	60	255	30	0	1	345	84	367	17	0	1	488	74	327	46	0	1	447	1624	
5:00 PM	15	90	9	0	0	114	22	59	7	0	1	88	12	103	8	0	0	123	13	93	3	0	1	109	434	
5:15 PM	14	73	16	0	0	103	16	50	5	0	1	71	24	120	5	0	2	149	11	76	11	0	2	98	421	
5:30 PM	12	64	12	0	1	88	12	66	1	0	0	79	18	72	7	0	1	97	12	61	8	0	0	81	345	
5:45 PM	8	49	17	0	0	74	12	51	2	0	0	65	15	81	6	0	0	102	6	50	10	1	0	67	308	
Hourly Total	49	276	54	0	1	379	62	228	15	0	2	303	69	376	26	0	3	471	42	280	32	1	3	355	1508	
Grand Total	154	970	154	0	3	1278	196	828	100	0	7	1124	245	1115	78	2	11	1440	194	1360	168	1	7	1723	5565	
Approach %	12.1	75.9	12.1	0.0	-	-	17.4	73.7	8.9	0.0	-	-	17.0	77.4	5.4	0.1	-	-	11.3	78.9	9.8	0.1	-	-	-	
Total %	2.8	17.4	2.8	0.0	-	23.0	3.5	14.9	1.8	0.0	-	20.2	4.4	20.0	1.4	0.0	-	25.9	3.5	24.4	3.0	0.0	-	-	31.0	
Lights	149	876	145	0	-	1170	186	755	92	0	-	1033	234	1082	72	2	-	1390	189	1335	164	1	-	-	1689	
% Lights	96.8	90.3	94.2	-	-	91.5	94.9	91.2	92.0	-	-	91.9	95.5	97.0	92.3	100.0	-	96.5	97.4	98.2	97.6	100.0	-	-	98.0	94.9
Other Vehicles	5	94	9	0	-	108	10	73	8	0	-	91	11	33	6	0	-	50	5	25	4	0	-	-	34	283
% Other Vehicles	3.2	9.7	5.8	-	-	8.5	5.1	8.8	8.0	-	-	8.1	4.5	3.0	7.7	0.0	-	3.5	2.6	1.8	2.4	0.0	-	-	2.0	5.1
Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
% Pedestrians	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	

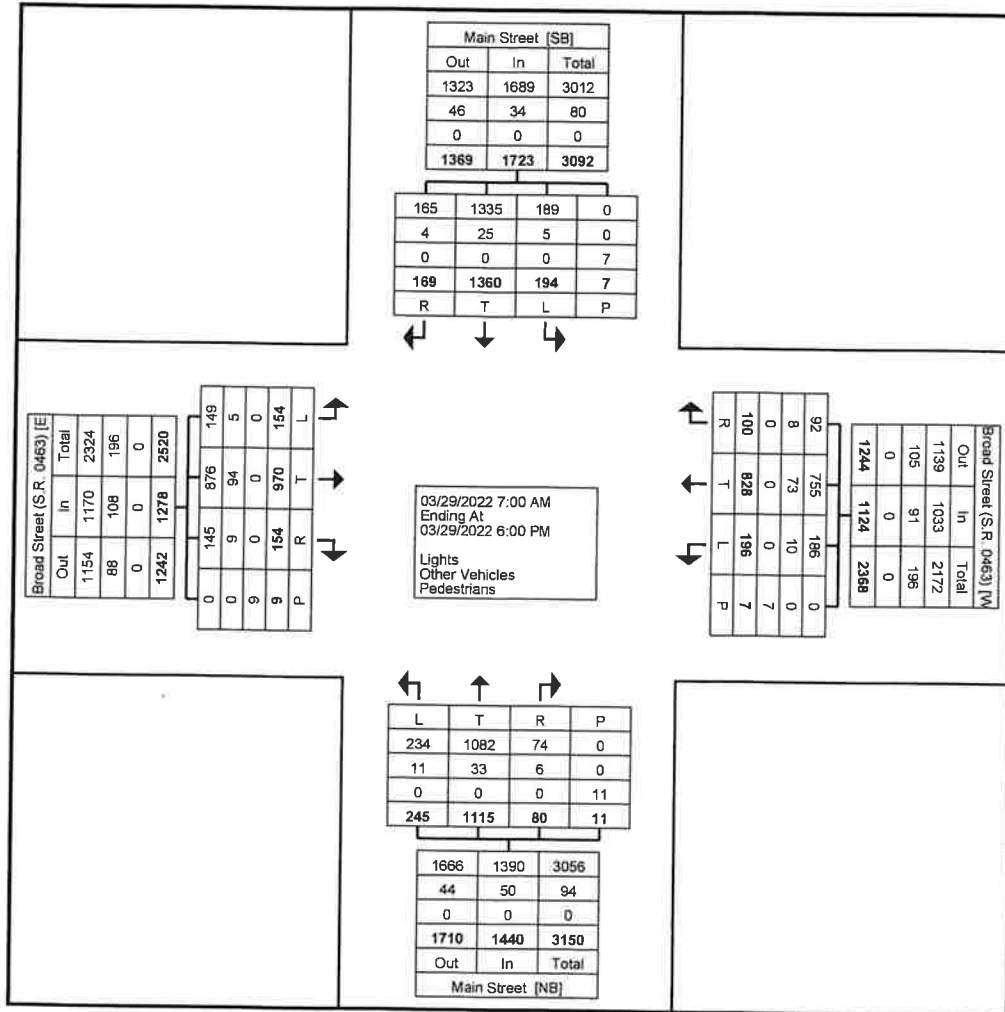




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610.326.3100

Counter: MIO:  
Set up By JH::

Count Name: Main Street & W.  
Broad Street (S.R. 0463)  
Site Code:  
Start Date: 03/29/2022  
Page No: 2



Turning Movement Data Plot



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Site Code:  
Start Date: 03/29/2022  
Page No: 3

### Turning Movement Peak Hour Data (7:30 AM)

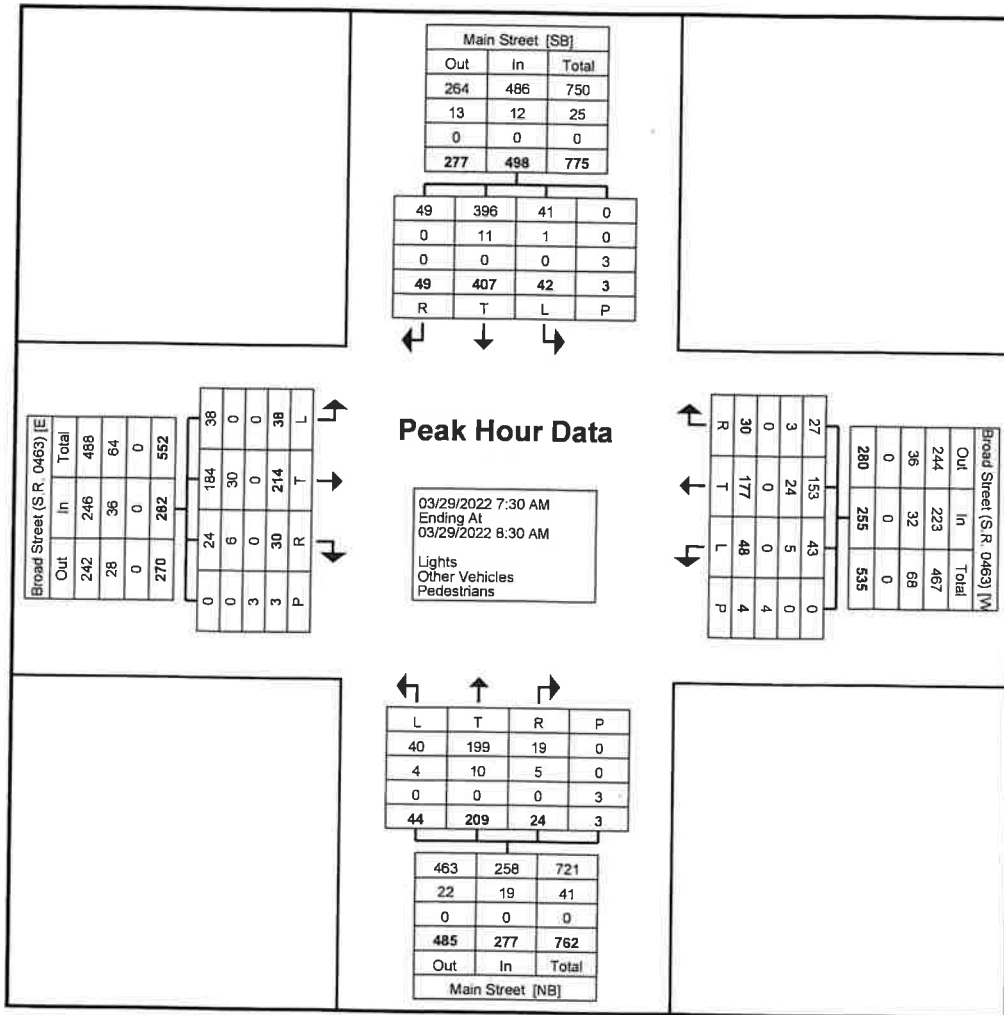
Start Time	Broad Street (S.R. 0463) Eastbound						Broad Street (S.R. 0463) Westbound						Main Street Northbound						Main Street Southbound						Int. Total
	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	
7:30 AM	2	50	2	0	2	54	16	48	9	0	2	73	9	40	6	0	1	55	6	96	14	0	1	116	298
7:45 AM	10	66	6	0	0	82	12	59	6	0	2	77	15	66	5	0	0	86	17	111	16	0	1	144	389
8:00 AM	9	55	11	0	1	75	12	36	8	0	0	56	13	51	8	0	2	70	14	109	10	0	1	133	334
8:15 AM	17	43	11	0	0	71	8	34	7	0	0	49	7	52	5	2	0	66	5	91	9	0	0	105	291
Total	38	214	30	0	3	282	48	177	30	0	4	255	44	209	22	2	3	277	42	407	49	0	3	498	1312
Approach %	13.5	75.9	10.6	0.0	-	-	18.8	69.4	11.8	0.0	-	-	15.9	75.5	7.9	0.7	-	-	8.4	81.7	9.8	0.0	-	-	-
Total %	2.9	16.3	2.3	0.0	-	21.5	3.7	13.5	2.3	0.0	-	19.4	3.4	15.9	1.7	0.2	-	21.1	3.2	31.0	3.7	0.0	-	38.0	-
PHF	0.559	0.811	0.682	0.000	-	0.860	0.750	0.750	0.833	0.000	-	0.828	0.733	0.792	0.917	0.250	-	0.805	0.618	0.917	0.766	0.000	-	0.865	0.843
Lights	38	184	24	0	-	246	43	153	27	0	-	223	40	199	17	2	-	258	41	396	49	0	-	486	1213
% Lights	100.0	86.0	80.0	-	-	87.2	89.6	86.4	90.0	-	-	87.5	90.9	95.2	77.3	100.0	-	93.1	97.6	97.3	100.0	-	-	97.6	92.5
Other Vehicles	0	30	6	0	-	36	5	24	3	0	-	32	4	10	5	0	-	19	1	11	0	0	-	12	99
% Other Vehicles	0.0	14.0	20.0	-	-	12.8	10.4	13.6	10.0	-	-	12.5	9.1	4.8	22.7	0.0	-	6.9	2.4	2.7	0.0	-	-	2.4	7.5
Pedestrians	-	-	-	-	3	-	-	-	-	-	1	-	-	-	-	-	3	-	-	-	-	-	3	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-



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Broad Street (S.R. 0463)  
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Start Date: 03/29/2022  
Page No: 4



Turning Movement Peak Hour Data Plot (7:30 AM)



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610.326.3100

Count Name: Main Street & W.  
Broad Street (S.R. 0463)  
Site Code:  
Start Date: 03/29/2022  
Page No: 3

Counter: MIO:  
Set up By JH:

### Turning Movement Peak Hour Data (7:30 AM)

Start Time	Broad Street (S.R. 0463) Eastbound						Broad Street (S.R. 0463) Westbound						Main Street Northbound						Main Street Southbound						Int. Total
	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	
7:30 AM	2	50	2	0	2	54	16	48	9	0	2	73	9	40	6	0	1	55	6	96	14	0	1	116	298
7:45 AM	10	66	6	0	0	82	12	59	6	0	2	77	15	66	5	0	0	86	17	111	16	0	1	144	389
8:00 AM	9	55	11	0	1	75	12	36	8	0	0	56	13	51	6	0	2	70	14	109	10	0	1	133	334
8:15 AM	17	43	11	0	0	71	8	34	7	0	0	49	7	52	5	2	0	66	5	91	9	0	0	105	291
Total	38	214	30	0	3	282	48	177	30	0	4	255	44	209	22	2	3	277	42	407	49	0	0	498	1312
Approach %	13.5	75.9	10.6	0.0	-	-	18.8	69.4	11.8	0.0	-	-	15.9	75.5	7.9	0.7	-	-	8.4	81.7	9.8	0.0	-	-	-
Total %	2.9	16.3	2.3	0.0	-	21.5	3.7	13.5	2.3	0.0	-	19.4	3.4	15.9	1.7	0.2	-	21.1	3.2	31.0	3.7	0.0	-	-	38.0
PHF	0.559	0.811	0.682	0.000	-	0.860	0.750	0.750	0.833	0.000	-	0.828	0.733	0.792	0.917	0.250	-	0.805	0.618	0.917	0.766	0.000	-	-	0.865
Lights	38	184	24	0	-	245	43	153	27	0	-	223	40	199	17	2	-	258	41	396	49	0	-	-	486
% Lights	100.0	86.0	80.0	-	-	87.2	89.6	86.4	90.0	-	-	87.5	90.9	95.2	77.3	100.0	-	93.1	97.6	97.3	100.0	-	-	-	97.6
Other Vehicles	0	30	6	0	-	36	5	24	3	0	-	32	4	10	5	0	-	19	1	11	0	0	-	-	12
% Other Vehicles	0.0	14.0	20.0	-	-	12.8	10.4	13.6	10.0	-	-	12.5	9.1	4.8	22.7	0.0	-	6.9	2.4	2.7	0.0	-	-	-	2.4
Pedestrians	-	-	-	-	3	-	-	-	-	-	5	-	-	-	-	-	3	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	1.033	-	-	-	-	-	1.961	-	-	-	-	-	1.033	-	-	-	-	-	0.262	-	-

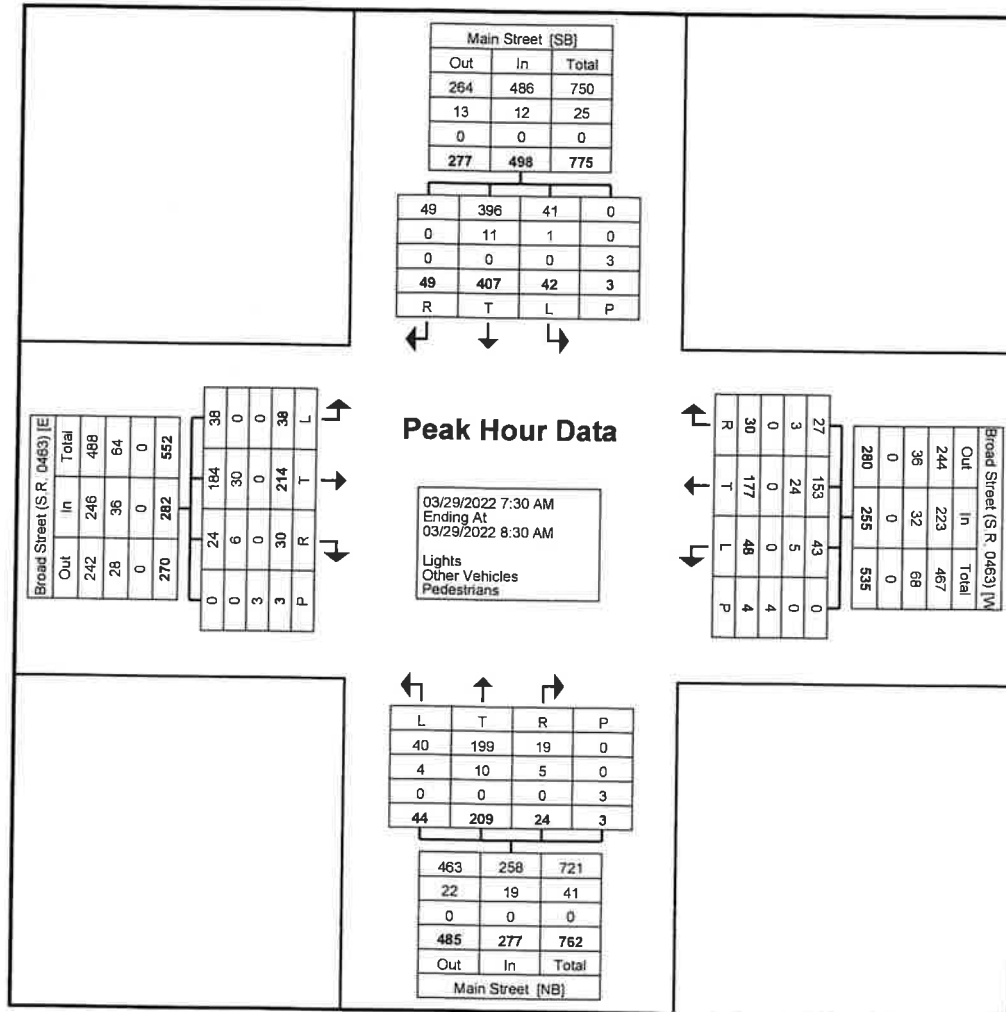




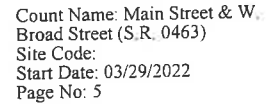
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610.326.3100

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Broad Street (S.R. 0463)  
Site Code:  
Start Date: 03/29/2022  
Page No: 4



Turning Movement Peak Hour Data Plot (7:30 AM)

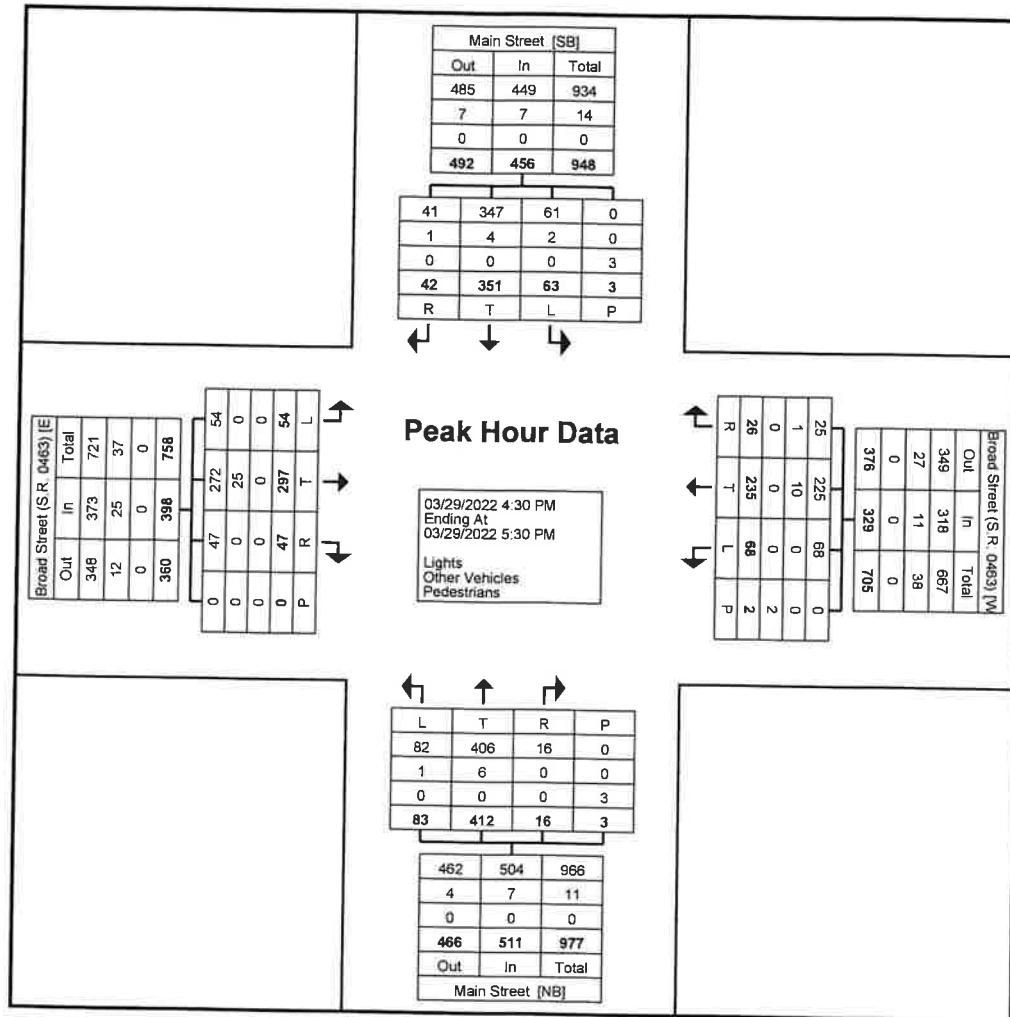




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610.326.3100

Counter: MIO:  
Set up By JH.:

Count Name: Main Street & W.  
Broad Street (S.R. 0463)  
Site Code:  
Start Date: 03/29/2022  
Page No: 6



Turning Movement Peak Hour Data Plot (4:30 PM)



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Count Name: Main Street & W.  
Broad Street (S.R. 0463)  
Site Code:  
Start Date: 03/29/2022  
Page No: 5

Counter: MIO:  
Set up By JH:

### Turning Movement Peak Hour Data (4:30 PM)

Start Time	Broad Street (S.R. 0463) Eastbound						Broad Street (S.R. 0463) Westbound						Main Street Northbound						Main Street Southbound						Int. Total
	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	Left	Thru	Right	Right on Red	Peds	App. Total	
4:30 PM	14	77	14	0	0	105	15	61	7	0	0	83	21	91	1	0	0	113	22	102	15	0	0	139	440
4:45 PM	11	57	8	0	0	76	15	65	7	0	0	87	26	98	2	0	0	126	17	80	13	0	0	110	399
5:00 PM	15	90	9	0	0	114	22	59	7	0	1	88	12	103	8	0	0	123	13	93	3	0	0	109	434
5:15 PM	14	73	16	0	0	103	16	50	5	0	1	71	24	120	5	0	2	149	11	76	11	0	2	98	421
Total	54	297	47	0	0	398	68	235	26	0	2	329	83	412	16	0	3	511	63	351	42	0	3	458	1694
Approach %	13.6	74.6	11.8	0.0	-	-	20.7	71.4	7.9	0.0	-	-	16.2	80.6	3.1	0.0	-	-	13.8	77.0	9.2	0.0	-	-	-
Total %	3.2	17.5	2.8	0.0	-	23.5	4.0	13.9	1.5	0.0	-	19.4	4.9	24.3	0.9	0.0	-	30.2	3.7	20.7	2.5	0.0	-	26.9	-
PHF	0.900	0.825	0.734	0.000	-	0.873	0.773	0.904	0.929	0.000	-	0.935	0.798	0.858	0.500	0.000	-	0.857	0.716	0.860	0.700	0.000	-	0.820	0.963
Lights	54	272	47	0	-	373	68	225	25	0	-	318	82	406	16	0	-	504	61	347	41	0	-	449	1644
% Lights	100.0	91.6	100.0	-	-	93.7	100.0	95.7	96.2	-	-	96.7	98.8	98.5	100.0	-	-	98.6	96.8	98.9	97.6	-	-	98.5	97.0
Other Vehicles	0	25	0	0	-	25	0	10	1	0	-	11	1	6	0	0	-	7	2	4	1	0	-	7	50
% Other Vehicles	0.0	8.4	0.0	-	-	6.3	0.0	4.3	3.8	-	-	3.3	1.2	1.5	0.0	-	-	1.4	3.2	1.1	2.4	-	-	1.5	3.0
Pedestrians	-	-	-	-	0	-	-	-	-	-	2	-	-	-	-	-	3	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	100.0	-	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-





Count Name: Main Street & W.  
Broad Street (S.R. 0463)  
Site Code:  
Start Date: 03/29/2022  
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# **APPENDIX C:**

## **Traffic Volume Development Data**



## Trip Distribution Data



# **APPENDIX C:**

## **Traffic Volume Development Data**





## Trip Distribution Data



**Trip Distribution (To/From)**

	AM				PM				OVERALL		
	IN	OUT	TOTAL		IN	OUT	TOTAL		TOTAL		USE
N. Main Street (to/from the north)	500	278	778	29.5%	457	494	951	28.0%	1729	28.7%	29%
S. Main Street (to/from the south)	278	487	765	29.0%	513	467	980	28.8%	1745	28.9%	29%
W. Broad Street (to/from the west)	283	271	554	21.0%	399	361	760	22.4%	1314	21.8%	22%
E. Broad Street (to/from the east)	256	281	537	20.4%	330	377	707	20.8%	1244	20.6%	20%
Total:	1317	1317	2634	100%	1699	1699	3398	100%	6032	100%	100%

# Volume Development Worksheets



**Trip Distribution (To/From)**

	AM				PM				OVERALL		
	IN	OUT	TOTAL		IN	OUT	TOTAL		TOTAL		USE
N. Main Street (to/from the north)	500	278	778	29.5%	457	494	951	28.0%	1729	28.7%	29%
S. Main Street (to/from the south)	278	487	765	29.0%	513	467	980	28.8%	1745	28.9%	29%
W. Broad Street (to/from the west)	283	271	554	21.0%	399	361	760	22.4%	1314	21.8%	22%
E. Broad Street (to/from the east)	256	281	537	20.4%	330	377	707	20.8%	1244	20.6%	20%
Total:	1317	1317	2634	100%	1699	1699	3398	100%	6032	100%	100%

# Volume Development Worksheets





TPD# PNPG.00002  
 10/17/2024  
 Traffic Volumes Worksheet  
 Intersection:  
 Synchro Node:

Main Street (N/S) & Broad Street (E/W)									
1	Adjacent intersections:	West	0	East	0	North	0	South	0

Time Period: Weekday A.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection Volume
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	
2022 Existing (Raw) Counts	38	214	30	48	177	30	44	209	24	42	407	49	1312
Base growth (0.21% compounded for 2 yrs)	0	1	0	0	1	0	0	1	0	0	2	0	5
2024 Existing Volumes (Balanced)	38	215	30	48	178	30	44	210	24	42	409	49	1317
Base growth (0.21% compounded for 2 yrs)	0	1	0	0	1	0	0	1	0	0	2	0	5
2026 Base Volumes	38	216	30	48	179	30	44	211	24	42	411	49	1322

Site Trips  
 New  
 Enter = 1  
 Exit = 3

Site Trip Assignment % - Enter	22%					21%		29%					
Site Trip Assignment % - Exit										21%	29%	22%	
Total Site Trips	0	0	0	0	0	0	0	1	0	0	1	1	
2026 Projected Volumes	38	216	30	48	179	30	44	212	24	42	412	50	1325

Time Period: Weekday P.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection Volume
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	
2022 Existing (Raw) Counts	54	297	47	68	235	26	83	412	16	63	351	42	1694
Base growth (0.21% compounded for 2 yrs)	0	1	0	0	1	0	0	2	0	0	1	0	5
2024 Existing Volumes (Balanced)	54	298	47	68	236	26	83	414	16	63	352	42	1699
Base growth (0.21% compounded for 2 yrs)	0	2	0	0	1	0	0	2	0	0	2	0	7
2026 Base Volumes	54	300	47	68	237	26	83	416	16	63	354	42	1706

Site Trips  
 New  
 Enter = 3  
 Exit = 2

Site Trip Assignment % - Enter	22%					21%		29%					
Site Trip Assignment % - Exit										21%	29%	22%	
Total Site Trips	1	0	0	0	0	0	0	1	0	0	1	0	
2026 Projected Volumes	55	300	47	68	237	26	83	417	16	63	355	42	1709

TPD# PNP0.00002  
 10/17/2024  
 Traffic Volumes Worksheet  
 Intersection:  
 Synchro Node:

N. Main Street & Proposed Site Driveway												
2	Adjacent Intersections:			West	0	East	0	North	0	South	0	

Time Period: Weekday A.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	Volume
2022 Existing (Raw) Counts								277			498		775
Base growth (0.21% compounded for 2 yrs)								1			2		3
2024 Existing Volumes (Balanced)	0	0	0	0	0	0	0	278	0	0	500	0	778
Base growth (0.21% compounded for 2 yrs)								1			2		3
2026 Base Volumes	0	0	0	0	0	0	0	279	0	0	502	0	781

Site Trips  
 New  
 Enter = 1  
 Exit = 3

Site Trip Assignment % - Enter										71%	29%		
Site Trip Assignment % - Exit				71%		29%							
Total Site Trips	0	0	0	2	0	1	0	0	1	0	0	0	
2026 Projected Volumes	0	0	0	2	0	1	0	279	1	0	502	0	785

Time Period: Weekday P.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	Volume
2022 Existing (Raw) Counts								492			456		948
Base growth (0.21% compounded for 2 yrs)								2			1		3
2024 Existing Volumes (Balanced)	0	0	0	0	0	0	0	494	0	0	457	0	951
Base growth (0.21% compounded for 2 yrs)								2			2		4
2026 Base Volumes	0	0	0	0	0	0	0	496	0	0	459	0	955

Site Trips  
 New  
 Enter = 3  
 Exit = 2

Site Trip Assignment % - Enter										71%	29%		
Site Trip Assignment % - Exit				71%		29%							
Total Site Trips	0	0	0	1	0	1	0	0	2	1	0	0	
2026 Projected Volumes	0	0	0	1	0	1	0	496	2	1	459	0	960

TPD# PNPG.00002  
 10/17/2024  
 Traffic Volumes Worksheet  
 Intersection:  
 Synchro Node:

Main Street (N/S) & Broad Street (E/W)									
1	Adjacent intersections:	West	0	East	0	North	0	South	0

Time Period: Weekday A.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	
2022 Existing (Raw) Counts	38	214	30	48	177	30	44	209	24	42	407	49	1312
Base growth (0.21% compounded for 2 yrs)	0	1	0	0	1	0	0	1	0	0	2	0	5
2024 Existing Volumes (Balanced)	38	215	30	48	178	30	44	210	24	42	409	49	1317
Base growth (0.21% compounded for 2 yrs)	0	1	0	0	1	0	0	1	0	0	2	0	5
2026 Base Volumes	38	216	30	48	179	30	44	211	24	42	411	49	1322

Site Trips  
 New  
 Enter = 1  
 Exit = 3

Site Trip Assignment % - Enter	22%					21%		29%			21%	29%	22%	
Site Trip Assignment % - Exit														
Total Site Trips	0	0	0	0	0	0	0	1	0	0	1	1		
2026 Projected Volumes	38	216	30	48	179	30	44	212	24	42	412	50	1325	

Time Period: Weekday P.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	
2022 Existing (Raw) Counts	54	297	47	68	235	26	83	412	16	63	351	42	1694
Base growth (0.21% compounded for 2 yrs)	0	1	0	0	1	0	0	2	0	0	1	0	5
2024 Existing Volumes (Balanced)	54	298	47	68	236	26	83	414	16	63	352	42	1699
Base growth (0.21% compounded for 2 yrs)	0	2	0	0	1	0	0	2	0	0	2	0	7
2026 Base Volumes	54	300	47	68	237	26	83	416	16	63	354	42	1706

Site Trips  
 New  
 Enter = 3  
 Exit = 2

Site Trip Assignment % - Enter	22%					21%		29%			21%	29%	22%	
Site Trip Assignment % - Exit														
Total Site Trips	1	0	0	0	0	0	0	1	0	0	1	0		
2026 Projected Volumes	55	300	47	68	237	26	83	417	16	63	355	42	1709	

TPD# PNPG.00002  
 10/17/2024  
 Traffic Volumes Worksheet  
 Intersection:  
 Synchro Node:

N. Main Street & Proposed Site Driveway											
2	Adjacent Intersections:			West	0	East	0	North	0	South	0

Time Period: Weekday A.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	
2022 Existing (Raw) Counts								277			498		775
Base growth (0.21% compounded for 2 yrs)								1			2		3
2024 Existing Volumes (Balanced)	0	0	0	0	0	0	0	278	0	0	500	0	778
Base growth (0.21% compounded for 2 yrs)								1			2		3
2026 Base Volumes	0	0	0	0	0	0	0	279	0	0	502	0	781

Site Trips  
 New  
 Enter = 1  
 Exit = 3

Site Trip Assignment % - Enter									71%	29%			
Site Trip Assignment % - Exit													
Total Site Trips	0	0	0	2	0	1	0	0	1	0	0	0	
2026 Projected Volumes	0	0	0	2	0	1	0	279	1	0	502	0	785

Time Period: Weekday P.M. Peak Hour

	Eastbound			Westbound			Northbound			Southbound			Intersection
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	
2022 Existing (Raw) Counts								492			456		948
Base growth (0.21% compounded for 2 yrs)								2			1		3
2024 Existing Volumes (Balanced)	0	0	0	0	0	0	0	494	0	0	457	0	951
Base growth (0.21% compounded for 2 yrs)								2			2		4
2026 Base Volumes	0	0	0	0	0	0	0	496	0	0	459	0	955

Site Trips  
 New  
 Enter = 3  
 Exit = 2

Site Trip Assignment % - Enter									71%	29%			
Site Trip Assignment % - Exit													
Total Site Trips	0	0	0	1	0	1	0	0	2	1	0	0	
2026 Projected Volumes	0	0	0	1	0	1	0	496	2	1	459	0	960

## **APPENDIX D:**

### **Critical and Follow-up Headway Calculations**





**PNPG.00002**  
**N. Main Street & Site Driveway**

Critical Headway

			tc base	tc hv	phv	t cg	G	t 3lt	Base Crit
major left	AM	SB L	4.3	1	2%	0	1	0	<b>4.3</b>
	PM	SB L	4.3	1	2%	0	1	0	<b>4.3</b>
minor right	AM	WB R	6.2	1	2%	0.1	0	0	<b>6.2</b>
	PM	WB R	6.2	1	2%	0.1	0	0	<b>6.2</b>
minor left	AM	WB L	7.1	1	2%	0.2	0	0.7	<b>6.4</b>
	PM	WB L	7.1	1	2%	0.2	0	0.7	<b>6.4</b>

Follow-up headway

			t fbase	t fhv	phv	Follow-up
major left	AM	SB L	3	0.9	2%	<b>3.0</b>
	PM	SB L	3	0.9	2%	<b>3.0</b>
minor right	AM	WB R	3.1	0.9	2%	<b>3.1</b>
	PM	WB R	3.1	0.9	2%	<b>3.1</b>
minor left	AM	WB L	3	0.9	2%	<b>3.0</b>
	PM	WB L	3	0.9	2%	<b>3.0</b>

# **APPENDIX D:**

## **Critical and Follow-up Headway Calculations**



**PNPG.00002**  
**N. Main Street & Site Driveway**

Critical Headway

			tc base	tc hv	phv	t cg	G	t 3lt	Base Crit
major left	AM	SB L	4.3	1	2%	0	1	0	<b>4.3</b>
	PM	SB L	4.3	1	2%	0	1	0	<b>4.3</b>
minor right	AM	WB R	6.2	1	2%	0.1	0	0	<b>6.2</b>
	PM	WB R	6.2	1	2%	0.1	0	0	<b>6.2</b>
minor left	AM	WB L	7.1	1	2%	0.2	0	0.7	<b>6.4</b>
	PM	WB L	7.1	1	2%	0.2	0	0.7	<b>6.4</b>

Follow-up headway

			t fbase	t fhv	phv	Follow-up
major left	AM	SB L	3	0.9	2%	<b>3.0</b>
	PM	SB L	3	0.9	2%	<b>3.0</b>
minor right	AM	WB R	3.1	0.9	2%	<b>3.1</b>
	PM	WB R	3.1	0.9	2%	<b>3.1</b>
minor left	AM	WB L	3	0.9	2%	<b>3.0</b>
	PM	WB L	3	0.9	2%	<b>3.0</b>

# **APPENDIX E:**

## **Capacity Analysis Worksheets**



## Supporting Calculations





# **APPENDIX E:**

## **Capacity Analysis Worksheets**



## Supporting Calculations



### Heavy Vehicle Calculations for N. Main Street at the Proposed Site Driveway

Weekday A.M. Peak Hour							
N. Main Street & Site Driveway - Northbound Through Movement				N. Main Street & Site Driveway - Southbound Through Movement			
Percentage of Heavy Vehicles traveling from Main Street (N/W) & Broad Street (E/W) Intersection				Percentage of Heavy Vehicles traveling to Main Street (N/W) & Broad Street (E/W) Intersection			
Movement	Total Vehicles	Heavy Vehicles	HV %	Movement	Total Vehicles	Heavy Vehicles	HV %
NB T	277	13		SB T	498	12	
Combined	277	13	5%	Combined	498	12	2%

Weekday P.M. Peak Hour							
N. Main Street & Site Driveway - Northbound Through Movement				N. Main Street & Site Driveway - Southbound Through Movement			
Percentage of Heavy Vehicles traveling from Main Street (N/W) & Broad Street (E/W) Intersection				Percentage of Heavy Vehicles traveling to Main Street (N/W) & Broad Street (E/W) Intersection			
Movement	Total Vehicles	Heavy Vehicles	HV %	Movement	Total Vehicles	Heavy Vehicles	HV %
NB T	492	7		SB T	457	7	
Combined	492	7	1%	Combined	457	7	2%

**Notes:**

- (1) HV % = Heavy Vehicle Percentage
- (2) Vehicle count information obtained from the 3/29/2022 Turning Movement Counts
- (3) NB T = EB L + WB R + NB T movements at the Main Street (N/S) & Broad Street (E/W) Intersection
- (4) SB T = SB L + SB T + SB R movements at the Main Street (N/S) & Broad Street (E/W) Intersection

## Existing Conditions



### Heavy Vehicle Calculations for N. Main Street at the Proposed Site Driveway

Weekday A.M. Peak Hour							
N. Main Street & Site Driveway - Northbound Through Movement				N. Main Street & Site Driveway - Southbound Through Movement			
Percentage of Heavy Vehicles traveling from Main Street (N/W) & Broad Street (E/W) Intersection				Percentage of Heavy Vehicles traveling to Main Street (N/W) & Broad Street (E/W) Intersection			
Movement	Total Vehicles	Heavy Vehicles	HV %	Movement	Total Vehicles	Heavy Vehicles	HV %
NB T	277	13		SB T	498	12	
Combined	277	13	5%	Combined	498	12	2%

Weekday P.M. Peak Hour							
N. Main Street & Site Driveway - Northbound Through Movement				N. Main Street & Site Driveway - Southbound Through Movement			
Percentage of Heavy Vehicles traveling from Main Street (N/W) & Broad Street (E/W) Intersection				Percentage of Heavy Vehicles traveling to Main Street (N/W) & Broad Street (E/W) Intersection			
Movement	Total Vehicles	Heavy Vehicles	HV %	Movement	Total Vehicles	Heavy Vehicles	HV %
NB T	492	7		SB T	457	7	
Combined	492	7	1%	Combined	457	7	2%

**Notes:**

- (1) HV % = Heavy Vehicle Percentage
- (2) Vehicle count information obtained from the 3/29/2022 Turning Movement Counts
- (3) NB T = EB L + WB R + NB T movements at the Main Street (N/S) & Broad Street (E/W) Intersection
- (4) SB T = SB L + SB T + SB R movements at the Main Street (N/S) & Broad Street (E/W) Intersection



## Existing Conditions



1: Main Street & Broad Street  
Existing Conditions

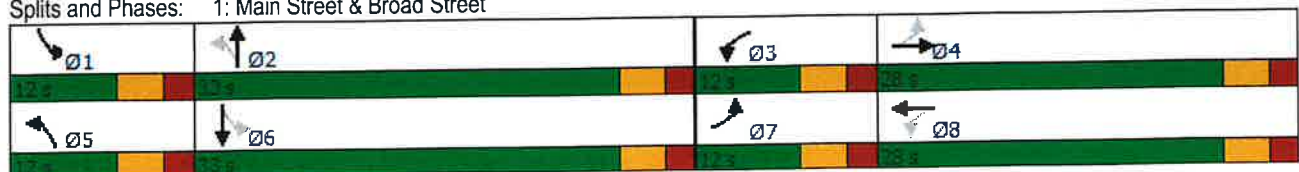
PNPG.00002  
Timing Plan: Weekday A.M. Peak Hour

	↖	→	↘	↙	←	↖	↙	↑	↗	↘	↓	↙
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (vph)	38	215	30	48	178	30	44	210	24	42	409	49
Future Volume (vph)	38	215	30	48	178	30	44	210	24	42	409	49
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	10	12	12	10	11	11	10	12	12	10	11	11
Grade (%)		-2%			-1%			3%			1%	
Storage Length (ft)	280		0	100		0	100		0	0		265
Storage Lanes	1		0	1		0	1		0	1		1
Taper Length (ft)	25			25			25			25		
Right Turn on Red			No			No			No			No
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		581			338			365			982	
Travel Time (s)		15.8			9.2			10.0			26.8	
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Heavy Vehicles (%)	0%	14%	20%	10%	14%	10%	9%	5%	21%	2%	3%	0%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	3.0	11.0		3.0	11.0		3.0	10.0		3.0	10.0	
Minimum Split (s)	8.0	16.0		8.0	16.0		8.0	15.0		8.0	15.0	
Total Split (s)	12.0	28.0		12.0	28.0		12.0	33.0		12.0	33.0	
Total Split (%)	14.1%	32.9%		14.1%	32.9%		14.1%	38.8%		14.1%	38.8%	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag		Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	Max		None	Max		None	None		None	None	

Intersection Summary

Area Type: Other  
Cycle Length: 85  
Actuated Cycle Length: 74.2  
Natural Cycle: 60  
Control Type: Actuated-Uncoordinated

Splits and Phases: 1: Main Street & Broad Street

























1: Main Street & Broad Street  
Existing Conditions

PNPG.00002

Timing Plan: Weekday A.M. Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	38	215	30	48	178	30	44	210	24	42	409	49
Future Volume (veh/h)	38	215	30	48	178	30	44	210	24	42	409	49
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1875	1675	1590	1695	1638	1695	1623	1680	1455	1766	1752	1794
Adj Flow Rate, veh/h	45	256	36	57	212	36	52	250	27	50	487	58
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	0	14	20	10	14	10	9	5	21	2	3	0
Cap, veh/h	422	466	66	364	455	77	223	536	58	423	549	65
Arrive On Green	0.04	0.32	0.31	0.05	0.33	0.32	0.05	0.36	0.35	0.05	0.36	0.34
Sat Flow, veh/h	1785	1437	202	1614	1365	232	1546	1490	161	1682	1536	183
Grp Volume(v), veh/h	45	0	292	57	0	248	52	0	277	50	0	545
Grp Sat Flow(s),veh/h/ln	1785	0	1639	1614	0	1596	1546	0	1651	1682	0	1719
Q Serve(g_s), s	1.2	0.0	10.8	1.7	0.0	9.1	1.5	0.0	9.6	1.4	0.0	22.1
Cycle Q Clear(g_c), s	1.2	0.0	10.8	1.7	0.0	9.1	1.5	0.0	9.6	1.4	0.0	22.1
Prop In Lane	1.00		0.12	1.00		0.15	1.00		0.10	1.00		0.11
Lane Grp Cap(c), veh/h	422	0	532	364	0	532	223	0	593	423	0	614
V/C Ratio(X)	0.11	0.00	0.55	0.16	0.00	0.47	0.23	0.00	0.47	0.12	0.00	0.89
Avail Cap(c_a), veh/h	537	0	532	454	0	532	313	0	647	526	0	674
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	15.6	0.0	20.6	15.6	0.0	19.5	16.9	0.0	18.3	14.1	0.0	22.4
Incr Delay (d2), s/veh	0.1	0.0	4.0	0.2	0.0	2.9	0.5	0.0	0.6	0.1	0.0	12.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.9	0.0	8.1	1.1	0.0	6.6	1.0	0.0	6.5	0.9	0.0	16.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	15.7	0.0	24.6	15.8	0.0	22.4	17.4	0.0	18.9	14.2	0.0	35.3
LnGrp LOS	B	A	C	B	A	C	B	A	B	B	A	D
Approach Vol, veh/h	337			305			329			595		
Approach Delay, s/veh	23.4			21.2			18.6			33.6		
Approach LOS	C			C			B			C		
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	7.5	30.6	7.9	28.0	7.7	30.4	7.2	28.7				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	7.0	28.0	7.0	23.0	7.0	28.0	7.0	23.0				
Max Q Clear Time (g_c+1), s	3.9	11.6	4.2	12.8	4.0	24.1	3.7	11.1				
Green Ext Time (p_c), s	0.0	1.5	0.0	1.3	0.0	1.3	0.0	1.1				
Intersection Summary												
HCM 6th Ctrl Delay				25.8								
HCM 6th LOS				C								

1: Main Street & Broad Street  
Existing Conditions

PNPG.00002

Timing Plan: Weekday A.M. Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	38	215	30	48	178	30	44	210	24	42	409	49
Future Volume (vph)	38	215	30	48	178	30	44	210	24	42	409	49
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	10	12	12	10	11	11	10	12	12	10	11	11
Grade (%)		-2%			-1%			3%			1%	
Storage Length (ft)	280		0	100		0	100		0	0		265
Storage Lanes	1		0	1		0	1		0	1		1
Taper Length (ft)	25			25			25			25		
Right Turn on Red			No			No			No			No
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		581			338			365			982	
Travel Time (s)		15.8			9.2			10.0			26.8	
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Heavy Vehicles (%)	0%	14%	20%	10%	14%	10%	9%	5%	21%	2%	3%	0%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	3.0	11.0		3.0	11.0		3.0	10.0		3.0	10.0	
Minimum Split (s)	8.0	16.0		8.0	16.0		8.0	15.0		8.0	15.0	
Total Split (s)	12.0	28.0		12.0	28.0		12.0	33.0		12.0	33.0	
Total Split (%)	14.1%	32.9%		14.1%	32.9%		14.1%	38.8%		14.1%	38.8%	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag		Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	Max		None	Max		None	None		None	None	

Intersection Summary

Area Type: Other

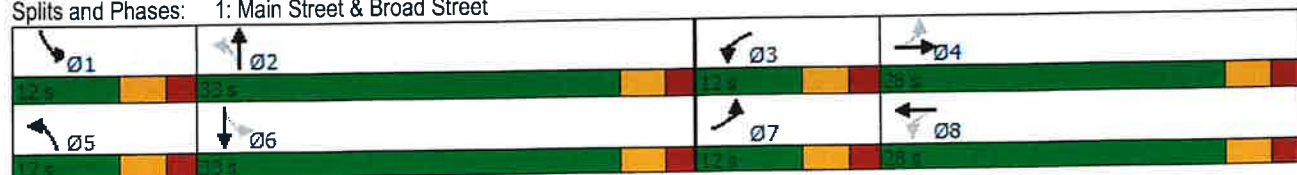
Cycle Length: 85

Actuated Cycle Length: 74.2

Natural Cycle: 60

Control Type: Actuated-Uncoordinated

Splits and Phases: 1: Main Street & Broad Street

























1: Main Street & Broad Street  
Existing Conditions

PNPG.00002

Timing Plan: Weekday A.M. Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	38	215	30	48	178	30	44	210	24	42	409	49
Future Volume (veh/h)	38	215	30	48	178	30	44	210	24	42	409	49
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1875	1675	1590	1695	1638	1695	1623	1680	1455	1766	1752	1794
Adj Flow Rate, veh/h	45	256	36	57	212	36	52	250	27	50	487	58
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	0	14	20	10	14	10	9	5	21	2	3	0
Cap, veh/h	422	466	66	364	455	77	223	536	58	423	549	65
Arrive On Green	0.04	0.32	0.31	0.05	0.33	0.32	0.05	0.36	0.35	0.05	0.36	0.34
Sat Flow, veh/h	1785	1437	202	1614	1365	232	1546	1490	161	1682	1536	183
Grp Volume(v), veh/h	45	0	292	57	0	248	52	0	277	50	0	545
Grp Sat Flow(s),veh/h/ln	1785	0	1639	1614	0	1596	1546	0	1651	1682	0	1719
Q Serve(g_s), s	1.2	0.0	10.8	1.7	0.0	9.1	1.5	0.0	9.6	1.4	0.0	22.1
Cycle Q Clear(g_c), s	1.2	0.0	10.8	1.7	0.0	9.1	1.5	0.0	9.6	1.4	0.0	22.1
Prop In Lane	1.00		0.12	1.00		0.15	1.00		0.10	1.00		0.11
Lane Grp Cap(c), veh/h	422	0	532	364	0	532	223	0	593	423	0	614
V/C Ratio(X)	0.11	0.00	0.55	0.16	0.00	0.47	0.23	0.00	0.47	0.12	0.00	0.89
Avail Cap(c_a), veh/h	537	0	532	454	0	532	313	0	647	526	0	674
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	15.6	0.0	20.6	15.6	0.0	19.5	16.9	0.0	18.3	14.1	0.0	22.4
Incr Delay (d2), s/veh	0.1	0.0	4.0	0.2	0.0	2.9	0.5	0.0	0.6	0.1	0.0	12.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.9	0.0	8.1	1.1	0.0	6.6	1.0	0.0	6.5	0.9	0.0	16.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	15.7	0.0	24.6	15.8	0.0	22.4	17.4	0.0	18.9	14.2	0.0	35.3
LnGrp LOS	B	A	C	B	A	C	B	A	B	B	A	D
Approach Vol, veh/h	337			305			329			595		
Approach Delay, s/veh	23.4			21.2			18.6			33.6		
Approach LOS	C			C			B			C		
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	7.5	30.6	7.9	28.0	7.7	30.4	7.2	28.7				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	7.0	28.0	7.0	23.0	7.0	28.0	7.0	23.0				
Max Q Clear Time (g_c+I1), s	3.9	11.6	4.2	12.8	4.0	24.1	3.7	11.1				
Green Ext Time (p_c), s	0.0	1.5	0.0	1.3	0.0	1.3	0.0	1.1				
Intersection Summary												
HCM 6th Ctrl Delay	25.8											
HCM 6th LOS	C											

1: Main Street & Broad Street  
Existing Conditions

PNPG.00002

Timing Plan: Weekday P.M. Peak Hour

	↖	→	↗	↖	←	↖	↖	↑	↗	↘	↓	↙
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (vph)	54	298	47	68	236	26	83	414	16	63	352	42
Future Volume (vph)	54	298	47	68	236	26	83	414	16	63	352	42
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	10	12	12	10	11	11	10	12	12	10	11	11
Grade (%)		-2%			-1%			3%			1%	
Storage Length (ft)	280		0	100		0	100		0	0		265
Storage Lanes	1		0	1		0	1		0	1		1
Taper Length (ft)	25			25			25			25		
Right Turn on Red			No			No			No			No
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		581			338			365			982	
Travel Time (s)		15.8			9.2			10.0			26.8	
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	8%	0%	0%	4%	4%	1%	2%	0%	3%	1%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	3.0	11.0		3.0	11.0		3.0	10.0		3.0	10.0	
Minimum Split (s)	8.0	16.0		8.0	16.0		8.0	15.0		8.0	15.0	
Total Split (s)	12.0	30.0		12.0	30.0		12.0	31.0		12.0	31.0	
Total Split (%)	14.1%	35.3%		14.1%	35.3%		14.1%	36.5%		14.1%	36.5%	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag		Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	Max		None	Max		None	None		None	None	

Intersection Summary

Area Type: Other

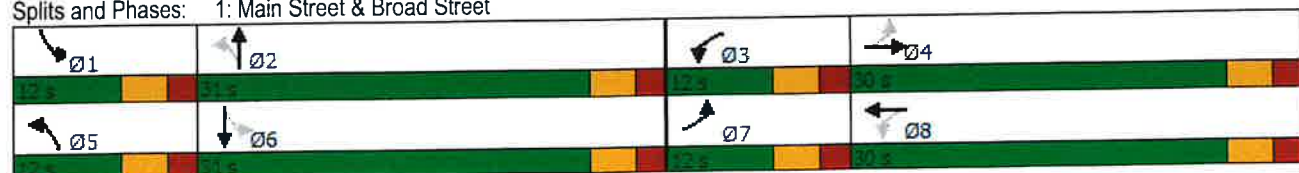
Cycle Length: 85

Actuated Cycle Length: 77

Natural Cycle: 60

Control Type: Actuated-Uncoordinated

Splits and Phases: 1: Main Street & Broad Street

























1: Main Street & Broad Street  
Existing Conditions

PNPG.00002

Timing Plan: Weekday P.M. Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	54	298	47	68	236	26	83	414	16	63	352	42
Future Volume (veh/h)	54	298	47	68	236	26	83	414	16	63	352	42
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1875	1761	1875	1837	1780	1780	1736	1722	1750	1752	1780	1766
Adj Flow Rate, veh/h	56	310	49	71	246	27	86	431	17	66	367	44
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	0	8	0	0	4	4	1	2	0	3	1	2
Cap, veh/h	470	528	83	395	575	63	293	505	20	257	460	55
Arrive On Green	0.05	0.36	0.34	0.06	0.36	0.35	0.07	0.31	0.29	0.06	0.30	0.28
Sat Flow, veh/h	1785	1484	235	1750	1576	173	1653	1645	65	1669	1560	187
Grp Volume(v), veh/h	56	0	359	71	0	273	86	0	448	66	0	411
Grp Sat Flow(s),veh/h/ln	1785	0	1719	1750	0	1749	1653	0	1710	1669	0	1747
Q Serve(g_s), s	1.4	0.0	12.4	1.8	0.0	8.6	2.5	0.0	18.0	1.9	0.0	15.9
Cycle Q Clear(g_c), s	1.4	0.0	12.4	1.8	0.0	8.6	2.5	0.0	18.0	1.9	0.0	15.9
Prop In Lane	1.00		0.14	1.00		0.10	1.00		0.04	1.00		0.11
Lane Grp Cap(c), veh/h	470	0	612	395	0	638	293	0	525	257	0	515
V/C Ratio(X)	0.12	0.00	0.59	0.18	0.00	0.43	0.29	0.00	0.85	0.26	0.00	0.80
Avail Cap(c_a), veh/h	576	0	612	483	0	638	356	0	632	341	0	646
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	13.7	0.0	19.2	14.1	0.0	17.5	17.3	0.0	23.8	17.9	0.0	23.8
Incr Delay (d2), s/veh	0.1	0.0	4.1	0.2	0.0	2.1	0.6	0.0	9.4	0.5	0.0	5.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	1.0	0.0	9.3	1.3	0.0	6.6	1.7	0.0	13.0	1.4	0.0	11.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	13.8	0.0	23.3	14.3	0.0	19.6	17.8	0.0	33.2	18.4	0.0	29.4
LnGrp LOS	B	A	C	B	A	B	B	A	C	B	A	C
Approach Vol, veh/h	415			344			534			477		
Approach Delay, s/veh	22.0			18.5			30.7			27.8		
Approach LOS	C			B			C			C		
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	8.3	26.4	8.3	30.0	9.2	25.6	7.7	30.6				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	7.0	26.0	7.0	25.0	7.0	26.0	7.0	25.0				
Max Q Clear Time (g_c+l1), s	4.4	20.0	4.3	14.4	5.0	17.9	3.9	10.6				
Green Ext Time (p_c), s	0.0	1.5	0.0	1.7	0.0	1.7	0.0	1.4				
Intersection Summary												
HCM 6th Ctrl Delay	25.5											
HCM 6th LOS	C											

1: Main Street & Broad Street  
Existing Conditions

PNPG.00002  
Timing Plan: Weekday P.M. Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	54	298	47	68	236	26	83	414	16	63	352	42
Future Volume (vph)	54	298	47	68	236	26	83	414	16	63	352	42
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	10	12	12	10	11	11	10	12	12	10	11	11
Grade (%)		-2%			-1%			3%			1%	
Storage Length (ft)	280		0	100		0	100		0	0		265
Storage Lanes	1		0	1		0	1		0	1		1
Taper Length (ft)	25			25			25			25		
Right Turn on Red			No			No			No			No
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		581			338			365			982	
Travel Time (s)		15.8			9.2			10.0			26.8	
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	8%	0%	0%	4%	4%	1%	2%	0%	3%	1%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	3.0	11.0		3.0	11.0		3.0	10.0		3.0	10.0	
Minimum Split (s)	8.0	16.0		8.0	16.0		8.0	15.0		8.0	15.0	
Total Split (s)	12.0	30.0		12.0	30.0		12.0	31.0		12.0	31.0	
Total Split (%)	14.1%	35.3%		14.1%	35.3%		14.1%	36.5%		14.1%	36.5%	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag		Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	Max		None	Max		None	None		None	None	

Intersection Summary

Area Type: Other  
Cycle Length: 85  
Actuated Cycle Length: 77  
Natural Cycle: 60  
Control Type: Actuated-Uncoordinated

Splits and Phases: 1: Main Street & Broad Street

























1: Main Street & Broad Street  
Existing Conditions

PNPG.00002

Timing Plan: Weekday P.M. Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	54	298	47	68	236	26	83	414	16	63	352	42
Future Volume (veh/h)	54	298	47	68	236	26	83	414	16	63	352	42
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1875	1761	1875	1837	1780	1780	1736	1722	1750	1752	1780	1766
Adj Flow Rate, veh/h	56	310	49	71	246	27	86	431	17	66	367	44
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	0	8	0	0	4	4	1	2	0	3	1	2
Cap, veh/h	470	528	83	395	575	63	293	505	20	257	460	55
Arrive On Green	0.05	0.36	0.34	0.06	0.36	0.35	0.07	0.31	0.29	0.06	0.30	0.28
Sat Flow, veh/h	1785	1484	235	1750	1576	173	1653	1645	65	1669	1560	187
Grp Volume(v), veh/h	56	0	359	71	0	273	86	0	448	66	0	411
Grp Sat Flow(s), veh/h/ln	1785	0	1719	1750	0	1749	1653	0	1710	1669	0	1747
Q Serve(g_s), s	1.4	0.0	12.4	1.8	0.0	8.6	2.5	0.0	18.0	1.9	0.0	15.9
Cycle Q Clear(g_c), s	1.4	0.0	12.4	1.8	0.0	8.6	2.5	0.0	18.0	1.9	0.0	15.9
Prop In Lane	1.00		0.14	1.00		0.10	1.00		0.04	1.00		0.11
Lane Grp Cap(c), veh/h	470	0	612	395	0	638	293	0	525	257	0	515
V/C Ratio(X)	0.12	0.00	0.59	0.18	0.00	0.43	0.29	0.00	0.85	0.26	0.00	0.80
Avail Cap(c_a), veh/h	576	0	612	483	0	638	356	0	632	341	0	646
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	13.7	0.0	19.2	14.1	0.0	17.5	17.3	0.0	23.8	17.9	0.0	23.8
Incr Delay (d2), s/veh	0.1	0.0	4.1	0.2	0.0	2.1	0.6	0.0	9.4	0.5	0.0	5.6
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%), veh/ln	1.0	0.0	9.3	1.3	0.0	6.6	1.7	0.0	13.0	1.4	0.0	11.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	13.8	0.0	23.3	14.3	0.0	19.6	17.8	0.0	33.2	18.4	0.0	29.4
LnGrp LOS	B	A	C	B	A	B	B	A	C	B	A	C
Approach Vol, veh/h	415			344			534			477		
Approach Delay, s/veh	22.0			18.5			30.7			27.8		
Approach LOS	C			B			C			C		
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	8.3	26.4	8.3	30.0	9.2	25.6	7.7	30.6				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	7.0	26.0	7.0	25.0	7.0	26.0	7.0	25.0				
Max Q Clear Time (g_c+I1), s	4.4	20.0	4.3	14.4	5.0	17.9	3.9	10.6				
Green Ext Time (p_c), s	0.0	1.5	0.0	1.7	0.0	1.7	0.0	1.4				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay	25.5											
HCM 6th LOS	C											

## 2026 Base (No-Build) Conditions





1: Main Street & Broad Street  
2026 Base (No-Build) Conditions

PNPG.00002

Timing Plan: Weekday A.M. Peak Hour

	↖	→	↗	↖	←	↖	↗	↑	↗	↘	↓	↘
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (vph)	38	216	30	48	179	30	44	211	24	42	411	49
Future Volume (vph)	38	216	30	48	179	30	44	211	24	42	411	49
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	10	12	12	10	11	11	10	12	12	10	11	11
Grade (%)		-2%			-1%			3%			1%	
Storage Length (ft)	280		0	100		0	100		0	0		265
Storage Lanes	1		0	1		0	1		0	1		1
Taper Length (ft)	25			25			25			25		
Right Turn on Red			No			No			No			No
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		581			338			365			982	
Travel Time (s)		15.8			9.2			10.0			26.8	
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Heavy Vehicles (%)	0%	14%	20%	10%	14%	10%	9%	5%	21%	2%	3%	0%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	3.0	11.0		3.0	11.0		3.0	10.0		3.0	10.0	
Minimum Split (s)	8.0	16.0		8.0	16.0		8.0	15.0		8.0	15.0	
Total Split (s)	12.0	28.0		12.0	28.0		12.0	33.0		12.0	33.0	
Total Split (%)	14.1%	32.9%		14.1%	32.9%		14.1%	38.8%		14.1%	38.8%	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag		Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	Max		None	Max		None	None		None	None	

Intersection Summary

Area Type: Other

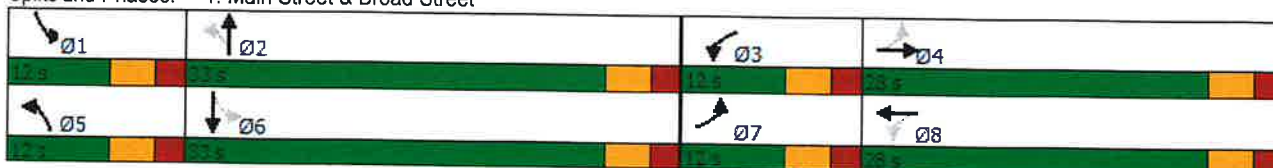
Cycle Length: 85

Actuated Cycle Length: 74.3

Natural Cycle: 60

Control Type: Actuated-Uncoordinated

Splits and Phases: 1: Main Street & Broad Street





## 2026 Base (No-Build) Conditions



1: Main Street & Broad Street  
2026 Base (No-Build) Conditions

PNPG.00002

Timing Plan: Weekday A.M. Peak Hour

	↖	→	↘	↙	←	↖	↙	↑	↗	↘	↓	↙
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↘		↖	↘		↖	↘		↖	↘	
Traffic Volume (vph)	38	216	30	48	179	30	44	211	24	42	411	49
Future Volume (vph)	38	216	30	48	179	30	44	211	24	42	411	49
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	10	12	12	10	11	11	10	12	12	10	11	11
Grade (%)		-2%			-1%			3%			1%	
Storage Length (ft)	280		0	100		0	100		0	0		265
Storage Lanes	1		0	1		0	1		0	1		1
Taper Length (ft)	25			25			25			25		
Right Turn on Red			No			No			No			No
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		581			338			365			982	
Travel Time (s)		15.8			9.2			10.0			26.8	
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Heavy Vehicles (%)	0%	14%	20%	10%	14%	10%	9%	5%	21%	2%	3%	0%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	3.0	11.0		3.0	11.0		3.0	10.0		3.0	10.0	
Minimum Split (s)	8.0	16.0		8.0	16.0		8.0	15.0		8.0	15.0	
Total Split (s)	12.0	28.0		12.0	28.0		12.0	33.0		12.0	33.0	
Total Split (%)	14.1%	32.9%		14.1%	32.9%		14.1%	38.8%		14.1%	38.8%	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag		Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	Max		None	Max		None	None		None	None	

Intersection Summary

Area Type: Other

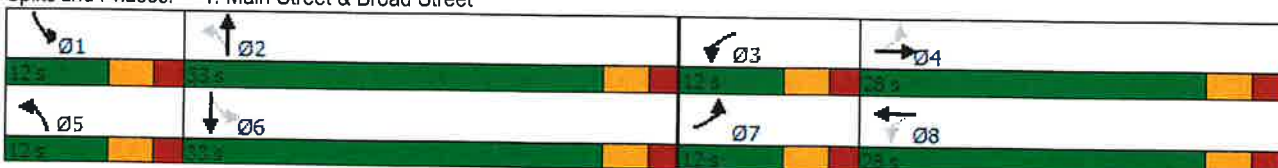
Cycle Length: 85

Actuated Cycle Length: 74.3

Natural Cycle: 60





















Control Type: Actuated-Uncoordinated

Splits and Phases: 1: Main Street & Broad Street



1: Main Street & Broad Street  
2026 Base (No-Build) Conditions

PNPG.00002  
Timing Plan: Weekday A.M. Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	38	216	30	48	179	30	44	211	24	42	411	49
Future Volume (veh/h)	38	216	30	48	179	30	44	211	24	42	411	49
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1875	1675	1590	1695	1638	1695	1623	1680	1455	1766	1752	1794
Adj Flow Rate, veh/h	45	257	36	57	213	36	52	251	27	50	489	58
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	0	14	20	10	14	10	9	5	21	2	3	0
Cap, veh/h	420	466	65	363	455	77	222	537	58	423	550	65
Arrive On Green	0.04	0.32	0.31	0.05	0.33	0.32	0.05	0.36	0.35	0.05	0.36	0.34
Sat Flow, veh/h	1785	1438	201	1614	1366	231	1546	1490	160	1682	1537	182
Grp Volume(v), veh/h	45	0	293	57	0	249	52	0	278	50	0	547
Grp Sat Flow(s),veh/h/ln	1785	0	1639	1614	0	1597	1546	0	1651	1682	0	1719
Q Serve(g_s), s	1.2	0.0	10.9	1.7	0.0	9.1	1.5	0.0	9.6	1.4	0.0	22.2
Cycle Q Clear(g_c), s	1.2	0.0	10.9	1.7	0.0	9.1	1.5	0.0	9.6	1.4	0.0	22.2
Prop In Lane	1.00		0.12	1.00		0.14	1.00		0.10	1.00		0.11
Lane Grp Cap(c), veh/h	420	0	531	363	0	531	222	0	595	423	0	615
V/C Ratio(X)	0.11	0.00	0.55	0.16	0.00	0.47	0.23	0.00	0.47	0.12	0.00	0.89
Avail Cap(c_a), veh/h	535	0	531	452	0	531	313	0	646	526	0	673
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	15.6	0.0	20.7	15.7	0.0	19.6	16.9	0.0	18.3	14.1	0.0	22.4
Incr Delay (d2), s/veh	0.1	0.0	4.1	0.2	0.0	2.9	0.5	0.0	0.6	0.1	0.0	13.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.9	0.0	8.2	1.1	0.0	6.7	1.0	0.0	6.5	0.9	0.0	16.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	15.7	0.0	24.8	15.9	0.0	22.5	17.4	0.0	18.8	14.2	0.0	35.5
LnGrp LOS	B	A	C	B	A	C	B	A	B	B	A	D
Approach Vol, veh/h	338			306			330			597		
Approach Delay, s/veh	23.6			21.3			18.6			33.8		
Approach LOS	C			C			B			C		
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	7.5	30.7	7.9	28.0	7.7	30.5	7.3	28.7				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	7.0	28.0	7.0	23.0	7.0	28.0	7.0	23.0				
Max Q Clear Time (g_c+I1), s	3.9	11.6	4.2	12.9	4.0	24.2	3.7	11.1				
Green Ext Time (p_c), s	0.0	1.5	0.0	1.3	0.0	1.3	0.0	1.1				
Intersection Summary												
HCM 6th Ctrl Delay	26.0											
HCM 6th LOS	C											



1: Main Street & Broad Street  
2026 Base (No-Build) Conditions

PNPG.00002

Timing Plan: Weekday P.M. Peak Hour

	↖	→	↘	↙	←	↖	↙	↑	↗	↘	↓	↙
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (vph)	54	300	47	68	237	26	83	416	16	63	354	42
Future Volume (vph)	54	300	47	68	237	26	83	416	16	63	354	42
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	10	12	12	10	11	11	10	12	12	10	11	11
Grade (%)		-2%			-1%			3%			1%	
Storage Length (ft)	280		0	100		0	100		0	0		265
Storage Lanes	1		0	1		0	1		0	1		1
Taper Length (ft)	25			25			25			25		
Right Turn on Red			No			No			No			No
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		581			338			365			982	
Travel Time (s)		15.8			9.2			10.0			26.8	
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	8%	0%	0%	4%	4%	1%	2%	0%	3%	1%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	3.0	11.0		3.0	11.0		3.0	10.0		3.0	10.0	
Minimum Split (s)	8.0	16.0		8.0	16.0		8.0	15.0		8.0	15.0	
Total Split (s)	12.0	30.0		12.0	30.0		12.0	31.0		12.0	31.0	
Total Split (%)	14.1%	35.3%		14.1%	35.3%		14.1%	36.5%		14.1%	36.5%	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag		Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	Max		None	Max		None	None		None	None	

Intersection Summary

Area Type: Other

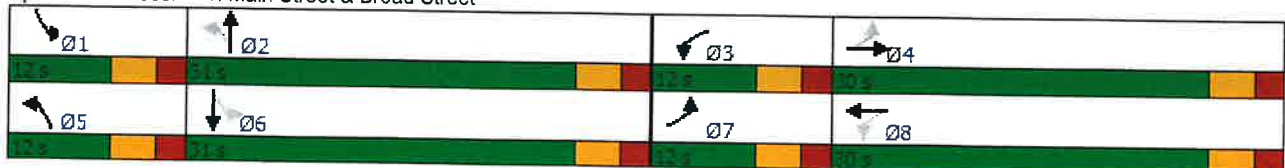
Cycle Length: 85

Actuated Cycle Length: 77.1

Natural Cycle: 60





















Control Type: Actuated-Uncoordinated

Splits and Phases: 1: Main Street & Broad Street



1: Main Street & Broad Street  
2026 Base (No-Build) Conditions

PNPG.00002  
Timing Plan: Weekday A.M. Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	38	216	30	48	179	30	44	211	24	42	411	49
Future Volume (veh/h)	38	216	30	48	179	30	44	211	24	42	411	49
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1875	1675	1590	1695	1638	1695	1623	1680	1455	1766	1752	1794
Adj Flow Rate, veh/h	45	257	36	57	213	36	52	251	27	50	489	58
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	0	14	20	10	14	10	9	5	21	2	3	0
Cap, veh/h	420	466	65	363	455	77	222	537	58	423	550	65
Arrive On Green	0.04	0.32	0.31	0.05	0.33	0.32	0.05	0.36	0.35	0.05	0.36	0.34
Sat Flow, veh/h	1785	1438	201	1614	1366	231	1546	1490	160	1682	1537	182
Grp Volume(v), veh/h	45	0	293	57	0	249	52	0	278	50	0	547
Grp Sat Flow(s),veh/h/ln	1785	0	1639	1614	0	1597	1546	0	1651	1682	0	1719
Q Serve(g_s), s	1.2	0.0	10.9	1.7	0.0	9.1	1.5	0.0	9.6	1.4	0.0	22.2
Cycle Q Clear(g_c), s	1.2	0.0	10.9	1.7	0.0	9.1	1.5	0.0	9.6	1.4	0.0	22.2
Prop In Lane	1.00		0.12	1.00		0.14	1.00		0.10	1.00		0.11
Lane Grp Cap(c), veh/h	420	0	531	363	0	531	222	0	595	423	0	615
V/C Ratio(X)	0.11	0.00	0.55	0.16	0.00	0.47	0.23	0.00	0.47	0.12	0.00	0.89
Avail Cap(c_a), veh/h	535	0	531	452	0	531	313	0	646	526	0	673
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	15.6	0.0	20.7	15.7	0.0	19.6	16.9	0.0	18.3	14.1	0.0	22.4
Incr Delay (d2), s/veh	0.1	0.0	4.1	0.2	0.0	2.9	0.5	0.0	0.6	0.1	0.0	13.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.9	0.0	8.2	1.1	0.0	6.7	1.0	0.0	6.5	0.9	0.0	16.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	15.7	0.0	24.8	15.9	0.0	22.5	17.4	0.0	18.8	14.2	0.0	35.5
LnGrp LOS	B	A	C	B	A	C	B	A	B	B	A	D
Approach Vol, veh/h	338			306			330			597		
Approach Delay, s/veh	23.6			21.3			18.6			33.8		
Approach LOS	C			C			B			C		
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	7.5	30.7	7.9	28.0	7.7	30.5	7.3	28.7				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	7.0	28.0	7.0	23.0	7.0	28.0	7.0	23.0				
Max Q Clear Time (g_c+1), s	3.9	11.6	4.2	12.9	4.0	24.2	3.7	11.1				
Green Ext Time (p_c), s	0.0	1.5	0.0	1.3	0.0	1.3	0.0	1.1				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay			26.0									
HCM 6th LOS			C									



1: Main Street & Broad Street  
2026 Base (No-Build) Conditions

PNPG.00002  
Timing Plan: Weekday P.M. Peak Hour

	↖	→	↘	↙	←	↖	↙	↑	↗	↘	↓	↙
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (vph)	54	300	47	68	237	26	83	416	16	63	354	42
Future Volume (vph)	54	300	47	68	237	26	83	416	16	63	354	42
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	10	12	12	10	11	11	10	12	12	10	11	11
Grade (%)		-2%			-1%			3%			1%	
Storage Length (ft)	280		0	100		0	100		0	0		265
Storage Lanes	1		0	1		0	1		0	1		1
Taper Length (ft)	25			25			25			25		
Right Turn on Red			No			No			No			No
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		581			338			365			982	
Travel Time (s)		15.8			9.2			10.0			26.8	
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	8%	0%	0%	4%	4%	1%	2%	0%	3%	1%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	3.0	11.0		3.0	11.0		3.0	10.0		3.0	10.0	
Minimum Split (s)	8.0	16.0		8.0	16.0		8.0	15.0		8.0	15.0	
Total Split (s)	12.0	30.0		12.0	30.0		12.0	31.0		12.0	31.0	
Total Split (%)	14.1%	35.3%		14.1%	35.3%		14.1%	36.5%		14.1%	36.5%	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag		Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	Max		None	Max		None	None		None	None	

Intersection Summary

Area Type: Other

Cycle Length: 85

Actuated Cycle Length: 77.1

Natural Cycle: 60





















Control Type: Actuated-Uncoordinated

Splits and Phases: 1: Main Street & Broad Street



1: Main Street & Broad Street  
2026 Base (No-Build) Conditions

PNPG.00002  
Timing Plan: Weekday P.M. Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	54	300	47	68	237	26	83	416	16	63	354	42
Future Volume (veh/h)	54	300	47	68	237	26	83	416	16	63	354	42
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1875	1761	1875	1837	1780	1780	1736	1722	1750	1752	1780	1766
Adj Flow Rate, veh/h	56	312	49	71	247	27	86	433	17	66	369	44
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	0	8	0	0	4	4	1	2	0	3	1	2
Cap, veh/h	468	528	83	393	574	63	292	507	20	256	462	55
Arrive On Green	0.05	0.36	0.34	0.06	0.36	0.35	0.07	0.31	0.29	0.06	0.30	0.28
Sat Flow, veh/h	1785	1485	233	1750	1577	172	1653	1645	65	1669	1561	186
Grp Volume(v), veh/h	56	0	361	71	0	274	86	0	450	66	0	413
Grp Sat Flow(s), veh/h/ln	1785	0	1719	1750	0	1749	1653	0	1710	1669	0	1747
Q Serve(g_s), s	1.4	0.0	12.6	1.8	0.0	8.7	2.5	0.0	18.1	1.9	0.0	16.0
Cycle Q Clear(g_c), s	1.4	0.0	12.6	1.8	0.0	8.7	2.5	0.0	18.1	1.9	0.0	16.0
Prop In Lane	1.00		0.14	1.00		0.10	1.00		0.04	1.00		0.11
Lane Grp Cap(c), veh/h	468	0	611	393	0	637	292	0	527	256	0	517
V/C Ratio(X)	0.12	0.00	0.59	0.18	0.00	0.43	0.29	0.00	0.85	0.26	0.00	0.80
Avail Cap(c_a), veh/h	574	0	611	481	0	637	356	0	631	341	0	645
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	13.8	0.0	19.3	14.1	0.0	17.6	17.3	0.0	23.8	17.9	0.0	23.8
Incr Delay (d2), s/veh	0.1	0.0	4.2	0.2	0.0	2.1	0.6	0.0	9.6	0.5	0.0	5.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	1.0	0.0	9.3	1.3	0.0	6.7	1.7	0.0	13.1	1.4	0.0	11.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	13.9	0.0	23.5	14.4	0.0	19.7	17.8	0.0	33.4	18.4	0.0	29.5
LnGrp LOS	B	A	C	B	A	B	B	A	C	B	A	C
Approach Vol, veh/h	417				345				536			
Approach Delay, s/veh	22.2				18.6				30.9			
Approach LOS	C				B				C			
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	8.3	26.5	8.3	30.0	9.2	25.7	7.7	30.6				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	7.0	26.0	7.0	25.0	7.0	26.0	7.0	25.0				
Max Q Clear Time (g_c+I1), s	4.4	20.1	4.3	14.6	5.0	18.0	3.9	10.7				
Green Ext Time (p_c), s	0.0	1.5	0.0	1.7	0.0	1.7	0.0	1.4				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay	25.7											
HCM 6th LOS	C											

## 2026 Projected (Build) Conditions

























1: Main Street & Broad Street  
2026 Base (No-Build) Conditions

PNPG.00002

Timing Plan: Weekday P.M. Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	54	300	47	68	237	26	83	416	16	63	354	42
Future Volume (veh/h)	54	300	47	68	237	26	83	416	16	63	354	42
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1875	1761	1875	1837	1780	1780	1736	1722	1750	1752	1780	1766
Adj Flow Rate, veh/h	56	312	49	71	247	27	86	433	17	66	369	44
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	0	8	0	0	4	4	1	2	0	3	1	2
Cap, veh/h	468	528	83	393	574	63	292	507	20	256	462	55
Arrive On Green	0.05	0.36	0.34	0.06	0.36	0.35	0.07	0.31	0.29	0.06	0.30	0.28
Sat Flow, veh/h	1785	1485	233	1750	1577	172	1653	1645	65	1669	1561	186
Grp Volume(v), veh/h	56	0	361	71	0	274	86	0	450	66	0	413
Grp Sat Flow(s),veh/h/ln	1785	0	1719	1750	0	1749	1653	0	1710	1669	0	1747
Q Serve(g_s), s	1.4	0.0	12.6	1.8	0.0	8.7	2.5	0.0	18.1	1.9	0.0	16.0
Cycle Q Clear(g_c), s	1.4	0.0	12.6	1.8	0.0	8.7	2.5	0.0	18.1	1.9	0.0	16.0
Prop In Lane	1.00		0.14	1.00		0.10	1.00		0.04	1.00		0.11
Lane Grp Cap(c), veh/h	468	0	611	393	0	637	292	0	527	256	0	517
V/C Ratio(X)	0.12	0.00	0.59	0.18	0.00	0.43	0.29	0.00	0.85	0.26	0.00	0.80
Avail Cap(c_a), veh/h	574	0	611	481	0	637	356	0	631	341	0	645
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	13.8	0.0	19.3	14.1	0.0	17.6	17.3	0.0	23.8	17.9	0.0	23.8
Incr Delay (d2), s/veh	0.1	0.0	4.2	0.2	0.0	2.1	0.6	0.0	9.6	0.5	0.0	5.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	1.0	0.0	9.3	1.3	0.0	6.7	1.7	0.0	13.1	1.4	0.0	11.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	13.9	0.0	23.5	14.4	0.0	19.7	17.8	0.0	33.4	18.4	0.0	29.5
LnGrp LOS	B	A	C	B	A	B	B	A	C	B	A	C
Approach Vol, veh/h	417			345			536			479		
Approach Delay, s/veh	22.2			18.6			30.9			27.9		
Approach LOS	C			B			C			C		
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	8.3	26.5	8.3	30.0	9.2	25.7	7.7	30.6				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	7.0	26.0	7.0	25.0	7.0	26.0	7.0	25.0				
Max Q Clear Time (g_c+I1), s	4.4	20.1	4.3	14.6	5.0	18.0	3.9	10.7				
Green Ext Time (p_c), s	0.0	1.5	0.0	1.7	0.0	1.7	0.0	1.4				
Intersection Summary												
HCM 6th Ctrl Delay	25.7											
HCM 6th LOS	C											

## 2026 Projected (Build) Conditions





1: Main Street & Broad Street  
2026 Projected (Build) Conditions

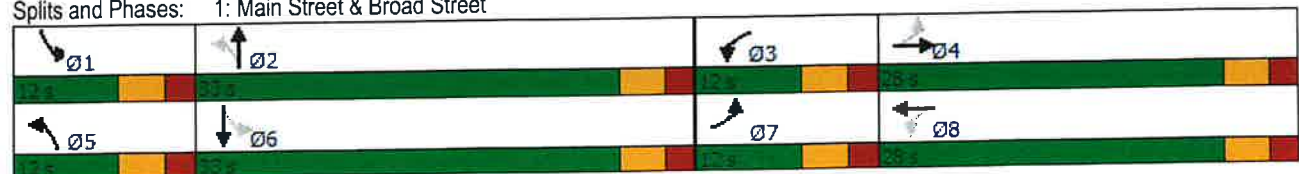
PNPG.00002  
Timing Plan: Weekday A.M. Peak Hour

	↖	→	↘	↙	←	↖	↙	↑	↗	↘	↓	↙
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (vph)	38	216	30	48	179	30	44	212	24	42	412	50
Future Volume (vph)	38	216	30	48	179	30	44	212	24	42	412	50
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	10	12	12	10	11	11	10	12	12	10	11	11
Grade (%)		-2%			-1%			3%			1%	
Storage Length (ft)	280		0	100		0	100		0	0		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			No			No			No			No
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		581			338			365			187	
Travel Time (s)		15.8			9.2			10.0			5.1	
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Heavy Vehicles (%)	0%	14%	20%	10%	14%	10%	9%	5%	21%	2%	3%	0%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	3.0	11.0		3.0	11.0		3.0	10.0		3.0	10.0	
Minimum Split (s)	8.0	16.0		8.0	16.0		8.0	15.0		8.0	15.0	
Total Split (s)	12.0	28.0		12.0	28.0		12.0	33.0		12.0	33.0	
Total Split (%)	14.1%	32.9%		14.1%	32.9%		14.1%	38.8%		14.1%	38.8%	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag		Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	Max		None	Max		None	None		None	None	

Intersection Summary

Area Type: Other  
Cycle Length: 85  
Actuated Cycle Length: 74.5  
Natural Cycle: 60  
Control Type: Actuated-Uncoordinated





















Splits and Phases: 1: Main Street & Broad Street



1: Main Street & Broad Street  
2026 Projected (Build) Conditions

PNPG.00002

Timing Plan: Weekday A.M. Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	38	216	30	48	179	30	44	212	24	42	412	50
Future Volume (veh/h)	38	216	30	48	179	30	44	212	24	42	412	50
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1875	1675	1590	1695	1638	1695	1623	1680	1455	1766	1752	1794
Adj Flow Rate, veh/h	45	257	36	57	213	36	52	252	27	50	490	60
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	0	14	20	10	14	10	9	5	21	2	3	0
Cap, veh/h	419	465	65	362	454	77	221	539	58	424	550	67
Arrive On Green	0.04	0.32	0.31	0.05	0.33	0.32	0.05	0.36	0.35	0.05	0.36	0.35
Sat Flow, veh/h	1785	1438	201	1614	1366	231	1546	1491	160	1682	1531	187
Grp Volume(v), veh/h	45	0	293	57	0	249	52	0	279	50	0	550
Grp Sat Flow(s),veh/h/ln	1785	0	1639	1614	0	1597	1546	0	1651	1682	0	1719
Q Serve(g_s), s	1.2	0.0	10.9	1.7	0.0	9.2	1.5	0.0	9.7	1.4	0.0	22.4
Cycle Q Clear(g_c), s	1.2	0.0	10.9	1.7	0.0	9.2	1.5	0.0	9.7	1.4	0.0	22.4
Prop In Lane	1.00		0.12	1.00		0.14	1.00		0.10	1.00		0.11
Lane Grp Cap(c), veh/h	419	0	530	362	0	530	221	0	597	424	0	617
V/C Ratio(X)	0.11	0.00	0.55	0.16	0.00	0.47	0.24	0.00	0.47	0.12	0.00	0.89
Avail Cap(c_a), veh/h	533	0	530	451	0	530	311	0	645	526	0	671
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	15.7	0.0	20.8	15.8	0.0	19.7	16.9	0.0	18.3	14.1	0.0	22.5
Incr Delay (d2), s/veh	0.1	0.0	4.1	0.2	0.0	3.0	0.5	0.0	0.6	0.1	0.0	13.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.9	0.0	8.2	1.1	0.0	6.7	1.0	0.0	6.5	0.9	0.0	16.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	15.8	0.0	24.9	16.0	0.0	22.6	17.5	0.0	18.8	14.2	0.0	35.9
LnGrp LOS	B	A	C	B	A	C	B	A	B	B	A	D
Approach Vol, veh/h	338			306			331			600		
Approach Delay, s/veh	23.7			21.4			18.6			34.1		
Approach LOS	C			C			B			C		
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	7.5	30.8	7.9	28.0	7.7	30.7	7.3	28.7				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	7.0	28.0	7.0	23.0	7.0	28.0	7.0	23.0				
Max Q Clear Time (g_c+1), s	3.9	11.7	4.2	12.9	4.0	24.4	3.7	11.2				
Green Ext Time (p_c), s	0.0	1.5	0.0	1.3	0.0	1.3	0.0	1.1				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay	26.1											
HCM 6th LOS	C											



1: Main Street & Broad Street  
2026 Projected (Build) Conditions

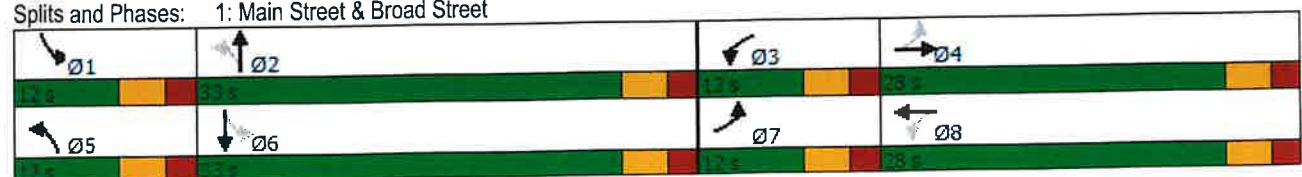
PNPG.00002  
Timing Plan: Weekday A.M. Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	38	216	30	48	179	30	44	212	24	42	412	50
Future Volume (vph)	38	216	30	48	179	30	44	212	24	42	412	50
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	10	12	12	10	11	11	10	12	12	10	11	11
Grade (%)		-2%			-1%			3%			1%	
Storage Length (ft)	280		0	100		0	100		0	0		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			No			No			No			No
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		581			338			365			187	
Travel Time (s)		15.8			9.2			10.0			5.1	
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Heavy Vehicles (%)	0%	14%	20%	10%	14%	10%	9%	5%	21%	2%	3%	0%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	3.0	11.0		3.0	11.0		3.0	10.0		3.0	10.0	
Minimum Split (s)	8.0	16.0		8.0	16.0		8.0	15.0		8.0	15.0	
Total Split (s)	12.0	28.0		12.0	28.0		12.0	33.0		12.0	33.0	
Total Split (%)	14.1%	32.9%		14.1%	32.9%		14.1%	38.8%		14.1%	38.8%	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag		Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	Max		None	Max		None	None		None	None	

Intersection Summary

Area Type: Other  
Cycle Length: 85  
Actuated Cycle Length: 74.5  
Natural Cycle: 60  
Control Type: Actuated-Uncoordinated

Splits and Phases: 1: Main Street & Broad Street



1: Main Street & Broad Street  
2026 Projected (Build) Conditions

PNPG.00002










Timing Plan: Weekday A.M. Peak Hour

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	38	216	30	48	179	30	44	212	24	42	412	50
Future Volume (veh/h)	38	216	30	48	179	30	44	212	24	42	412	50
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1875	1675	1590	1695	1638	1695	1623	1680	1455	1766	1752	1794
Adj Flow Rate, veh/h	45	257	36	57	213	36	52	252	27	50	490	60
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	0	14	20	10	14	10	9	5	21	2	3	0
Cap, veh/h	419	465	65	362	454	77	221	539	58	424	550	67
Arrive On Green	0.04	0.32	0.31	0.05	0.33	0.32	0.05	0.36	0.35	0.05	0.36	0.35
Sat Flow, veh/h	1785	1438	201	1614	1366	231	1546	1491	160	1682	1531	187
Grp Volume(v), veh/h	45	0	293	57	0	249	52	0	279	50	0	550
Grp Sat Flow(s), veh/h/ln	1785	0	1639	1614	0	1597	1546	0	1651	1682	0	1719
Q Serve(g_s), s	1.2	0.0	10.9	1.7	0.0	9.2	1.5	0.0	9.7	1.4	0.0	22.4
Cycle Q Clear(g_c), s	1.2	0.0	10.9	1.7	0.0	9.2	1.5	0.0	9.7	1.4	0.0	22.4
Prop In Lane	1.00		0.12	1.00		0.14	1.00		0.10	1.00		0.11
Lane Grp Cap(c), veh/h	419	0	530	362	0	530	221	0	597	424	0	617
V/C Ratio(X)	0.11	0.00	0.55	0.16	0.00	0.47	0.24	0.00	0.47	0.12	0.00	0.89
Avail Cap(c_a), veh/h	533	0	530	451	0	530	311	0	645	526	0	671
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	15.7	0.0	20.8	15.8	0.0	19.7	16.9	0.0	18.3	14.1	0.0	22.5
Incr Delay (d2), s/veh	0.1	0.0	4.1	0.2	0.0	3.0	0.5	0.0	0.6	0.1	0.0	13.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.9	0.0	8.2	1.1	0.0	6.7	1.0	0.0	6.5	0.9	0.0	16.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	15.8	0.0	24.9	16.0	0.0	22.6	17.5	0.0	18.8	14.2	0.0	35.9
LnGrp LOS	B	A	C	B	A	C	B	A	B	B	A	D
Approach Vol, veh/h	338			306			331			600		
Approach Delay, s/veh	23.7			21.4			18.6			34.1		
Approach LOS	C			C			B			C		
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	7.5	30.8	7.9	28.0	7.7	30.7	7.3	28.7				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	7.0	28.0	7.0	23.0	7.0	28.0	7.0	23.0				
Max Q Clear Time (g_c+l1), s	3.9	11.7	4.2	12.9	4.0	24.4	3.7	11.2				
Green Ext Time (p_c), s	0.0	1.5	0.0	1.3	0.0	1.3	0.0	1.1				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay	26.1											
HCM 6th LOS	C											



2: Main Street & Site Driveway  
2026 Projected (Build) Conditions

PNPG.00002  
Timing Plan: Weekday A.M. Peak Hour

						
Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (vph)	2	1	279	1	0	502
Future Volume (vph)	2	1	279	1	0	502
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	10	10	10	11
Grade (%)	0%		-1%			1%
Link Speed (mph)	25		25			25
Link Distance (ft)	200		187			130
Travel Time (s)	5.5		5.1			3.5
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	5%	2%	2%	2%
Shared Lane Traffic (%)						
Sign Control	Stop		Free		Free	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					



2: Main Street & Site Driveway  
2026 Projected (Build) Conditions

PNPG.00002  
Timing Plan: Weekday A.M. Peak Hour

Intersection						
Int Delay, s/veh	0					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W	W	T	T	T	T
Traffic Vol, veh/h	2	1	279	1	0	502
Future Vol, veh/h	2	1	279	1	0	502
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	-1	-	-	1
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	5	2	2	2
Mvmt Flow	2	1	310	1	0	558

Major/Minor	Minor1	Major1	Major2
Conflicting Flow All	590	311	0
Stage 1	311	-	-
Stage 2	279	-	-
Critical Hdwy	6.4	6.23	4.3
Critical Hdwy Stg 1	5.43	-	-
Critical Hdwy Stg 2	5.83	-	-
Follow-up Hdwy	3	3.1	3
Pot Cap-1 Maneuver	532	773	940
Stage 1	852	-	-
Stage 2	856	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	532	773	940
Mov Cap-2 Maneuver	532	-	-
Stage 1	852	-	-
Stage 2	856	-	-





Approach	WB	NB	SB
HCM Control Delay, s	11.1	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	594	940
HCM Lane V/C Ratio	-	-	0.006	-
HCM Control Delay (s)	-	-	11.1	0
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0	0

2: Main Street & Site Driveway  
2026 Projected (Build) Conditions

PNPG.00002  
Timing Plan: Weekday A.M. Peak Hour

						
Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (vph)	2	1	279	1	0	502
Future Volume (vph)	2	1	279	1	0	502
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	10	10	10	11
Grade (%)	0%		-1%			1%
Link Speed (mph)	25		25			25
Link Distance (ft)	200		187			130
Travel Time (s)	5.5		5.1			3.5
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	5%	2%	2%	2%
Shared Lane Traffic (%)						
Sign Control	Stop		Free			Free
<b>Intersection Summary</b>						
Area Type:	Other					
Control Type:	Unsignalized					

2: Main Street & Site Driveway  
2026 Projected (Build) Conditions


PNPG.00002  
Timing Plan: Weekday A.M. Peak Hour

Intersection						
Int Delay, s/veh	0					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W	R	T	R	S	T
Traffic Vol, veh/h	2	1	279	1	0	502
Future Vol, veh/h	2	1	279	1	0	502
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	-1	-	-	1
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	5	2	2	2
Mvmt Flow	2	1	310	1	0	558
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	590	311	0	0	311	0
Stage 1	311	-	-	-	-	-
Stage 2	279	-	-	-	-	-
Critical Hdwy	6.4	6.23	-	-	4.3	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3	3.1	-	-	3	-
Pot Cap-1 Maneuver	532	773	-	-	940	-
Stage 1	852	-	-	-	-	-
Stage 2	856	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	532	773	-	-	940	-
Mov Cap-2 Maneuver	532	-	-	-	-	-
Stage 1	852	-	-	-	-	-
Stage 2	856	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	11.1	0	0			
HCM LOS	B					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	594	940	-	
HCM Lane V/C Ratio	-	-	0.006	-	-	
HCM Control Delay (s)	-	-	11.1	0	-	
HCM Lane LOS	-	-	B	A	-	
HCM 95th %tile Q(veh)	-	-	0	0	-	



1: Main Street & Broad Street  
2026 Projected (Build) Conditions

PNPG.00002  
Timing Plan: Weekday P.M. Peak Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	55	300	47	68	237	26	83	417	16	63	355	42
Future Volume (vph)	55	300	47	68	237	26	83	417	16	63	355	42
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	10	12	12	10	11	11	10	12	12	10	11	11
Grade (%)		-2%			-1%			3%			1%	
Storage Length (ft)	280		0	100		0	100		0	0		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			No			No			No			No
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		581			338			365			187	
Travel Time (s)		15.8			9.2			10.0			5.1	
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	8%	0%	0%	4%	4%	1%	2%	0%	3%	1%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	3.0	11.0		3.0	11.0		3.0	10.0		3.0	10.0	
Minimum Split (s)	8.0	16.0		8.0	16.0		8.0	15.0		8.0	15.0	
Total Split (s)	12.0	30.0		12.0	30.0		12.0	31.0		12.0	31.0	
Total Split (%)	14.1%	35.3%		14.1%	35.3%		14.1%	36.5%		14.1%	36.5%	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag		Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	Max		None	Max		None	None		None	None	

Intersection Summary

Area Type: Other  
Cycle Length: 85  
Actuated Cycle Length: 77.1  
Natural Cycle: 60  
Control Type: Actuated-Uncoordinated

Splits and Phases: 1: Main Street & Broad Street





1: Main Street & Broad Street  
2026 Projected (Build) Conditions

PNPG.00002

Timing Plan: Weekday P.M. Peak Hour

	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Movement												
Lane Configurations	↰	↱		↰	↱		↰	↱		↰	↱	
Traffic Volume (veh/h)	55	300	47	68	237	26	83	417	16	63	355	42
Future Volume (veh/h)	55	300	47	68	237	26	83	417	16	63	355	42
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No				No			No			No	
Adj Sat Flow, veh/h/ln	1875	1761	1875	1837	1780	1780	1736	1722	1750	1752	1780	1766
Adj Flow Rate, veh/h	57	312	49	71	247	27	86	434	17	66	370	44
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	0	8	0	0	4	4	1	2	0	3	1	2
Cap, veh/h	468	528	83	392	573	63	292	508	20	256	463	55
Arrive On Green	0.05	0.36	0.34	0.06	0.36	0.35	0.07	0.31	0.29	0.06	0.30	0.28
Sat Flow, veh/h	1785	1485	233	1750	1577	172	1653	1646	64	1669	1561	186
Grp Volume(v), veh/h	57	0	361	71	0	274	86	0	451	66	0	414
Grp Sat Flow(s), veh/h/ln	1785	0	1719	1750	0	1749	1653	0	1710	1669	0	1747
Q Serve(g_s), s	1.4	0.0	12.6	1.8	0.0	8.7	2.5	0.0	18.1	1.9	0.0	16.0
Cycle Q Clear(g_c), s	1.4	0.0	12.6	1.8	0.0	8.7	2.5	0.0	18.1	1.9	0.0	16.0
Prop In Lane	1.00		0.14	1.00		0.10	1.00		0.04	1.00		0.11
Lane Grp Cap(c), veh/h	468	0	610	392	0	636	292	0	528	256	0	518
V/C Ratio(X)	0.12	0.00	0.59	0.18	0.00	0.43	0.29	0.00	0.85	0.26	0.00	0.80
Avail Cap(c_a), veh/h	572	0	610	480	0	636	355	0	631	340	0	644
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	13.8	0.0	19.3	14.2	0.0	17.6	17.3	0.0	23.8	17.9	0.0	23.8
Incr Delay (d2), s/veh	0.1	0.0	4.2	0.2	0.0	2.1	0.6	0.0	9.7	0.5	0.0	5.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	1.0	0.0	9.4	1.3	0.0	6.7	1.7	0.0	13.2	1.4	0.0	11.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	13.9	0.0	23.5	14.4	0.0	19.8	17.8	0.0	33.4	18.4	0.0	29.5
LnGrp LOS	B	A	C	B	A	B	B	A	C	B	A	C
Approach Vol, veh/h		418			345			537			480	
Approach Delay, s/veh		22.2			18.7			30.9			28.0	
Approach LOS		C			B			C			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	8.3	26.6	8.3	30.0	9.2	25.7	7.7	30.6				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	7.0	26.0	7.0	25.0	7.0	26.0	7.0	25.0				
Max Q Clear Time (g_c+I1), s	4.4	20.1	4.3	14.6	5.0	18.0	3.9	10.7				
Green Ext Time (p_c), s	0.0	1.5	0.0	1.7	0.0	1.6	0.0	1.4				
Intersection Summary												
HCM 6th Ctrl Delay			25.7									
HCM 6th LOS			C									



1: Main Street & Broad Street  
2026 Projected (Build) Conditions

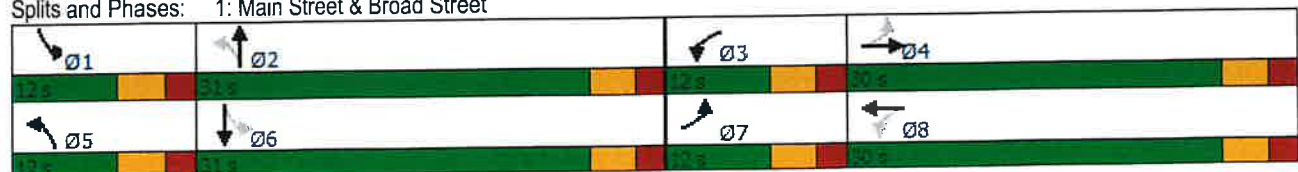
PNPG.00002  
Timing Plan: Weekday P.M. Peak Hour

	↖	→	↘	↙	←	↖	↙	↑	↗	↘	↓	↙
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↘		↖	↘		↖	↘		↖	↘	
Traffic Volume (vph)	55	300	47	68	237	26	83	417	16	63	355	42
Future Volume (vph)	55	300	47	68	237	26	83	417	16	63	355	42
Ideal Flow (vphpl)	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800	1800
Lane Width (ft)	10	12	12	10	11	11	10	12	12	10	11	11
Grade (%)		-2%			-1%			3%			1%	
Storage Length (ft)	280		0	100		0	100		0	0		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			No			No			No			No
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		581			338			365			187	
Travel Time (s)		15.8			9.2			10.0			5.1	
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	8%	0%	0%	4%	4%	1%	2%	0%	3%	1%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	3.0	11.0		3.0	11.0		3.0	10.0		3.0	10.0	
Minimum Split (s)	8.0	16.0		8.0	16.0		8.0	15.0		8.0	15.0	
Total Split (s)	12.0	30.0		12.0	30.0		12.0	31.0		12.0	31.0	
Total Split (%)	14.1%	35.3%		14.1%	35.3%		14.1%	36.5%		14.1%	36.5%	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	-1.0	-1.0		-1.0	-1.0		-1.0	-1.0		-1.0	-1.0	
Total Lost Time (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag		Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	Max		None	Max		None	None		None	None	

Intersection Summary

Area Type: Other  
Cycle Length: 85  
Actuated Cycle Length: 77.1  
Natural Cycle: 60  
Control Type: Actuated-Uncoordinated





















Splits and Phases: 1: Main Street & Broad Street



1: Main Street & Broad Street  
2026 Projected (Build) Conditions

PNPG.00002










Timing Plan: Weekday P.M. Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	55	300	47	68	237	26	83	417	16	63	355	42
Future Volume (veh/h)	55	300	47	68	237	26	83	417	16	63	355	42
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1875	1761	1875	1837	1780	1780	1736	1722	1750	1752	1780	1766
Adj Flow Rate, veh/h	57	312	49	71	247	27	86	434	17	66	370	44
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	0	8	0	0	4	4	1	2	0	3	1	2
Cap, veh/h	468	528	83	392	573	63	292	508	20	256	463	55
Arrive On Green	0.05	0.36	0.34	0.06	0.36	0.35	0.07	0.31	0.29	0.06	0.30	0.28
Sat Flow, veh/h	1785	1485	233	1750	1577	172	1653	1646	64	1669	1561	186
Grp Volume(v), veh/h	57	0	361	71	0	274	86	0	451	66	0	414
Grp Sat Flow(s),veh/h/ln	1785	0	1719	1750	0	1749	1653	0	1710	1669	0	1747
Q Serve(g_s), s	1.4	0.0	12.6	1.8	0.0	8.7	2.5	0.0	18.1	1.9	0.0	16.0
Cycle Q Clear(g_c), s	1.4	0.0	12.6	1.8	0.0	8.7	2.5	0.0	18.1	1.9	0.0	16.0
Prop In Lane	1.00		0.14	1.00		0.10	1.00		0.04	1.00		0.11
Lane Grp Cap(c), veh/h	468	0	610	392	0	636	292	0	528	256	0	518
V/C Ratio(X)	0.12	0.00	0.59	0.18	0.00	0.43	0.29	0.00	0.85	0.26	0.00	0.80
Avail Cap(c_a), veh/h	572	0	610	480	0	636	355	0	631	340	0	644
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	13.8	0.0	19.3	14.2	0.0	17.6	17.3	0.0	23.8	17.9	0.0	23.8
Incr Delay (d2), s/veh	0.1	0.0	4.2	0.2	0.0	2.1	0.6	0.0	9.7	0.5	0.0	5.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	1.0	0.0	9.4	1.3	0.0	6.7	1.7	0.0	13.2	1.4	0.0	11.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	13.9	0.0	23.5	14.4	0.0	19.8	17.8	0.0	33.4	18.4	0.0	29.5
LnGrp LOS	B	A	C	B	A	B	B	A	C	B	A	C
Approach Vol, veh/h	418			345			537			480		
Approach Delay, s/veh	22.2			18.7			30.9			28.0		
Approach LOS	C			B			C			C		
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	8.3	26.6	8.3	30.0	9.2	25.7	7.7	30.6				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	7.0	26.0	7.0	25.0	7.0	26.0	7.0	25.0				
Max Q Clear Time (g_c+I1), s	4.4	20.1	4.3	14.6	5.0	18.0	3.9	10.7				
Green Ext Time (p_c), s	0.0	1.5	0.0	1.7	0.0	1.6	0.0	1.4				
Intersection Summary												
HCM 6th Ctrl Delay	25.7											
HCM 6th LOS	C											



2: Main Street & Site Driveway  
2026 Projected (Build) Conditions

PNPG.00002  
Timing Plan: Weekday P.M. Peak Hour

						
Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (vph)	1	1	496	2	1	459
Future Volume (vph)	1	1	496	2	1	459
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	10	10	10	11
Grade (%)	0%		-1%			1%
Link Speed (mph)	25		25			25
Link Distance (ft)	200		187			130
Travel Time (s)	5.5		5.1			3.5
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	1%	2%	2%	2%
Shared Lane Traffic (%)						
Sign Control	Stop		Free			Free
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

2: Main Street & Site Driveway  
2026 Projected (Build) Conditions

PNPG.00002  
Timing Plan: Weekday P.M. Peak Hour

Intersection						
Int Delay, s/veh	0					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	1	1	496	2	1	459
Future Vol, veh/h	1	1	496	2	1	459
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	-1	-	-	1
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	1	2	2	2
Mvmt Flow	1	1	551	2	1	510

Major/Minor	Minor1	Major1	Major2
Conflicting Flow All	809	552	0
Stage 1	552	-	-
Stage 2	257	-	-
Critical Hdwy	6.4	6.23	4.3
Critical Hdwy Stg 1	5.43	-	-
Critical Hdwy Stg 2	5.83	-	-
Follow-up Hdwy	3	3.1	3
Pot Cap-1 Maneuver	392	561	774
Stage 1	651	-	-
Stage 2	879	-	-
Platoon blocked, %		-	-
Mov Cap-1 Maneuver	391	561	774
Mov Cap-2 Maneuver	391	-	-
Stage 1	651	-	-
Stage 2	877	-	-

Approach	WB	NB	SB
HCM Control Delay, s	12.8	0	0
HCM LOS	B		











  

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	461	774
HCM Lane V/C Ratio	-	-	0.005	0.001
HCM Control Delay (s)	-	-	12.8	9.7
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0	0



2: Main Street & Site Driveway  
2026 Projected (Build) Conditions

PNPG.00002  
Timing Plan: Weekday P.M. Peak Hour

						
Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						 
Traffic Volume (vph)	1	1	496	2	1	459
Future Volume (vph)	1	1	496	2	1	459
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	10	10	10	11
Grade (%)	0%		-1%			1%
Link Speed (mph)	25		25			25
Link Distance (ft)	200		187			130
Travel Time (s)	5.5		5.1			3.5
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	1%	2%	2%	2%
Shared Lane Traffic (%)						
Sign Control	Stop		Free			Free
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

2: Main Street & Site Driveway  
2026 Projected (Build) Conditions

PNPG.00002  
Timing Plan: Weekday P.M. Peak Hour

Intersection						
Int Delay, s/veh	0					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	Y		T			↑↑
Traffic Vol, veh/h	1	1	496	2	1	459
Future Vol, veh/h	1	1	496	2	1	459
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	-1	-	-	1
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	1	2	2	2
Mvmt Flow	1	1	551	2	1	510

Major/Minor	Minor1	Major1	Major2
Conflicting Flow All	809	552	0
Stage 1	552	-	-
Stage 2	257	-	-
Critical Hdwy	6.4	6.23	4.3
Critical Hdwy Stg 1	5.43	-	-
Critical Hdwy Stg 2	5.83	-	-
Follow-up Hdwy	3	3.1	3
Pot Cap-1 Maneuver	392	561	774
Stage 1	651	-	-
Stage 2	879	-	-
Platoon blocked, %		-	-
Mov Cap-1 Maneuver	391	561	774
Mov Cap-2 Maneuver	391	-	-
Stage 1	651	-	-
Stage 2	877	-	-

Approach	WB	NB	SB
HCM Control Delay, s	12.8	0	0
HCM LOS	B		

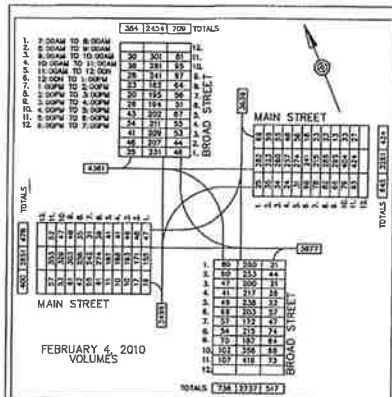
  

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	461	774	-
HCM Lane V/C Ratio	-	0.005	0.001	-
HCM Control Delay (s)	-	12.8	9.7	0
HCM Lane LOS	-	B	A	A
HCM 95th %tile Q(veh)	-	0	0	-

# **APPENDIX F:**

## **PennDOT-Approved Signal Plan**

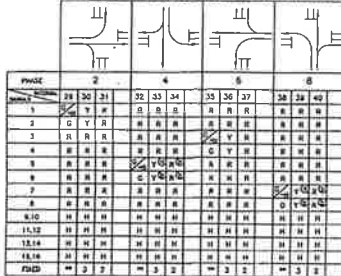




### EMERGENCY PRE-EMPTION NOTES:

- CONTROLLER TO BE EQUIPPED WITH EMERGENCY PRE-EMPTION FOR THE NORTHBOUND & SOUTHBOUND APPROACHES OF BROAD STREET AND THE EASTBOUND & WESTBOUND APPROACHES OF MAIN STREET WITH A P.A. SILENCE FOR EACH DIRECTION OF OPERATION.
- THIS EMERGENCY BEACON SHALL CONSIST OF A FLASHING WHITE FLOOD LIGHT, AND SHALL FLASH WHEN THE EMERGENCY VEHICLE HAS CONTROL OF THE INTERSECTION FOR THE APPROPRIATE APPROACH.
- LOCATION OF EMERGENCY VEHICLE DETECTORS ARE TO BE FIELD ADJUSTED TO ACHIEVE MAXIMUM OPERATION.
- THE SIGNALS, WHEN ACTIVATED BY AN EMERGENCY VEHICLE, SHALL TERMINATE ALL GREEN INDICATIONS IMMEDIATELY, FOLLOWED BY THE COMPLETE YELLOW AND RED CLEARANCE INTERVALS, ACCORDINGLY, THEN THE GREEN INTERVAL FOR THE PRE-EMPTED PHASE SHALL FOLLOW.
- THE SIGNALS, WHEN ACTIVATED BY AN EMERGENCY VEHICLE, SHALL TIME OUT ALL YELLOW AND RED INDICATIONS, FOLLOWED BY THE GREEN INTERVAL OF THE PRE-EMPTION PHASE COVERED BY THE APPROACHING EMERGENCY VEHICLE.
- IF THE SIGNAL HAS BEEN ACTIVATED BY A PEDESTRIAN PUSH BUTTON AND THE SIGNAL IS PRE-EMPTED DURING THE "MAIN" PHASE, THE MAIN PHASE SHALL TERMINATE IMMEDIATELY FOLLOWED BY THE "FLASHING DON'T WALK" INDICATION IN ITS ENTIRETY, FOLLOWED BY THE APPROPRIATE SELECTIVE CLEARANCES BEFORE PROCEEDING TO THE PRE-EMPTION PHASE.
- IF THE SIGNALS, WHEN ACTIVATED BY AN EMERGENCY VEHICLE, ARE FLASHING ALL SIGNALS SHALL REMAIN FLASHING.
- IF ADDITIONAL PRE-EMPTION PHASES ARE ACTIVATED WHILE IN PRE-EMPTION, THE ORIGINAL PRE-EMPTION PHASE SHALL TIME OUT BEFORE PROCEEDING TO THE NEXT PRE-EMPTION PHASE.
- UPON COMPLETION OF PRE-EMPTION, PHASE 2+6 OR 8 IN RETURNING TO NORMAL OPERATION, PHASE 2+6 INTERVAL 12 SHALL FOLLOW.
- IN EMERGENCY PRE-EMPTION, NO PRIORITY SHALL BE ESTABLISHED. PRE-EMPTION SHALL BE A "FIRST COME, FIRST SERVED" OPERATION.

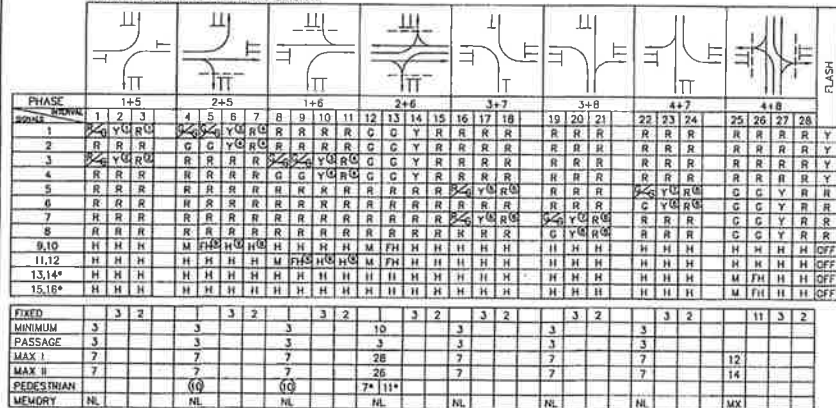
### EMERGENCY PRE-EMPTION PHASING MOVEMENT, SEQUENCE AND TIMING DIAGRAM



\*\* FOR DURATION OF PRE-EMPTION

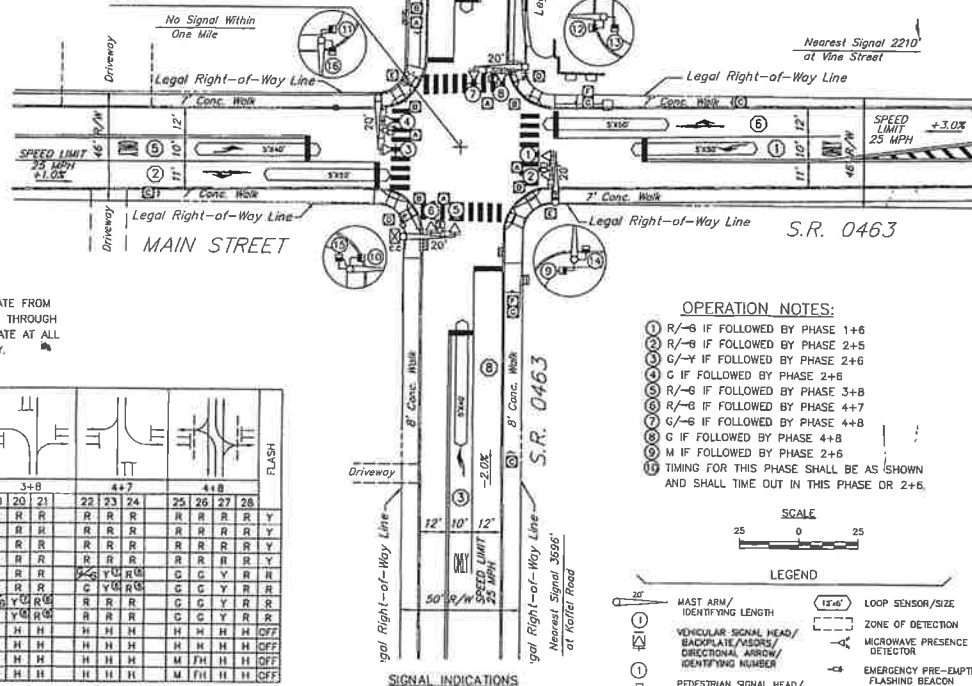
- NOTE:  
IF PRE-EMPTION EQUIPMENT HAS ENCODING CAPABILITIES FOR VEHICLE IDENTIFICATION, IT IS RECOMMENDED TO HAVE THE ZERO "00" FEATURE ON TO GIVE UNCODED DRIVERS THE ABILITY TO ACTIVATE THE EMERGENCY PRE-EMPTION.  
① G/F WHEN RETURNING TO NORMAL OPERATION  
② G WHEN RETURNING TO NORMAL OPERATION

### MOVEMENT, SEQUENCE AND TIMING DIAGRAM



\*UPON PEDESTRIAN ACTUATION ONLY. OTHERWISE HAND SYMBOL AT ALL TIMES.

S.R. 0463 SEG. 0200 OFF. 0000  
S.R. 1003 SEG. 0010 OFF. 0000



### OPERATION NOTES:

- R/-B IF FOLLOWED BY PHASE 1+6
  - R/-B IF FOLLOWED BY PHASE 2+6
  - G/-Y IF FOLLOWED BY PHASE 2+6
  - G IF FOLLOWED BY PHASE 2+6
  - R/-B IF FOLLOWED BY PHASE 3+8
  - R/-B IF FOLLOWED BY PHASE 4+7
  - G/-B IF FOLLOWED BY PHASE 4+8
  - G IF FOLLOWED BY PHASE 4+8
  - M IF FOLLOWED BY PHASE 2+6
- TIMING FOR THIS PHASE SHALL BE AS SHOWN AND SHALL TIME OUT IN THIS PHASE OR 2+6.

SCALE

25 0 25

### LEGEND

- MAST ARM/IDENTIFYING LENGTH
- VEHICULAR SIGNAL HEAD/BACKPLATE/VEHICLE IDENTIFYING NUMBER
- PEDESTRIAN SIGNAL HEAD/IDENTIFYING NUMBER
- PEDESTRIAN PUSHBUTTON/SIGN
- SIGN/IDENTIFYING LETTER
- VIDEO DETECTOR
- CONTROLLER CABINET
- LOOP SENSOR/ZONE OF DETECTION
- EMERGENCY PRE-EMPTION FLASHING BEACON
- EMERGENCY PRE-EMPTION CURB RAMP
- UTILITY POLE
- PHASE NUMBER
- INLET
- WAVEFORM RADAR DETECTOR

### GENERAL NOTES

NO MODIFICATIONS OF THIS INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS GRANTED IN WRITING BY A REPRESENTATIVE OF THE DEPARTMENT OF TRANSPORTATION.

ALL MAINTENANCE WORK INCLUDING TRIMMING OF TREES, NECESSARY FOR PROPER VIEWING OF THE SIGNALS IS THE RESPONSIBILITY OF THE PERMITTEE.

ALL SIGNS AND PAVEMENT MARKINGS INDICATED ON THIS DRAWING ARE CONSIDERED PART OF THE PERMIT AND SHALL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH PUBLICATION NO. 212.

POST MOUNTED SIGNALS SHALL BE INSTALLED WITH THE SIGNAL HEADS A MINIMUM OF 2 FEET ABOVE THE FACE OF CURB OR THE EDGE OF THE SHOULDER. SUPPORT POLES FOR OVERHEAD SIGNALS SHALL ALSO HAVE A MINIMUM CLEARANCE HORIZONTALLY OF 2 FEET.

SIGNALS ERECTED OVER THE ROADWAY SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 16 FT. ABOVE THE ROADWAY. POST MOUNTED SIGNALS SHALL BE A MINIMUM OF 8 FT. ABOVE THE SIDEWALK OR PAVEMENT.

ALL OVERHEAD SIGNALS MUST BE RIGIDLY MOUNTED, TOP AND BOTTOM, AND EQUIPPED WITH BACKPLATES.

THE MINIMUM HORIZONTAL DISTANCE BETWEEN SIGNALS MEASURED AT RIGHT ANGLES TO THE APPROACH SHALL BE 8 FEET.

EXACT LOCATION OF DETECTORS SHALL BE DETERMINED PRIOR TO INSTALLATION BY A REPRESENTATIVE OF PENNDOT.

CURBS TO BE INSTALLED BY MUNICIPALITY AND WHERE NOTED, SHALL BE PLAIN CONCRETE CURBS OR GRANITE CURBS, INSTALLED IN ACCORDANCE WITH DEPARTMENT SPECIFICATIONS ORW 408.

PRIOR TO INSTALLATION THE CONTRACTOR SHALL CONSULT WITH THE LOCAL OFFICIALS AND UTILITY COMPANIES TO RESOLVE ANY PROBLEMS WHICH MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.

THIS DRAWING CANNOT BE USED AS AN CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLIES WITH THE PROVISIONS OF THE LATEST AMENDMENT TO ACT 207, PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES, DATED DECEMBER 20, 1974.

WHEN LIQUID FUELS MONEY IS USED, SIGNAL INSTALLATION MUST CONFORM TO FORM 408 AND A COPY OF THE PROPOSED SPECIFICATIONS MUST BE SUBMITTED TO THE DISTRICT TRAFFIC UNIT, FOR REVIEW, PRIOR TO BIDDING.

PERMITTEE SHALL OBTAIN A HIGHWAY OCCUPANCY PERMIT FOR ANY CHANGES IN INTERSECTION GEOMETRY REGARDING EXCAVATION.

CONCRETE INSTALLED IN BITUMINOUS ROADWAY LESS THAN 5 YEARS OLD, OR CONCRETE ROADWAY REGARDLESS OF AGE, MUST BE BORED OR JACKED UNDER THE ROADWAY, INSTALL IN ACCORDANCE WITH TRAFFIC SIGNAL STANDARDS 10-8800 SERIES.

PENNSYLVANIA DEPARTMENT OF TRANSPORTATION  
ENGINEERING DISTRICT 6-0

COUNTY: MONTGOMERY

MUNICIPALITY: HATFIELD BOROUGH

INTERSECTION: MAIN STREET (S.R. 0463)

AND BROAD STREET (S.R. 0463/1003)

REVIEWED:

DATE

RECOMMENDED:

DATE

DOUGLAS MAY

DATE

4/2/96

1. ADD LEFT TURN SIGNAL PHASING CHANGE

2. ADD W/NO SIGNALS AND TIME CHANGE

3. CHANGED "0" A "T" TO MATCH W/NO SIGS

4. ADD OVER PRE-EMPTION ALL APPROACHES

5. TIMING CHANGE

6. TIMING CHANGE, NEW COUNTS

7. ADD COUNTDOWN W/NO SIGNALS

8. ADD COUNTDOWN W/NO SIGNALS

9. ADD COUNTDOWN W/NO SIGNALS

10. ADD COUNTDOWN W/NO SIGNALS

11. ADD COUNTDOWN W/NO SIGNALS

12. ADD COUNTDOWN W/NO SIGNALS

13. ADD COUNTDOWN W/NO SIGNALS

14. ADD COUNTDOWN W/NO SIGNALS

15. ADD COUNTDOWN W/NO SIGNALS

16. ADD COUNTDOWN W/NO SIGNALS

17. ADD COUNTDOWN W/NO SIGNALS

18. ADD COUNTDOWN W/NO SIGNALS

19. ADD COUNTDOWN W/NO SIGNALS

20. ADD COUNTDOWN W/NO SIGNALS

21. ADD COUNTDOWN W/NO SIGNALS

22. ADD COUNTDOWN W/NO SIGNALS

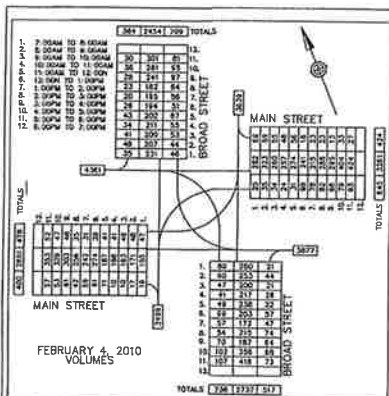
23. ADD COUNTDOWN W/NO SIGNALS



# **APPENDIX F:**

## **PennDOT-Approved Signal Plan**





### EMERGENCY PRE-EMPTION NOTES:

- \* CONTROLLER TO BE COUPLED WITH EMERGENCY PRE-EMPTION FOR THE NORTHBOUND & SOUTHBOUND APPROACHES OF BROAD STREET AND THE EASTBOUND & WESTBOUND APPROACHES OF MAIN STREET WITH A FAIL-SAFE DEVICE FOR EACH DIRECTION OF TRAVEL.
- \* THIS EMERGENCY BEACON SHALL CONSIST OF A FLASHING WHITE FLOOD LIGHT, AND SHALL FLASH WHEN THE EMERGENCY VEHICLE HAS CONTROL OF THE INTERSECTION FOR THE APPROPRIATE APPROACH.
- \* LOCATION OF EMERGENCY VEHICLE DETECTORS ARE TO BE FIELD ADJUSTED TO ACHIEVE MAXIMUM OPERATION.
- \* THE SIGNALS, WHEN ACTIVATED BY AN EMERGENCY VEHICLE, SHALL TERMINATE ALL GREEN INDICATORS IMMEDIATELY, FOLLOWED BY THE COMPLETE YELLOW AND RED CLEARANCE INTERVALS, ACCORDINGLY, THEN THE GREEN INTERVAL FOR THE PRE-EMPTED PHASE SHALL FOLLOW.
- \* THE SIGNALS, WHEN ACTIVATED BY AN EMERGENCY VEHICLE, SHALL TIME OUT ALL YELLOW AND RED INDICATORS, FOLLOWED BY THE GREEN INTERVAL OF THE PRE-EMPTED PHASE COVERED BY THE APPROACHING EMERGENCY VEHICLE.
- \* IF THE SIGNAL HAS BEEN ACTIVATED BY A PEDESTRIAN PUSH BUTTON AND THE SIGNAL IS PRE-EMPTED DURING THE "WALK" PHASE, THE WALK PHASE SHALL TERMINATE IMMEDIATELY FOLLOWED BY THE "FLASHING DON'T WALK" INDICATION IN ITS ENTIRETY, FOLLOWED BY THE APPROPRIATE SELECTIVE CLEARANCES BEFORE PROCEEDING TO THE PRE-EMPTED PHASE.
- \* IF THE SIGNALS, WHEN ACTIVATED BY AN EMERGENCY VEHICLE, ARE FLASHING ALL SIGNALS SHALL REMAIN FLASHING.
- \* IF ADDITIONAL PRE-EMPTION PHASES ARE ACTIVATED WHILE IN PRE-EMPTION, THE ORIGINAL PRE-EMPTION PHASE SHALL TIME OUT BEFORE PROCEEDING TO THE NEXT PRE-EMPTION PHASE.
- \* UPON COMPLETION OF PRE-EMPTION, PHASE 2+5 OR 8 IN RETURNING TO NORMAL OPERATION, PHASE 2+5 INTERVAL 12 SHALL FOLLOW.
- \* IN EMERGENCY PRE-EMPTION, NO PRIORITY SHALL BE ESTABLISHED. PRE-EMPTION SHALL BE A FIRST COME, FIRST SERVED OPERATION.

### EMERGENCY PRE-EMPTION PHASING MOVEMENT, SEQUENCE AND TIMING DIAGRAM

PHASE	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28
1	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
2	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
3	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
4	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
5	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
6	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
7	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
8	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
9	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
10	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
11	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
12	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
13	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
14	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
15	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
16	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
17	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
18	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
19	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
20	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
21	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
22	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
23	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
24	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
25	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
26	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
27	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
28	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R

\*\* FOR DURATION OF PRE-EMPTION

NOTE:  
IF PRE-EMPTION EQUIPMENT HAS ENCODING CAPABILITIES FOR VEHICLE IDENTIFICATION, IT IS RECOMMENDED TO HAVE THE ZERO "00" FEATURE ON TO ONE UNOCCUPIED CHITTERS THE ABILITY TO ACTIVATE THE EMERGENCY PRE-EMPTION.

① G/Y WHEN RETURNING TO NORMAL OPERATION

② G WHEN RETURNING TO NORMAL OPERATION

\*\* MAX II TIMINGS TO OPERATE FROM 14:00 TO 19:00, MONDAY THROUGH FRIDAY. MAX I TO OPERATE AT ALL OTHER TIMES OF THE DAY.

### MOVEMENT, SEQUENCE AND TIMING DIAGRAM

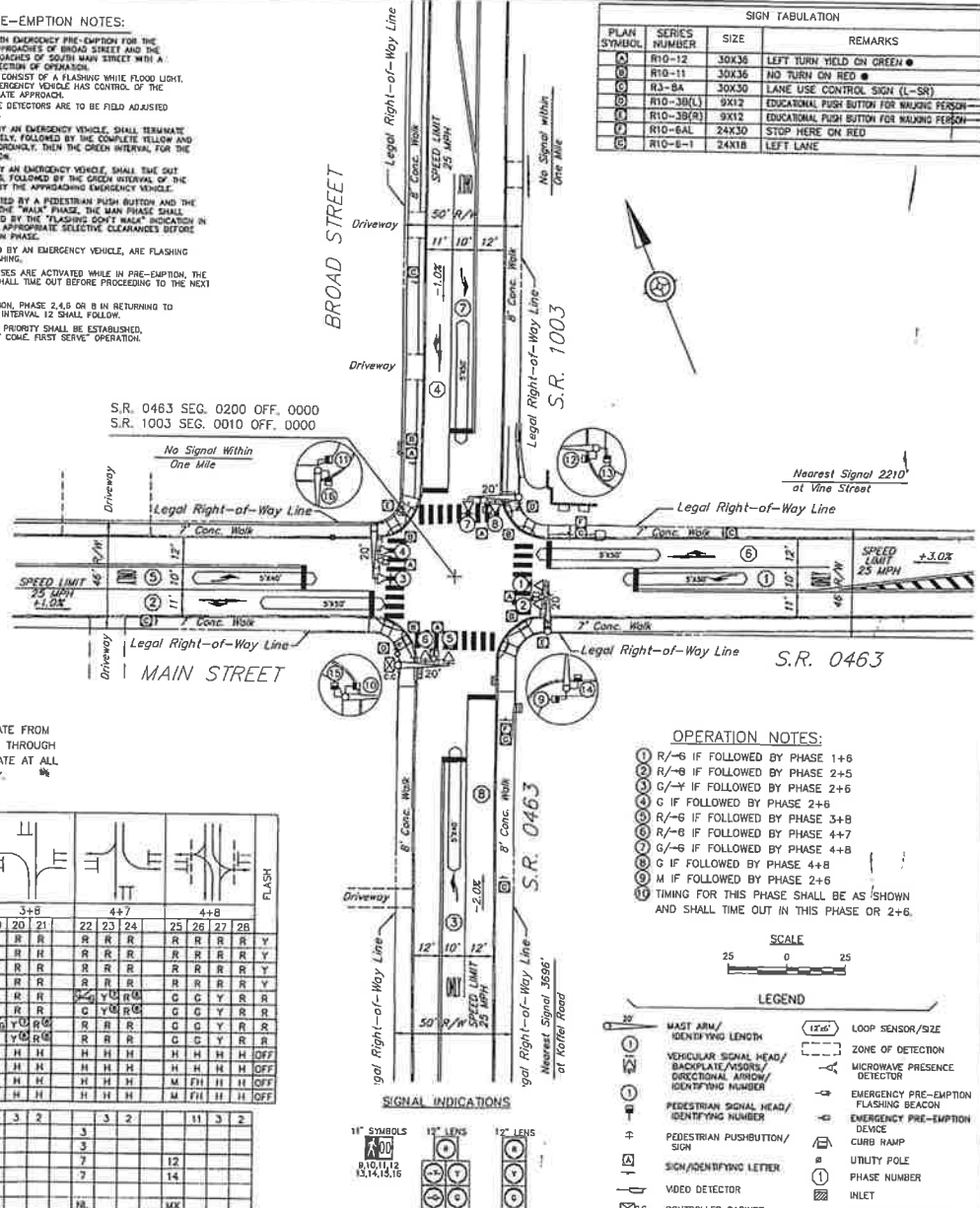
PHASE	1+5	2+5	1+6	2+6	3+7	3+8	4+7	4+8
1	R	R	R	R	R	R	R	R
2	R	R	R	R	R	R	R	R
3	R	R	R	R	R	R	R	R
4	R	R	R	R	R	R	R	R
5	R	R	R	R	R	R	R	R
6	R	R	R	R	R	R	R	R
7	R	R	R	R	R	R	R	R
8	R	R	R	R	R	R	R	R
9	R	R	R	R	R	R	R	R
10	R	R	R	R	R	R	R	R
11	R	R	R	R	R	R	R	R
12	R	R	R	R	R	R	R	R
13	R	R	R	R	R	R	R	R
14	R	R	R	R	R	R	R	R
15	R	R	R	R	R	R	R	R
16	R	R	R	R	R	R	R	R
17	R	R	R	R	R	R	R	R
18	R	R	R	R	R	R	R	R
19	R	R	R	R	R	R	R	R
20	R	R	R	R	R	R	R	R
21	R	R	R	R	R	R	R	R
22	R	R	R	R	R	R	R	R
23	R	R	R	R	R	R	R	R
24	R	R	R	R	R	R	R	R
25	R	R	R	R	R	R	R	R
26	R	R	R	R	R	R	R	R
27	R	R	R	R	R	R	R	R
28	R	R	R	R	R	R	R	R

\*UPON PEDESTRIAN ACTUATION ONLY. OTHERWISE HAND SYMBOL AT ALL TIMES.

PLAN SYMBOL	SERIES NUMBER	SIZE	REMARKS
①	R10-12	30X36	LEFT TURN YIELD ON GREEN
②	R10-11	30X36	NO TURN ON RED
③	R10-8A	30X30	LANE USE CONTROL SIGN (L-SR)
④	R10-38(L)	9X12	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON
⑤	R10-38(R)	9X12	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON
⑥	R10-6AL	24X30	STOP HERE ON RED
⑦	R10-6-1	24X18	LEFT LANE

### GENERAL NOTES

- NO MODIFICATIONS OF THIS INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS GRANTED IN WRITING BY A REPRESENTATIVE OF THE PENNSYLVANIA DEPARTMENT OF TRANSPORTATION.
- ALL MAINTENANCE WORK INCLUDING TRIMMING OF TREES, NECESSARY FOR PROPER OPERATION OF THE SIGNALS IS THE RESPONSIBILITY OF THE PERMITTEE.
- ALL SIGNS AND PAVEMENT MARKINGS INDICATED ON THIS DRAWING ARE CONSIDERED PART OF THE PERMIT AND SHALL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH PUBLICATION NO. 212.
- POST MOUNTED SIGNALS SHALL BE INSTALLED WITH THE SIGNAL HEADS A MINIMUM OF 2 FEET BEHIND THE FACE OF CURB OR THE EDGE OF THE SHOULDER. SUPPORT POLES FOR OVERHEAD SIGNALS SHALL ALSO HAVE A MINIMUM CLEARANCE HORIZONTALLY OF 2 FEET.
- SIGNALS ERECTED OVER THE ROADWAY SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 16 FT. ABOVE THE ROADWAY. POST MOUNTED SIGNALS SHALL BE A MINIMUM OF 8 FT. ABOVE THE SIDEWALK OR PAVEMENT.
- ALL OVERHEAD SIGNALS MUST BE RIGIDLY MOUNTED TOP AND BOTTOM, AND EQUIPPED WITH BACKPLATES.
- THE MINIMUM HORIZONTAL DISTANCE BETWEEN SIGNALS MEASURED AT RIGHT ANGLES TO THE APPROACH SHALL BE 8 FEET.
- EXACT LOCATION OF DETECTORS SHALL BE DETERMINED PRIOR TO INSTALLATION BY A REPRESENTATIVE OF PENNDOT.
- CURBING TO BE INSTALLED BY MUNICIPALITY AND WHERE NOTED, SHALL BE PLAIN CEMENT CONCRETE CURB OR GRANITE CURB, INSTALLED IN ACCORDANCE WITH DEPARTMENT SPECIFICATIONS ORM 408.
- PRIOR TO INSTALLATION THE CONTRACTOR SHALL CONSULT WITH THE LOCAL OFFICIALS AND UTILITY COMPANIES TO RESOLVE ANY PROBLEMS WHICH MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.
- THIS DRAWING CANNOT BE USED AS A CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLIES WITH THE PROVISIONS OF THE LATEST AMENDMENT TO ACT 267, PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES, DATED DECEMBER 20, 1974.
- WHEN LIQUID FUELS MONEY IS USED, SIGNAL INSTALLATION MUST CONFORM TO FORM 408 AND A COPY OF THE PROPOSED SPECIFICATIONS MUST BE SUBMITTED TO THE DISTRICT TRAFFIC UNIT, FOR REVIEW, PRIOR TO BIDDING.
- PERMITTEE SHALL OBTAIN A HIGHWAY OCCUPANCY PERMIT FOR ANY CHANGES IN INTERSECTION GEOMETRY REGARDING EXCAVATION, CULVERT, OR CONCRETE ROADWAY REGARDLESS OF AGE, MUST BE BORROWED ON JACKED UNDER THE ROADWAY, INSTALL IN ACCORDANCE WITH TRAFFIC SIGNAL STANDARDS 10-8800 SERIES.



### OPERATION NOTES:

- ① R/-6 IF FOLLOWED BY PHASE 1+5
- ② R/-8 IF FOLLOWED BY PHASE 2+5
- ③ G/-4 IF FOLLOWED BY PHASE 2+5
- ④ G IF FOLLOWED BY PHASE 2+6
- ⑤ R/-6 IF FOLLOWED BY PHASE 3+8
- ⑥ R/-8 IF FOLLOWED BY PHASE 4+7
- ⑦ G/-6 IF FOLLOWED BY PHASE 4+8
- ⑧ G IF FOLLOWED BY PHASE 4+8
- ⑨ M IF FOLLOWED BY PHASE 2+6
- ⑩ TIMING FOR THIS PHASE SHALL BE AS SHOWN AND SHALL TIME OUT IN THIS PHASE OR 2+6.

SCALE

25 0 25

### LEGEND

- ① WEST ARM/IDENTIFYING LENGTH
- ② VERTICAL SIGNAL HEAD/BACKPLATE/VEHICLE/IDENTIFYING NUMBER
- ③ PEDESTRIAN SIGNAL HEAD/IDENTIFYING NUMBER
- ④ PEDESTRIAN PUSH-BUTTON/SIGN
- ⑤ SIGN/IDENTIFYING LETTER
- ⑥ VIDEO DETECTOR
- ⑦ CONTROLLER CABINET
- ⑧ LOOP SENSOR/ZONE OF DETECTION
- ⑨ MICROWAVE PRESENCE DETECTOR
- ⑩ EMERGENCY PRE-EMPTION FLASHING BEACON
- ⑪ EMERGENCY PRE-EMPTION CURB RAMP
- ⑫ UTILITY POLE
- ⑬ PHASE NUMBER
- ⑭ INLET
- ⑮ WAVELENGTH RADAR DETECTOR

PENNSYLVANIA DEPARTMENT OF TRANSPORTATION  
ENGINEERING DISTRICT 6-0

COUNTY: MONTGOMERY  
MUNICIPALITY: HATFIELD BOROUGH

INTERSECTION: MAIN STREET (S.R. 0463)  
AND BROAD STREET (S.R. 0463/1003)

REVIEWED: \_\_\_\_\_ DATE: \_\_\_\_\_

MUNICIPAL OFFICIAL: \_\_\_\_\_ DATE: \_\_\_\_\_

RECOMMENDED: \_\_\_\_\_ DATE: \_\_\_\_\_

DOUGLAS MAY  
DISTRICT TRAFFIC ENGINEER

DATE: 4/2/96

NO.	REVISION	DATE	BY	CHKD.	DATE	BY	CHKD.
1	ADD LEFT TURN SIGNAL, PHASING CHANGE	5/1/97	MLK	5/1/97	MLK	5/1/97	MLK
2	ADD W/NO SIGNALS AND PHASING CHANGE	1/1/97	MLK	1/1/97	MLK	1/1/97	MLK
3	CHANGED "Y" & "T" TO MATCH W/NO SIGS	1/1/97	MLK	1/1/97	MLK	1/1/97	MLK
4	ADD DIER PRE-EMPTION ALL APPROACHES	1/1/97	MLK	1/1/97	MLK	1/1/97	MLK
5	TRIMMING CHANGE	1/1/97	MLK	1/1/97	MLK	1/1/97	MLK
6	TRIMMING CHANGE, NEW COUNTS	1/1/97	MLK	1/1/97	MLK	1/1/97	MLK
7	ADD COUNTDOWN W/NO SIGNALS	1/1/97	MLK	1/1/97	MLK	1/1/97	MLK
8	ADD W/NO SIGNALS UNDER 31-33, RELOCATE W/NO SIGNALS UNDER 31-33	1/1/97	MLK	1/1/97	MLK	1/1/97	MLK

SHEET 2 OF 2 PERMIT # 64-0934 FILE # 0934

# **APPENDIX G:**

## **Gap Analysis**



# **Gap Calculation for Unsignalized Intersection** **Left Turn from Minor Road to 2-Lane Major Road**

**Intersection:**    **Major St.**        **N. Main Street**  
                          **Minor St.**        **Full-Access Driveway**

**Time Studied:**    **Weekday A.M. Peak Hour**  
**Date of Study:**    **10/8/2024**

**Critical Gap:**                    **6.4**  
**Follow-Up Time:**                **3**

<b>Length of Gap (seconds)</b>	<b>Vehicles Accommodated</b>	<b>Number of Gaps Observed</b>	<b>Total Vehicles</b>
0 - 6.4	0		0
6.4 - 9.4	1	51	51
9.4 - 12.4	2	24	48
12.4 - 15.4	3	16	48
15.4 - 18.4	4	12	48
18.4 - 21.4	5	7	35
21.4 - 24.4	6	6	36
24.4 - 27.4	7	3	21
27.4+	8	11	88
<b>Total Vehicles Accommodated</b>			<b>375</b>

<b>Minimum Gap</b>	<b>Number of Cars</b>
0	0
6.4	1
9.4	2
12.4	3
15.4	4
18.4	5
21.4	6
24.4	7
27.4	8



## **APPENDIX G:**

### **Gap Analysis**



# **Gap Calculation for Unsignalized Intersection** **Left Turn from Minor Road to 2-Lane Major Road**

**Intersection:**    **Major St.**        **N. Main Street**  
                          **Minor St.**        **Full-Access Driveway**

**Time Studied:**    **Weekday A.M. Peak Hour**  
**Date of Study:**    **10/8/2024**

**Critical Gap:**                      **6.4**  
**Follow-Up Time:**                **3**

<b>Length of Gap (seconds)</b>	<b>Vehicles Accommodated</b>	<b>Number of Gaps Observed</b>	<b>Total Vehicles</b>
0 - 6.4	0		0
6.4 - 9.4	1	51	51
9.4 - 12.4	2	24	48
12.4 - 15.4	3	16	48
15.4 - 18.4	4	12	48
18.4 - 21.4	5	7	35
21.4 - 24.4	6	6	36
24.4 - 27.4	7	3	21
27.4+	8	11	88
<b>Total Vehicles Accommodated</b>			<b>375</b>

<b>Minimum Gap</b>	<b>Number of Cars</b>
0	0
6.4	1
9.4	2
12.4	3
15.4	4
18.4	5
21.4	6
24.4	7
27.4	8

## 64

Serialized Time	Seconds		Gops	1 sec	2	30	open	curr
7:30:09 AM	03	3		1 sec	2	30		
7:30:09 AM				2 sec	2	30		
7:30:09 AM				3 sec	8	51		
7:30:09 AM				4 sec	4	35		
7:30:17 AM	13	13		5 sec	5	25		
7:30:17 AM				6 sec	6	13		
7:30:17 AM				7 sec	7	20	1 sec	51
7:30:17 AM				8 sec	8	19		
7:30:34 AM	17	17		9 sec	9	15		
7:30:34 AM				10 sec	10	2	2 cars	24
7:30:34 AM				11 sec	11	7		
7:30:34 AM				12 sec	12	12		
7:30:34 AM				13 sec	13	8	3 cars	16
7:30:34 AM				14 sec	14	3		
7:30:50 AM	02	2		15 sec	15	5		
7:30:50 AM				16 sec	16	5	4 cars	12
7:30:50 AM				17 sec	17	4		
7:30:50 AM				18 sec	18	5		
7:30:50 AM				19 sec	19	5	5 cars	7
7:30:50 AM				20 sec	20	6		
7:30:50 AM				21 sec	21	6		
7:30:50 AM				22 sec	22	2	6 cars	1
7:30:50 AM				23 sec	23	2		
7:30:50 AM				24 sec	24	2		
7:30:50 AM				25 sec	25	2	2 cars	21
7:30:50 AM				26 sec	26	1		
7:30:50 AM				27 sec	27	0		
7:30:50 AM				28 sec	28	3	8 cars	11
7:30:50 AM				29 sec	29	0		
7:30:50 AM				30 sec	30	1	Total Cars	375
7:31:08 AM	14	14		31 sec	31	1		
7:31:08 AM				32 sec	32	0		
7:31:08 AM				33 sec	33	0		
7:31:08 AM				34 sec	34	1		
7:31:08 AM				35 sec	35	0		
7:31:08 AM				36 sec	36	0		
7:31:08 AM				37 sec	37	0		
7:31:08 AM				38 sec	38	0		
7:31:08 AM				39 sec	39	0		
7:31:08 AM				40 sec	40	0		
7:31:08 AM				41 sec	41	1		
7:31:08 AM				42 sec	42	0		
7:31:08 AM				43 sec	43	0		
7:31:08 AM				44 sec	44	1		
7:31:08 AM				45 sec	45	0		
7:31:08 AM				46 sec	46	1		
7:31:08 AM				47 sec	47	1		
7:31:08 AM				48 sec	48	0		
7:31:08 AM				49 sec	49	0		
7:31:08 AM				50 sec	50	0		
7:31:08 AM				51 sec	51	0		
7:31:08 AM				52 sec	52	0		
7:31:08 AM				53 sec	53	0		
7:31:08 AM				54 sec	54	0		
7:31:08 AM				55 sec	55	0		
7:31:08 AM				56 sec	56	0		
7:31:08 AM				57 sec	57	0		
7:31:08 AM				58 sec	58	0		
7:31:08 AM				59 sec	59	0		
7:31:08 AM				60 sec	60	0		
7:31:08 AM				61 min	61	0		
7:31:58 AM	02	2						
7:34:00 AM								
7:34:11 AM								
7:34:18 AM	07	7						
7:34:18 AM								
7:34:19 AM								
7:34:26 AM	07	7						
7:34:26 AM								
7:34:27 AM								
7:34:31 AM	04	4						
7:34:31 AM								
7:34:31 AM								
7:34:33 AM	02	2						
7:34:33 AM								
7:34:36 AM								
7:35:20 AM	44	44						
7:35:20 AM								
7:35:21 AM								
7:35:24 AM	01	1						
7:35:24 AM								
7:35:25 AM								
7:35:26 AM	01	1						
7:35:26 AM								
7:35:27 AM								
7:35:39 AM	12	12						
7:35:39 AM								
7:35:40 AM								
7:35:44 AM	04	4						
7:35:44 AM								
7:35:54 AM								
7:35:58 AM	02	2						
7:35:58 AM								
7:35:57 AM								
7:35:58 AM	01	1						
7:35:58 AM								
7:36:04 AM	06	6						
7:36:04 AM								
7:36:06 AM	02	2						
7:36:06 AM								
7:36:07 AM								
7:36:22 AM	15	15						
7:36:22 AM								
7:36:23 AM								
7:36:24 AM	01	1						
7:36:24 AM								
7:36:25 AM								
7:36:27 AM	02	2						
7:36:27 AM								
7:36:28 AM								
7:36:31 AM	03	3						
7:36:31 AM								
7:36:33 AM								
7:36:44 AM	11	11						
7:37:10 AM								
7:37:22 AM	12	12						
7:37:22 AM								
7:37:28 AM								
7:37:25 AM	02	2						
7:37:25 AM								
7:37:26 AM								
7:37:31 AM	05	5						
7:37:31 AM								
7:37:32 AM								
7:37:38 AM	04	4						
7:37:38 AM								
7:37:37 AM								
7:37:39 AM	02	2						
7:37:39 AM								
7:37:40 AM								
7:37:44 AM	04	4						
7:37:44 AM								
7:37:48 AM	04	4						
7:37:48 AM								
7:37:49 AM								
7:38:17 AM	28	28						

7:38:18 AM		
7:38:20 AM	02	2
7:38:21 AM		
7:38:25 AM	04	4
7:38:26 AM		
7:38:28 AM	02	2
7:38:30 AM		
7:38:36 AM	08	8
7:38:55 AM		
7:39:11 AM	16	16
7:39:12 AM		
7:39:14 AM	02	2
7:39:15 AM		
7:39:16 AM	01	1
7:39:17 AM		
7:39:24 AM	07	7
7:39:25 AM		
7:39:36 AM	11	11
7:39:38 AM		
7:39:46 AM	10	10
7:39:47 AM		
7:39:50 AM	03	3
7:39:52 AM		
7:39:57 AM	05	5
7:39:57 AM		
7:40:06 AM	09	9
7:40:15 AM		
7:40:18 AM	03	3
7:40:23 AM		
7:40:26 AM	03	3
7:40:28 AM		
7:40:30 AM	04	4
7:40:40 AM		
7:40:42 AM	02	2
7:40:43 AM		
7:40:47 AM	04	4
7:40:48 AM		
7:40:49 AM	01	1
7:40:50 AM		
7:40:57 AM	07	7
7:40:58 AM		
7:41:03 AM	05	5
7:41:04 AM		
7:41:08 AM	04	4
7:41:09 AM		
7:41:18 AM	09	9
7:41:21 AM		
7:41:24 AM	03	3
7:41:27 AM		
7:41:33 AM	06	6
7:41:36 AM		
7:41:42 AM	06	6
7:41:42 AM		
7:41:43 AM	01	1
7:41:52 AM		
7:41:54 AM	02	2
7:41:55 AM		
7:41:56 AM	01	1
7:41:58 AM		
7:42:04 AM	05	5
7:42:07 AM		
7:42:10 AM	03	3
7:42:15 AM		
7:42:33 AM	18	18
7:42:35 AM		
7:42:39 AM	04	4
7:42:41 AM		
7:42:48 AM	07	7
7:42:49 AM		
7:42:59 AM	10	10
7:43:00 AM		
7:43:02 AM	02	2
7:43:02 AM		
7:43:06 AM	04	4
7:43:10 AM		
7:43:12 AM	02	2
7:43:27 AM		
7:43:29 AM	02	2
7:43:31 AM		
7:43:34 AM	03	3
7:43:35 AM		
7:44:07 AM	32	32
7:44:08 AM		
7:44:09 AM	01	1
7:44:10 AM		
7:44:12 AM	02	2
7:44:14 AM		
7:44:30 AM	16	16
7:44:31 AM		
7:44:34 AM	03	3
7:44:35 AM		
7:44:36 AM	01	1
7:44:49 AM		
7:44:52 AM	03	3
7:44:53 AM		



# A.M. Minor Left-Turn GAP Analysis

Start/End Time	Seconds	
7:30:00 AM		
7:30:03 AM	03	1
7:30:04 AM		
7:30:17 AM	13	13
7:30:17 AM		
7:30:18 AM	17	17
7:30:18 AM		
7:30:34 AM		
7:30:47 AM	13	13
7:30:48 AM		
7:30:50 AM	02	2
7:30:51 AM		
7:30:53 AM	02	2
7:30:54 AM		
7:31:08 AM	14	14
7:31:08 AM		
7:31:20 AM	21	21
7:31:20 AM		
7:31:30 AM		
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7:49:37 AM	01	1
7:49:37 AM		
7:49:39 AM	12	12
7:49:50 AM		
7:49:55 AM	05	5
7:49:56 AM		
7:50:42 AM	46	46
7:50:43 AM		
7:50:59 AM	17	17
7:50:59 AM		
7:51:01 AM	02	2
7:51:03 AM		
7:51:11 AM	08	8
7:51:11 AM		
7:51:45 AM	34	34
7:51:47 AM		
7:51:49 AM	02	2
7:51:49 AM		
7:51:59 AM	10	10
7:52:00 AM		
7:52:03 AM	03	3
7:52:04 AM		
7:52:06 AM	02	2
7:52:08 AM		
7:52:09 AM	01	1
7:52:11 AM		
7:52:14 AM	03	3
7:52:15 AM		
7:52:16 AM	03	3
7:52:20 AM		
7:52:26 AM	06	6
7:52:27 AM		
7:52:30 AM	03	3
7:52:31 AM		
7:52:34 AM	03	3

7:52:35 AM 7:52:43 AM	08	8
7:52:44 AM 7:52:48 AM	04	4
7:52:49 AM 7:52:51 AM	02	2
7:52:53 AM 7:52:55 AM	02	2
7:52:58 AM 7:52:58 AM	02	2
7:52:59 AM 7:53:00 AM	01	1
7:53:43 AM 7:53:45 AM	02	2
7:53:47 AM 7:53:48 AM	01	1
7:54:01 AM 7:54:04 AM	03	3
7:54:05 AM 7:54:07 AM	02	2
7:54:07 AM 7:54:10 AM	03	3
7:54:10 AM 7:54:17 AM	07	7
7:54:18 AM 7:54:20 AM	02	2
7:54:22 AM 7:54:30 AM	08	8
7:54:32 AM 7:54:34 AM	02	2
7:55:09 AM 7:55:12 AM	03	3
7:55:13 AM 7:55:14 AM	01	1
7:55:19 AM 7:55:21 AM	02	2
7:55:22 AM 7:55:32 AM	10	10
7:55:33 AM 7:55:37 AM	04	4
7:55:42 AM 7:55:44 AM	02	2
7:56:49 AM 7:56:51 AM	02	2
7:56:53 AM 7:56:54 AM	01	1
7:56:55 AM 7:56:55 AM	00	0
7:56:56 AM 7:57:04 AM	04	4
7:57:05 AM 7:57:07 AM	02	2
7:57:08 AM 7:57:37 AM	24	24
7:57:39 AM 7:57:44 AM	12	12
7:57:46 AM 7:57:48 AM	03	3
7:57:53 AM 7:58:05 AM	13	13
7:58:15 AM 7:58:24 AM	09	9
7:58:25 AM 7:58:26 AM	01	1
7:58:31 AM 7:58:48 AM	17	17
7:58:49 AM 7:58:50 AM	01	1
7:58:52 AM 7:59:00 AM	08	8
7:59:01 AM 7:59:25 AM	24	24
7:59:25 AM 7:59:30 AM	05	5
7:59:31 AM 7:59:33 AM	02	2
7:59:34 AM 7:59:35 AM	01	1
7:59:40 AM 7:59:42 AM	04	4
7:59:43 AM 7:59:48 AM	05	5
7:59:52 AM 7:59:58 AM	04	4
7:59:56 AM 7:59:58 AM	03	3
7:59:59 AM 8:00:11 AM	12	12
8:00:12 AM 8:00:20 AM	08	8
8:00:27 AM 8:00:35 AM	08	8
8:00:36 AM 8:00:38 AM	02	2
8:00:39 AM 8:00:46 AM	07	7

8:00:48 AM 8:00:57 AM	09	9
8:00:54 AM 8:01:00 AM	02	2
8:01:01 AM 8:01:06 AM	05	5
8:01:06 AM 8:01:12 AM	06	6
8:01:17 AM 8:01:20 AM	03	3
8:01:21 AM 8:01:24 AM	03	3
8:01:25 AM 8:01:31 AM	06	6
8:01:32 AM 8:01:34 AM	02	2
8:01:34 AM 8:01:39 AM	05	5
8:01:40 AM 8:01:42 AM	02	2
8:01:44 AM 8:01:50 AM	06	6
8:02:00 AM 8:02:02 AM	02	2
8:02:03 AM 8:02:09 AM	06	6
8:02:39 AM 8:02:48 AM	09	9
8:02:50 AM 8:02:53 AM	03	3
8:02:54 AM 8:02:55 AM	01	1
8:03:03 AM 8:03:07 AM	04	4
8:03:09 AM 8:03:11 AM	05	5
8:03:15 AM 8:03:37 AM	22	22
8:03:39 AM 8:03:48 AM	09	9
8:04:11 AM 8:04:15 AM	04	4
8:04:16 AM 8:04:18 AM	02	2
8:04:19 AM 8:04:21 AM	02	2
8:04:23 AM 8:04:36 AM	13	13
8:04:37 AM 8:04:41 AM	04	4
8:04:42 AM 8:04:47 AM	05	5
8:04:48 AM 8:04:51 AM	03	3
8:04:52 AM 8:05:00 AM	08	8
8:05:01 AM 8:05:10 AM	09	9
8:05:30 AM 8:05:32 AM	02	2
8:05:33 AM 8:05:35 AM	02	2
8:05:38 AM 8:05:43 AM	05	5
8:05:44 AM 8:05:46 AM	02	2
8:05:48 AM 8:05:50 AM	02	2
8:05:51 AM 8:06:09 AM	18	18
8:06:11 AM 8:06:13 AM	04	4
8:06:15 AM 8:06:30 AM	15	15
8:06:31 AM 8:06:50 AM	19	19
8:06:53 AM 8:06:55 AM	02	2
8:06:56 AM 8:07:01 AM	05	5
8:07:05 AM 8:07:11 AM	06	6
8:07:12 AM 8:07:13 AM	01	1
8:07:14 AM 8:07:16 AM	02	2
8:07:17 AM 8:07:36 AM	19	19
8:07:37 AM 8:07:58 AM	19	19
8:07:57 AM 8:07:59 AM	02	2
8:08:00 AM 8:08:19 AM	19	19
8:08:20 AM		

8:08:33 AM	13	13
8:08:35 AM		
8:08:39 AM	03	3
8:08:50 AM		
8:08:53 AM	03	3
8:08:54 AM		
8:08:55 AM	02	2
8:08:58 AM		
8:09:26 AM	28	28
8:09:27 AM		
8:09:36 AM	09	9
8:09:37 AM		
8:10:02 AM	25	25
8:10:04 AM		
8:10:13 AM	09	9
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8:10:23 AM	05	5
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8:11:42 AM	05	5
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8:11:45 AM	03	3
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8:12:12 AM	13	13
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8:12:55 AM	08	8
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8:13:01 AM	05	5
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8:13:22 AM	21	21
8:13:23 AM		
8:13:25 AM	02	2
8:13:29 AM		
8:13:41 AM	12	12
8:13:43 AM		
8:13:50 AM	07	7
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8:13:54 AM	03	3
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8:14:07 AM	12	12
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8:14:21 AM	02	2
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8:14:24 AM	03	3
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8:14:35 AM	11	11
8:14:37 AM		
8:14:48 AM	11	11
8:14:48 AM		
8:14:51 AM	03	3
8:15:06 AM		
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8:16:54 AM	03	3
8:16:55 AM		
8:16:59 AM	04	4
8:17:00 AM		
8:17:01 AM	01	1
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8:00:58 AM 8:01:00 AM	02	2
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8:01:25 AM 8:01:31 AM	06	6
8:01:32 AM 8:01:34 AM	02	2
8:01:34 AM 8:01:39 AM	05	5
8:01:40 AM 8:01:42 AM	02	2
8:01:44 AM 8:01:50 AM	06	6
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8:02:39 AM 8:02:48 AM	09	9
8:02:50 AM 8:02:53 AM	03	3
8:02:54 AM 8:02:55 AM	01	1
8:03:03 AM 8:03:07 AM	04	4
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8:03:15 AM 8:03:37 AM	22	22
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8:04:37 AM 8:04:41 AM	04	4
8:04:42 AM 8:04:47 AM	05	5
8:04:48 AM 8:04:51 AM	03	3
8:04:52 AM 8:05:00 AM	08	8
8:05:01 AM 8:05:10 AM	09	9
8:05:30 AM 8:05:32 AM	02	2
8:05:33 AM 8:05:35 AM	02	2
8:05:38 AM 8:05:43 AM	05	5
8:05:44 AM 8:05:46 AM	02	2
8:05:48 AM 8:05:50 AM	02	2
8:05:51 AM 8:06:09 AM	18	18
8:06:11 AM 8:06:15 AM	04	4
8:06:15 AM 8:06:30 AM	15	15
8:06:31 AM 8:06:50 AM	19	19
8:06:53 AM 8:06:55 AM	02	2
8:06:56 AM 8:07:01 AM	05	5
8:07:05 AM 8:07:11 AM	06	6
8:07:12 AM 8:07:13 AM	01	1
8:07:14 AM 8:07:16 AM	02	2
8:07:17 AM 8:07:36 AM	19	19
8:07:37 AM 8:07:56 AM	19	19
8:07:57 AM 8:07:59 AM	02	2
8:08:00 AM 8:08:19 AM	19	19
8:08:20 AM		

8:06:33 AM	13	13
8:06:36 AM		
8:06:39 AM	03	3
8:06:50 AM		
8:06:53 AM	03	3
8:06:54 AM		
8:06:56 AM	02	2
8:06:58 AM		
8:06:58 AM	28	28
8:06:58 AM		
8:09:27 AM		
8:09:36 AM	09	9
8:09:37 AM		
8:10:03 AM	25	25
8:10:04 AM		
8:10:13 AM	09	9
8:10:14 AM		
8:10:17 AM	03	3
8:10:18 AM		
8:10:23 AM	05	5
8:10:23 AM		
8:10:26 AM	03	3
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8:10:37 AM	07	7
8:10:37 AM		
8:10:48 AM		
8:10:52 AM	14	14
8:10:53 AM		
8:11:01 AM	08	8
8:11:02 AM		
8:11:05 AM	03	3
8:11:06 AM		
8:11:09 AM	03	3
8:11:12 AM		
8:11:37 AM	25	25
8:11:37 AM		
8:11:42 AM	05	5
8:11:42 AM		
8:11:45 AM	03	3
8:11:59 AM		
8:12:12 AM	13	13
8:12:16 AM		
8:12:46 AM	30	30
8:12:47 AM		
8:12:55 AM	08	8
8:12:56 AM		
8:13:01 AM	05	5
8:13:01 AM		
8:13:21 AM	21	21
8:13:22 AM		
8:13:23 AM		
8:13:25 AM	02	2
8:13:29 AM		
8:13:41 AM	12	12
8:13:43 AM		
8:13:50 AM	07	7
8:13:51 AM		
8:13:54 AM	03	3
8:13:55 AM		
8:14:07 AM	12	12
8:14:08 AM		
8:14:10 AM	02	2
8:14:12 AM		
8:14:19 AM		
8:14:21 AM	02	2
8:14:21 AM		
8:14:24 AM	03	3
8:14:24 AM		
8:14:24 AM		
8:14:35 AM	11	11
8:14:37 AM		
8:14:40 AM	11	11
8:14:48 AM		
8:14:51 AM	03	3
8:14:51 AM		
8:15:06 AM		
8:15:14 AM	28	28
8:15:36 AM		
8:15:51 AM	15	15
8:15:53 AM		
8:16:09 AM	16	16
8:16:11 AM		
8:16:13 AM	02	2
8:16:22 AM		
8:16:27 AM	05	5
8:16:28 AM		
8:16:35 AM	07	7
8:16:35 AM		
8:16:35 AM		
8:16:35 AM		
8:16:50 AM	15	15
8:16:50 AM		
8:16:51 AM		
8:16:54 AM	03	3
8:16:55 AM		
8:16:59 AM	04	4
8:17:00 AM		
8:17:01 AM	01	1
8:17:01 AM		
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8:17:18 AM	17	17
8:17:19 AM		
8:17:21 AM	02	2

8:17:22 AM 8:17:25 AM	03	3
8:17:27 AM 8:17:34 AM	03	7
8:17:35 AM 8:17:37 AM	02	3
8:17:38 AM 8:17:45 AM	03	3
8:17:45 AM 8:17:47 AM	02	2
8:17:48 AM 8:17:49 AM	01	3
8:17:52 AM 8:18:00 AM	08	8
8:18:01 AM 8:18:03 AM	02	2
8:18:04 AM 8:18:51 AM	43	43
8:18:52 AM 8:18:57 AM	05	5
8:18:57 AM 8:19:09 AM	12	12
8:19:10 AM 8:19:13 AM	03	3
8:19:14 AM 8:19:16 AM	02	2
8:19:21 AM 8:19:23 AM	02	2
8:19:24 AM 8:19:28 AM	04	4
8:19:29 AM 8:19:30 AM	01	3
8:19:31 AM 8:19:33 AM	02	2
8:19:34 AM 8:19:44 AM	10	10
8:19:44 AM 8:19:51 AM	07	7
8:19:52 AM 8:20:14 AM	22	22
8:20:15 AM 8:20:22 AM	07	7
8:20:24 AM 8:20:32 AM	08	8
8:20:33 AM 8:20:34 AM	01	1
8:20:36 AM 8:20:45 AM	09	9
8:20:46 AM 8:20:47 AM	01	1
8:20:50 AM 8:20:55 AM	05	5
8:20:56 AM 8:20:58 AM	02	2
8:21:01 AM 8:21:32 AM	31	31
8:21:33 AM 8:21:38 AM	01	1
8:21:38 AM 8:21:45 AM	07	7
8:21:51 AM 8:21:58 AM	08	8
8:22:00 AM 8:22:01 AM	01	1
8:22:06 AM 8:22:14 AM	08	8
8:22:15 AM 8:22:15 AM	00	0
8:22:21 AM 8:22:29 AM	08	8
8:22:31 AM 8:22:39 AM	08	8
8:22:40 AM 8:22:45 AM	09	5
8:22:46 AM 8:22:57 AM	11	11
8:22:58 AM 8:23:02 AM	04	4
8:23:03 AM 8:23:15 AM	12	12
8:23:22 AM 8:23:24 AM	02	2
8:23:25 AM 8:23:27 AM	02	2
8:23:30 AM 8:23:34 AM	04	4
8:23:35 AM 8:23:38 AM	03	3
8:23:39 AM 8:23:42 AM	03	3
8:23:42 AM 8:23:48 AM	06	6
8:23:53 AM 8:23:55 AM	02	2
8:23:58 AM 8:24:11 AM	13	13

8:24:12 AM 8:24:26 AM	18	14
8:24:27 AM 8:24:31 AM	04	4
8:24:33 AM 8:24:45 AM	12	12
8:24:45 AM 8:24:47 AM	01	1
8:24:54 AM 8:25:02 AM	08	8
8:25:03 AM 8:25:06 AM	03	3
8:25:06 AM 8:25:10 AM	04	4
8:25:11 AM 8:25:12 AM	01	1
8:25:13 AM 8:25:14 AM	01	1
8:25:16 AM 8:25:17 AM	01	1
8:25:23 AM 8:25:27 AM	04	4
8:25:28 AM 8:25:30 AM	02	2
8:25:31 AM 8:26:12 AM	41	41
8:26:13 AM 8:26:20 AM	07	7
8:26:21 AM 8:26:23 AM	02	2
8:26:24 AM 8:26:25 AM	01	1
8:26:31 AM 8:26:33 AM	02	2
8:26:34 AM 8:26:42 AM	08	8
8:26:45 AM 8:26:49 AM	04	4
8:26:50 AM 8:26:52 AM	02	2
8:26:58 AM 8:27:09 AM	11	11
8:27:10 AM 8:27:14 AM	04	4
8:27:16 AM 8:27:28 AM	12	12
8:27:29 AM 8:27:31 AM	02	2
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8:27:52 AM 8:28:01 AM	09	9
8:28:01 AM 8:28:13 AM	12	12
8:28:14 AM 8:28:17 AM	03	3
8:28:18 AM 8:28:21 AM	03	3
8:28:23 AM 8:28:25 AM	02	2
8:28:26 AM 8:28:28 AM	02	2
8:28:33 AM 8:28:46 AM	13	13
8:28:47 AM 8:29:10 AM	23	23
8:29:12 AM 8:29:19 AM	07	7
8:29:23 AM 8:29:38 AM	15	15
8:29:39 AM 8:29:41 AM	02	2
8:29:42 AM 8:29:45 AM	03	3
8:29:45 AM 8:29:49 AM	04	4

8:17:22 AM 8:17:25 AM	03	1
8:17:27 AM 8:17:34 AM	07	7
8:17:35 AM 8:17:37 AM	02	2
8:17:38 AM 8:17:45 AM	07	7
8:17:45 AM 8:17:47 AM	03	2
8:17:48 AM 8:17:49 AM	01	1
8:17:52 AM 8:18:00 AM	06	8
8:18:01 AM 8:18:03 AM	02	2
8:18:04 AM 8:18:51 AM	47	47
8:18:52 AM 8:18:57 AM	06	5
8:18:57 AM 8:19:09 AM	13	12
8:19:10 AM 8:19:13 AM	03	1
8:19:14 AM 8:19:16 AM	02	2
8:19:21 AM 8:19:23 AM	02	2
8:19:24 AM 8:19:28 AM	04	4
8:19:29 AM 8:19:30 AM	01	1
8:19:31 AM 8:19:33 AM	02	2
8:19:34 AM 8:19:44 AM	10	10
8:19:44 AM 8:19:51 AM	07	7
8:19:52 AM 8:20:14 AM	22	22
8:20:15 AM 8:20:22 AM	07	7
8:20:24 AM 8:20:32 AM	08	8
8:20:33 AM 8:20:34 AM	01	1
8:20:36 AM 8:20:45 AM	09	9
8:20:46 AM 8:20:47 AM	01	1
8:20:50 AM 8:20:55 AM	05	5
8:20:56 AM 8:20:58 AM	02	2
8:21:01 AM 8:21:32 AM	31	31
8:21:33 AM 8:21:36 AM	03	3
8:21:38 AM 8:21:45 AM	07	7
8:21:51 AM 8:21:59 AM	08	8
8:22:00 AM 8:22:01 AM	01	1
8:22:06 AM 8:22:14 AM	06	8
8:22:15 AM 8:22:15 AM	00	0
8:22:21 AM 8:22:29 AM	06	8
8:22:31 AM 8:22:39 AM	08	8
8:22:40 AM 8:22:45 AM	05	5
8:22:46 AM 8:22:57 AM	11	11
8:22:58 AM 8:23:02 AM	04	4
8:23:03 AM 8:23:15 AM	12	12
8:23:21 AM 8:23:24 AM	02	2
8:23:25 AM 8:23:27 AM	02	2
8:23:30 AM 8:23:34 AM	04	4
8:23:35 AM 8:23:38 AM	03	3
8:23:39 AM 8:23:42 AM	03	3
8:23:42 AM 8:23:48 AM	06	6
8:23:53 AM 8:23:55 AM	02	2
8:23:59 AM 8:24:11 AM	13	13



8:24:12 AM 8:24:16 AM	14	14
8:24:27 AM 8:24:31 AM	04	4
8:24:33 AM 8:24:45 AM	12	12
8:24:46 AM 8:24:47 AM	01	1
8:24:54 AM 8:25:02 AM	08	8
8:25:03 AM 8:25:06 AM	03	3
8:25:06 AM 8:25:10 AM	04	4
8:25:11 AM 8:25:12 AM	01	1
8:25:13 AM 8:25:14 AM	01	1
8:25:16 AM 8:25:17 AM	01	1
8:25:23 AM 8:25:27 AM	04	4
8:25:28 AM 8:25:30 AM	02	2
8:25:31 AM 8:26:12 AM	41	41
8:26:13 AM 8:26:20 AM	07	7
8:26:21 AM 8:26:23 AM	02	2
8:26:24 AM 8:26:25 AM	01	1
8:26:31 AM 8:26:33 AM	02	2
8:26:34 AM 8:26:42 AM	08	8
8:26:45 AM 8:26:49 AM	04	4
8:26:50 AM 8:26:52 AM	02	2
8:26:58 AM 8:27:09 AM	11	11
8:27:10 AM 8:27:14 AM	04	4
8:27:16 AM 8:27:18 AM	12	12
8:27:23 AM 8:27:31 AM	02	2
8:27:32 AM 8:27:48 AM	16	16
8:27:52 AM 8:28:01 AM	09	9
8:28:01 AM 8:28:13 AM	12	12
8:28:14 AM 8:28:17 AM	03	3
8:28:18 AM 8:28:21 AM	03	3
8:28:23 AM 8:28:25 AM	02	2
8:28:26 AM 8:28:28 AM	02	2
8:28:33 AM 8:28:46 AM	13	13
8:28:47 AM 8:29:10 AM	23	23
8:29:12 AM 8:29:19 AM	07	7
8:29:21 AM 8:29:18 AM	15	15
8:29:39 AM 8:29:41 AM	02	2
8:29:42 AM 8:29:45 AM	03	3
8:29:45 AM 8:29:49 AM	04	4

# **Gap Calculation for Unsignalized Intersection** **Left Turn from Minor Road to 2-Lane Major Road**

**Intersection:**    **Major St.**        **N. Main Street**  
                          **Minor St.**        **Full-Access Driveway**

**Time Studied:**    **Weekday P.M. Peak Hour**  
**Date of Study:**    **10/8/2024**

**Critical Gap:**                    **6.4**  
**Follow-Up Time:**                **3**

<b>Length of Gap (seconds)</b>	<b>Vehicles Accommodated</b>	<b>Number of Gaps Observed</b>	<b>Total Vehicles</b>
0 - 6.4	0		0
6.4 - 9.4	1	45	45
9.4 - 12.4	2	24	48
12.4 - 15.4	3	20	60
15.4 - 18.4	4	9	36
18.4 - 21.4	5	8	40
21.4 - 24.4	6	5	30
24.4 - 27.4	7	3	21
27.4+	8	4	32
<b>Total Vehicles Accommodated</b>			<b>312</b>

<b>Minimum Gap</b>	<b>Number of Cars</b>
0	0
6.4	1
9.4	2
12.4	3
15.4	4
18.4	5
21.4	6
24.4	7
27.4	8

### P.M. Minor Left-Turn GAP Analysis

**Critical** **6.4**

Follow-up:	1.0
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Start/End Time	Seconds	Gap	1 sec	2 sec	3 sec	4 sec	5 sec	6 sec	7 sec	8 sec	9 sec	10 sec	11 sec	12 sec	13 sec	14 sec	15 sec	16 sec	17 sec	18 sec	19 sec	20 sec	21 sec	22 sec	23 sec	24 sec	25 sec	26 sec	27 sec	28 sec	29 sec	30 sec	31 sec	32 sec	33 sec	34 sec	35 sec	36 sec	37 sec	38 sec	39 sec	40 sec	41 sec	42 sec	43 sec	44 sec	45 sec	46 sec	47 sec	48 sec	49 sec	50 sec	51 sec	52 sec	53 sec	54 sec	55 sec	56 sec	57 sec	58 sec	59 sec	60 sec	61 sec	62 sec	63 sec	64 sec	65 sec	66 sec	67 sec	68 sec	69 sec	70 sec	71 sec	72 sec	73 sec	74 sec	75 sec	76 sec	77 sec	78 sec	79 sec	80 sec	81 sec	82 sec	83 sec	84 sec	85 sec	86 sec	87 sec	88 sec	89 sec	90 sec	91 sec	92 sec	93 sec	94 sec	95 sec	96 sec	97 sec	98 sec	99 sec	100 sec	101 sec	102 sec	103 sec	104 sec	105 sec	106 sec	107 sec	108 sec	109 sec	110 sec	111 sec	112 sec	113 sec	114 sec	115 sec	116 sec	117 sec	118 sec	119 sec	120 sec	121 sec	122 sec	123 sec	124 sec	125 sec	126 sec	127 sec	128 sec	129 sec	130 sec	131 sec	132 sec	133 sec	134 sec	135 sec	136 sec	137 sec	138 sec	139 sec	140 sec	141 sec	142 sec	143 sec	144 sec	145 sec	146 sec	147 sec	148 sec	149 sec	150 sec	151 sec	152 sec	153 sec	154 sec	155 sec	156 sec	157 sec	158 sec	159 sec	160 sec	161 sec	162 sec	163 sec	164 sec	165 sec	166 sec	167 sec	168 sec	169 sec	170 sec	171 sec	172 sec	173 sec	174 sec	175 sec	176 sec	177 sec	178 sec	179 sec	180 sec	181 sec	182 sec	183 sec	184 sec	185 sec	186 sec	187 sec	188 sec	189 sec	190 sec	191 sec	192 sec	193 sec	194 sec	195 sec	196 sec	197 sec	198 sec	199 sec	200 sec	201 sec	202 sec	203 sec	204 sec	205 sec	206 sec	207 sec	208 sec	209 sec	210 sec	211 sec	212 sec	213 sec	214 sec	215 sec	216 sec	217 sec	218 sec	219 sec	220 sec	221 sec	222 sec	223 sec	224 sec	225 sec	226 sec	227 sec	228 sec	229 sec	230 sec	231 sec	232 sec	233 sec	234 sec	235 sec	236 sec	237 sec	238 sec	239 sec	240 sec	241 sec	242 sec	243 sec	244 sec	245 sec	246 sec	247 sec	248 sec	249 sec	250 sec	251 sec	252 sec	253 sec	254 sec	255 sec	256 sec	257 sec	258 sec	259 sec	260 sec	261 sec	262 sec	263 sec	264 sec	265 sec	266 sec	267 sec	268 sec	269 sec	270 sec	271 sec	272 sec	273 sec	274 sec	275 sec	276 sec	277 sec	278 sec	279 sec	280 sec	281 sec	282 sec	283 sec	284 sec	285 sec	286 sec	287 sec	288 sec	289 sec	290 sec	291 sec	292 sec	293 sec	294 sec	295 sec	296 sec	297 sec	298 sec	299 sec	300 sec	301 sec	302 sec	303 sec	304 sec	305 sec	306 sec	307 sec	308 sec	309 sec	310 sec	311 sec	312 sec	313 sec	314 sec	315 sec	316 sec	317 sec	318 sec	319 sec	320 sec	321 sec	322 sec	323 sec	324 sec	325 sec	326 sec	327 sec	328 sec	329 sec	330 sec	331 sec	332 sec	333 sec	334 sec	335 sec	336 sec	337 sec	338 sec	339 sec	340 sec	341 sec	342 sec	343 sec	344 sec	345 sec	346 sec	347 sec	348 sec	349 sec	350 sec	351 sec	352 sec	353 sec	354 sec	355 sec	356 sec	357 sec	358 sec	359 sec	360 sec	361 sec	362 sec	363 sec	364 sec	365 sec	366 sec	367 sec	368 sec	369 sec	370 sec	371 sec	372 sec	373 sec	374 sec	375 sec	376 sec	377 sec	378 sec	379 sec	380 sec	381 sec	382 sec	383 sec	384 sec	385 sec	386 sec	387 sec	388 sec	389 sec	390 sec	391 sec	392 sec	393 sec	394 sec	395 sec	396 sec	397 sec	398 sec	399 sec	400 sec	401 sec	402 sec	403 sec	404 sec	405 sec	406 sec	407 sec	408 sec	409 sec	410 sec	411 sec	412 sec	413 sec	414 sec	415 sec	416 sec	417 sec	418 sec	419 sec	420 sec	421 sec	422 sec	423 sec	424 sec	425 sec	426 sec	427 sec	428 sec	429 sec	430 sec	431 sec	432 sec	433 sec	434 sec	435 sec	436 sec	437 sec	438 sec	439 sec	440 sec	441 sec	442 sec	443 sec	444 sec	445 sec	446 sec	447 sec	448 sec	449 sec	450 sec	451 sec	452 sec	453 sec	454 sec	455 sec	456 sec	457 sec	458 sec	459 sec	460 sec	461 sec	462 sec	463 sec	464 sec	465 sec	466 sec	467 sec	468 sec	469 sec	470 sec	471 sec	472 sec	473 sec	474 sec	475 sec	476 sec	477 sec	478 sec	479 sec	480 sec	481 sec	482 sec	483 sec	484 sec	485 sec	486 sec	487 sec	488 sec	489 sec	490 sec	491 sec	492 sec	493 sec	494 sec	495 sec	496 sec	497 sec	498 sec	499 sec	500 sec	501 sec	502 sec	503 sec	504 sec	505 sec	506 sec	507 sec	508 sec	509 sec	510 sec	511 sec	512 sec	513 sec	514 sec	515 sec	516 sec	517 sec	518 sec	519 sec	520 sec	521 sec	522 sec	523 sec	524 sec	525 sec	526 sec	527 sec	528 sec	529 sec	530 sec	531 sec	532 sec	533 sec	534 sec	535 sec	536 sec	537 sec	538 sec	539 sec	540 sec	541 sec	542 sec	543 sec	544 sec	545 sec	546 sec	547 sec	548 sec	549 sec	550 sec	551 sec	552 sec	553 sec	554 sec	555 sec	556 sec	557 sec	558 sec	559 sec	560 sec	561 sec	562 sec	563 sec	564 sec	565 sec	566 sec	567 sec	568 sec	569 sec	570 sec	571 sec	572 sec	573 sec	574 sec	575 sec	576 sec	577 sec	578 sec	579 sec	580 sec	581 sec	582 sec	583 sec	584 sec	585 sec	586 sec	587 sec	588 sec	589 sec	590 sec	591 sec	592 sec	593 sec	594 sec	595 sec	596 sec	597 sec	598 sec	599 sec	600 sec	601 sec	602 sec	603 sec	604 sec	605 sec	606 sec	607 sec	608 sec	609 sec	610 sec	611 sec	612 sec	613 sec	614 sec	615 sec	616 sec	617 sec	618 sec	619 sec	620 sec	621 sec	622 sec	623 sec	624 sec	625 sec	626 sec	627 sec	628 sec	629 sec	630 sec	631 sec	632 sec	633 sec	634 sec	635 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sec	886 sec	887 sec	888 sec	889 sec	890 sec	891 sec	892 sec	893 sec	894 sec	895 sec	896 sec	897 sec	898 sec	899 sec	900 sec	901 sec	902 sec	903 sec	904 sec	905 sec	906 sec	907 sec	908 sec	909 sec	910 sec	911 sec	912 sec	913 sec	914 sec	915 sec	916 sec	917 sec	918 sec	919 sec	920 sec	921 sec	922 sec	923 sec	924 sec	925 sec	926 sec	927 sec	928 sec	929 sec	930 sec	931 sec	932 sec	933 sec	934 sec	935 sec	936 sec	937 sec	938 sec	939 sec	940 sec	941 sec	942 sec	943 sec	944 sec	945 sec	946 sec	947 sec	948 sec	949 sec	950 sec	951 sec	952 sec	953 sec	954 sec	955 sec	956 sec	957 sec	958 sec	959 sec	960 sec	961 sec	962 sec	963 sec	964 sec	965 sec	966 sec	967 sec	968 sec	969 sec	970 sec	971 sec	972 sec	973 sec	974 sec	975 sec	976 sec	977 sec	978 sec	979 sec	980 sec	981 sec	982 sec	983 sec	984 sec	985 sec	986 sec	987 sec	988 sec	989 sec	990 sec	991 sec	992 sec	993 sec	994 sec	995 sec	996 sec	997 sec	998 sec	999 sec	1000 sec
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# **Gap Calculation for Unsignalized Intersection** **Left Turn from Minor Road to 2-Lane Major Road**

**Intersection:**    **Major St.**        **N. Main Street**  
                          **Minor St.**        **Full-Access Driveway**

**Time Studied:**    **Weekday P.M. Peak Hour**  
**Date of Study:**    **10/8/2024**

**Critical Gap:**                    **6.4**  
**Follow-Up Time:**                **3**

<b>Length of Gap (seconds)</b>	<b>Vehicles Accommodated</b>	<b>Number of Gaps Observed</b>	<b>Total Vehicles</b>
0 - 6.4	0		0
6.4 - 9.4	1	45	45
9.4 - 12.4	2	24	48
12.4 - 15.4	3	20	60
15.4 - 18.4	4	9	36
18.4 - 21.4	5	8	40
21.4 - 24.4	6	5	30
24.4 - 27.4	7	3	21
27.4+	8	4	32
<b>Total Vehicles Accommodated</b>			<b>312</b>

<b>Minimum Gap</b>	<b>Number of Cars</b>
0	0
6.4	1
9.4	2
12.4	3
15.4	4
18.4	5
21.4	6
24.4	7
27.4	8

P.M. Minor Left-Turn GAP Analysis

Critical: 4.4  
Follow-up: 3.0

Start/End Time Secondary

Gap:	1	44	30	30
1 sec	1	44	30	30
2 sec	2	72	30	30
3 sec	3	45	30	30
4 sec	4	30	30	30
5 sec	5	216	30	30
6 sec	6	144	30	30
7 sec	7	135	45	45
8 sec	8	137	30	30
9 sec	9	127	30	30
10 sec	10	108	30	40
11 sec	11	54	30	30
12 sec	12	36	30	30
13 sec	13	18	30	30
14 sec	14	27	30	30
15 sec	15	36	30	30
16 sec	16	144	30	30
17 sec	17	36	30	30
18 sec	18	36	30	30
19 sec	19	36	30	30
20 sec	20	36	30	30
21 sec	21	36	30	30
22 sec	22	216	30	30
23 sec	23	27	30	30
24 sec	24	27	30	30
25 sec	25	27	30	30
26 sec	26	27	30	30
27 sec	27	36	30	30
28 sec	28	36	30	30
29 sec	29	36	30	30
30 sec	30	36	30	30
31 sec	31	36	30	30
32 sec	32	36	30	30
33 sec	33	36	30	30
34 sec	34	36	30	30
35 sec	35	36	30	30
36 sec	36	36	30	30
37 sec	37	36	30	30
38 sec	38	36	30	30
39 sec	39	36	30	30
40 sec	40	36	30	30
41 sec	41	36	30	30
42 sec	42	36	30	30
43 sec	43	36	30	30
44 sec	44	36	30	30
45 sec	45	36	30	30
46 sec	46	36	30	30
47 sec	47	36	30	30
48 sec	48	36	30	30
49 sec	49	36	30	30
50 sec	50	36	30	30
51 sec	51	36	30	30
52 sec	52	36	30	30
53 sec	53	36	30	30
54 sec	54	36	30	30
55 sec	55	36	30	30
56 sec	56	36	30	30
57 sec	57	36	30	30
58 sec	58	36	30	30
59 sec	59	36	30	30
60 sec	60	36	30	30
61 min	61	36	30	30

Total Cars: 317



4:36:16 PM 4:36:18 PM	02	2
4:36:27 PM 4:36:34 PM	07	7
4:36:39 PM 4:36:46 PM	07	7
4:36:51 PM 4:36:55 PM	04	4
4:36:56 PM 4:37:00 PM	04	4
4:37:01 PM 4:37:05 PM	08	8
4:37:40 PM 4:37:42 PM	02	2
4:37:43 PM 4:37:44 PM	01	1
4:37:46 PM 4:37:47 PM	01	1
4:37:49 PM 4:37:51 PM	02	2
4:37:52 PM 4:37:57 PM	05	5
4:38:00 PM 4:38:14 PM	14	14
4:38:16 PM 4:38:19 PM	03	3
4:38:19 PM 4:38:22 PM	03	3
4:39:13 PM 4:39:15 PM	03	3
4:39:17 PM 4:39:19 PM	02	2
4:39:23 PM 4:39:24 PM	01	1
4:39:27 PM 4:39:31 PM	04	4
4:39:32 PM 4:39:35 PM	03	3
4:40:43 PM 4:40:46 PM	03	3
4:40:53 PM 4:40:55 PM	02	2
4:40:55 PM 4:41:04 PM	09	9
4:41:05 PM 4:41:07 PM	02	2
4:41:08 PM 4:41:10 PM	02	2
4:41:34 PM 4:42:03 PM	29	29
4:42:08 PM 4:42:09 PM	01	1
4:42:10 PM 4:42:12 PM	02	2
4:42:13 PM 4:42:14 PM	01	1
4:42:30 PM 4:42:34 PM	04	4
4:42:34 PM 4:42:38 PM	04	4
4:42:41 PM 4:42:59 PM	18	18
4:43:02 PM 4:43:20 PM	18	18
4:43:20 PM 4:43:22 PM	02	2
4:43:25 PM 4:43:30 PM	05	5
4:43:31 PM 4:43:32 PM	01	1
4:43:40 PM 4:43:53 PM	13	13
4:43:53 PM 4:44:06 PM	13	13
4:44:07 PM 4:44:30 PM	23	23
4:44:31 PM 4:44:32 PM	01	1
4:44:33 PM 4:44:18 PM	05	5
4:44:39 PM 4:44:49 PM	10	10
4:44:50 PM 4:44:54 PM	04	4
4:44:54 PM 4:44:56 PM	02	2
4:44:56 PM 4:45:00 PM	04	4
4:45:00 PM 4:45:03 PM	03	3
4:45:04 PM 4:45:06 PM	02	2
4:45:06 PM 4:45:12 PM	06	6
4:45:14 PM		

4:43:16 PM	02	2
4:43:30 PM		
4:43:40 PM	10	10
4:43:41 PM		
4:43:42 PM	01	1
4:43:43 PM		
4:43:55 PM	12	12
4:43:56 PM		
4:43:57 PM	01	1
4:43:58 PM		
4:44:00 PM	02	2
4:44:01 PM		
4:44:18 PM	17	17
4:44:19 PM		
4:44:22 PM	03	3
4:44:23 PM		
4:44:22 PM		
4:44:30 PM	14	14
4:44:30 PM		
4:44:37 PM		
4:44:42 PM	05	5
4:44:45 PM		
4:44:47 PM	02	2
4:44:47 PM		
4:44:56 PM	09	9
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4:47:17 PM	05	5
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4:47:44 PM	01	1
4:47:45 PM		
4:47:47 PM	02	2
4:47:47 PM		
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4:47:50 PM	03	3
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4:47:50 PM	01	1
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5:18:23 PM 5:18:43 PM	20	20
5:18:44 PM 5:18:52 PM	08	8
5:18:53 PM 5:18:57 PM	04	4
5:18:58 PM 5:18:59 PM	01	1
5:18:59 PM 5:19:03 PM	08	8
5:19:07 PM 5:19:18 PM	41	41
5:19:48 PM 5:19:53 PM	04	4
5:19:55 PM 5:19:57 PM	02	2
5:19:57 PM 5:20:01 PM	04	4
5:20:01 PM 5:20:04 PM	03	3
5:20:05 PM 5:20:10 PM	05	5
5:20:11 PM 5:20:15 PM	04	4
5:20:17 PM 5:20:18 PM	01	1
5:20:19 PM 5:20:36 PM	17	17
5:20:40 PM 5:20:47 PM	07	7
5:21:00 PM 5:21:02 PM	02	2
5:21:11 PM 5:21:13 PM	02	2
5:21:14 PM 5:21:17 PM	03	3
5:21:18 PM 5:21:21 PM	03	3
5:21:23 PM 5:21:25 PM	02	2
5:21:25 PM 5:21:34 PM	09	9
5:21:38 PM 5:21:45 PM	07	7
5:22:08 PM 5:22:11 PM	03	3
5:22:19 PM 5:22:26 PM	07	7
5:22:27 PM 5:22:30 PM	03	3
5:22:31 PM 5:22:31 PM	00	0
5:22:36 PM 5:22:37 PM	01	1
5:22:38 PM 5:22:41 PM	03	3
5:22:42 PM 5:22:56 PM	14	14
5:22:57 PM 5:23:04 PM	07	7



5:07:40 PM	01	1
5:07:41 PM		
5:07:42 PM	01	1
5:07:43 PM		
5:07:52 PM	10	10
5:07:53 PM		
5:08:08 PM	15	15
5:08:09 PM		
5:08:11 PM	02	2
5:08:12 PM		
5:08:14 PM	02	2
5:08:15 PM		
5:08:24 PM	09	9
5:08:24 PM		
5:08:29 PM	05	5
5:08:30 PM		
5:08:38 PM	08	8
5:08:39 PM		
5:08:42 PM	03	3
5:08:46 PM		
5:08:49 PM	03	3
5:08:57 PM		
5:09:00 PM	03	3
5:09:02 PM		
5:09:06 PM	04	4
5:09:08 PM		
5:09:11 PM	03	3
5:09:12 PM		
5:09:21 PM	09	9
5:09:21 PM		
5:09:28 PM	07	7
5:09:28 PM		
5:09:49 PM	11	11
5:10:00 PM		
5:10:05 PM	05	5
5:10:06 PM		
5:10:06 PM	00	0
5:10:07 PM		
5:10:13 PM	06	6
5:10:13 PM		
5:10:20 PM	07	7
5:10:21 PM		
5:10:23 PM	02	2
5:10:23 PM		
5:10:27 PM	04	4
5:10:33 PM		
5:10:36 PM	03	3
5:10:37 PM		
5:10:39 PM	02	2
5:10:40 PM		
5:10:44 PM	04	4
5:10:44 PM		
5:10:55 PM	11	11
5:10:56 PM		
5:11:18 PM	21	21
5:11:20 PM		
5:11:27 PM	07	7
5:11:28 PM		
5:11:32 PM	04	4
5:11:33 PM		
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5:11:59 PM		
5:12:01 PM	02	2
5:12:03 PM		
5:12:03 PM	01	1
5:12:07 PM		
5:12:08 PM	01	1
5:12:14 PM		
5:12:33 PM	19	19
5:12:34 PM		
5:12:37 PM	03	3
5:12:40 PM		
5:12:43 PM	03	3
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5:13:38 PM		
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5:13:52 PM	02	2
5:13:52 PM		
5:13:55 PM	07	7
5:14:00 PM		
5:14:18 PM	18	18
5:14:19 PM		
5:14:23 PM	04	4
5:14:24 PM		
5:14:42 PM	18	18
5:14:59 PM		
5:15:04 PM	05	5
5:15:05 PM		
5:15:08 PM	03	3

5:15:08 PM 5:15:22 PM	14	14
5:15:24 PM 5:15:32 PM	08	8
5:15:34 PM 5:15:37 PM	03	3
5:15:38 PM 5:15:40 PM	02	2
5:15:40 PM 5:15:43 PM	03	3
5:15:44 PM 5:15:59 PM	15	15
5:16:05 PM 5:16:17 PM	12	12
5:16:11 PM 5:16:42 PM	11	11
5:16:43 PM 5:16:55 PM	12	12
5:16:56 PM 5:17:04 PM	08	8
5:17:04 PM 5:17:09 PM	05	5
5:17:10 PM 5:17:12 PM	02	2
5:17:13 PM 5:17:14 PM	01	1
5:17:16 PM 5:17:29 PM	13	13
5:17:55 PM 5:17:57 PM	02	2
5:17:59 PM 5:18:03 PM	04	4
5:18:07 PM 5:18:13 PM	05	5
5:18:14 PM 5:18:23 PM	09	9
5:18:23 PM 5:18:43 PM	20	20
5:18:44 PM 5:18:52 PM	08	8
5:18:53 PM 5:18:57 PM	04	4
5:18:58 PM 5:18:59 PM	01	1
5:18:59 PM 5:19:07 PM	08	8
5:19:07 PM 5:19:18 PM	41	41
5:19:19 PM 5:19:53 PM	04	4
5:19:55 PM 5:19:57 PM	02	2
5:19:57 PM 5:20:01 PM	04	4
5:20:01 PM 5:20:04 PM	03	3
5:20:05 PM 5:20:10 PM	05	5
5:20:11 PM 5:20:15 PM	04	4
5:20:17 PM 5:20:18 PM	01	1
5:20:19 PM 5:20:18 PM	17	17
5:20:40 PM 5:20:47 PM	07	7
5:21:00 PM 5:21:02 PM	02	2
5:21:11 PM 5:21:13 PM	02	2
5:21:14 PM 5:21:17 PM	03	3
5:21:18 PM 5:21:21 PM	03	3
5:21:23 PM 5:21:25 PM	02	2
5:21:25 PM 5:21:34 PM	09	9
5:21:38 PM 5:21:45 PM	07	7
5:22:08 PM 5:22:11 PM	03	3
5:22:19 PM 5:22:26 PM	07	7
5:22:27 PM 5:22:30 PM	03	3
5:22:31 PM 5:22:31 PM	00	0
5:22:36 PM 5:22:37 PM	01	1
5:22:38 PM 5:22:41 PM	03	3
5:22:42 PM 5:22:56 PM	14	14
5:22:57 PM 5:23:04 PM	07	7

5:23:07 PM	24	24
5:23:31 PM		
5:23:32 PM	16	18
5:23:48 PM		
5:24:03 PM	03	1
5:24:06 PM		
5:24:07 PM		
5:24:10 PM	03	1
5:24:11 PM	01	1
5:24:12 PM		
5:24:14 PM		
5:24:21 PM	07	7
5:24:22 PM	37	37
5:24:59 PM		
5:25:00 PM		
5:25:07 PM	07	7
5:25:10 PM		
5:25:11 PM	01	1
5:25:12 PM	01	1
5:25:13 PM		
5:25:14 PM	01	1
5:25:15 PM		
5:25:20 PM	05	5
5:25:25 PM		
5:25:28 PM		
5:25:31 PM	05	5
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5:25:41 PM		
5:25:41 PM	11	11
5:25:52 PM		
5:25:52 PM	03	3
5:25:55 PM		
5:25:56 PM		
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5:26:25 PM	09	9
5:26:34 PM		
5:26:35 PM	03	3
5:26:38 PM		
5:26:39 PM	06	6
5:26:45 PM		
5:26:47 PM		
5:26:49 PM	02	2
5:27:02 PM		
5:27:11 PM	09	9
5:27:12 PM	01	1
5:27:13 PM		
5:27:15 PM	08	8
5:27:23 PM		
5:27:27 PM	09	9
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5:27:37 PM		
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5:27:40 PM	06	6
5:27:46 PM		
5:28:10 PM	02	2
5:28:12 PM		
5:28:14 PM	19	19
5:28:33 PM		
5:28:34 PM		
5:28:35 PM	01	1
5:28:43 PM	08	8
5:28:51 PM		
5:28:52 PM	25	26
5:29:18 PM		
5:29:19 PM	05	5
5:29:24 PM		
5:29:25 PM		
5:29:27 PM	02	2

## **APPENDIX H:**

### **Auxiliary Turn Lane Warrant Analyses**



5:23:07 PM	24	24
5:23:31 PM		
5:23:32 PM	16	16
5:23:40 PM		
5:24:03 PM	03	3
5:24:06 PM		
5:24:07 PM	03	3
5:24:10 PM		
5:24:11 PM	01	1
5:24:12 PM		
5:24:14 PM	07	7
5:24:21 PM		
5:24:22 PM	37	37
5:24:39 PM		
5:25:00 PM	07	7
5:25:07 PM		
5:25:10 PM	01	1
5:25:11 PM		
5:25:12 PM	01	1
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5:25:14 PM	01	1
5:25:15 PM		
5:25:20 PM	05	5
5:25:25 PM		
5:25:26 PM	05	5
5:25:31 PM		
5:25:39 PM	02	2
5:25:41 PM		
5:25:41 PM	11	11
5:25:52 PM		
5:25:52 PM	03	3
5:25:55 PM		
5:25:56 PM	13	13
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5:27:40 PM	06	6
5:27:46 PM		
5:28:10 PM	02	2
5:28:12 PM		
5:28:14 PM	19	19
5:28:11 PM		
5:28:34 PM	01	1
5:28:35 PM		
5:28:43 PM	08	8
5:28:51 PM		
5:28:52 PM	26	26
5:29:18 PM		
5:29:19 PM	05	5
5:29:24 PM		
5:29:25 PM	02	2
5:29:27 PM		



## **APPENDIX H:**

### **Auxiliary Turn Lane Warrant Analyses**



# Turn Lane Warrant and Length Analysis Workbook

## STUDY LOCATION AND ANALYSIS INFORMATION

Municipality:	Hatfield Borough	Analysis Date:	10/17/2024
County:	Montgomery County	Conducted By:	MF
PennDOT Engineering District:	6	Checked By:	PHS
		Agency/Company Name:	Traffic Planning and Design, Inc.
Intersection & Approach Description: N. Main Street & Proposed Site Driveway			
Analysis Period:	2026 Projected Conditions	Number of Approach Lanes:	1
Design Hour:	Weekday A.M. Peak Hour	Undivided or Divided Highway:	Undivided
Intersection Control:	Unsignalized	Type of Analysis:	Left Turn Lane
Posted Speed Limit (MPH):	25	Left or Right-Turn Lane Analysis?:	Left Turn Lane
Type of Terrain:	Level		

## VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	0	2.0%	0
	Through	-	502	2.0%	508
	Right	No			N/A
Opposing	Left	No			N/A
	Through	-	279	5.0%	286
	Right	Yes	1	2.0%	2

Advancing Volume: 508  
Opposing Volume: 288  
Left Turn Volume: 0

% Left Turns in Advancing Volume: 0.00%

Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	No			N/A
	Through	-	279	5.0%	N/A
	Right	-	1	2.0%	N/A

Advancing Volume: N/A  
Right Turn Volume: N/A

## TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings		Right Turn Lane Warrant Findings	
Applicable Warrant Figure:	Figure 1	Applicable Warrant Figure:	N/A
Warrant Met?:	#DIV/0!	Warrant Met?:	N/A

## TURN LANE LENGTH CALCULATIONS

Intersection Control:	Unsignalized
Design Hour Volume of Turning Lane:	0
Cycles Per Hour (Assumed):	60
Cycles Per Hour (If Known):	60

Average # of Vehicles/Cycle: #DIV/0!

PennDOT Publication 46, Exhibit 11-6

Type of Traffic Control	Speed (MPH)					
	25-35		40-45		50-60	
	High	Low	High	Low	High	Low
Signalized	A	A	B or C	B or C	B or C	B or C
Unsignalized	A	A	C	B	B or C	B

Left Turn Lane Storage Length, Condition A: #DIV/0! Feet

Condition B: #DIV/0! Feet

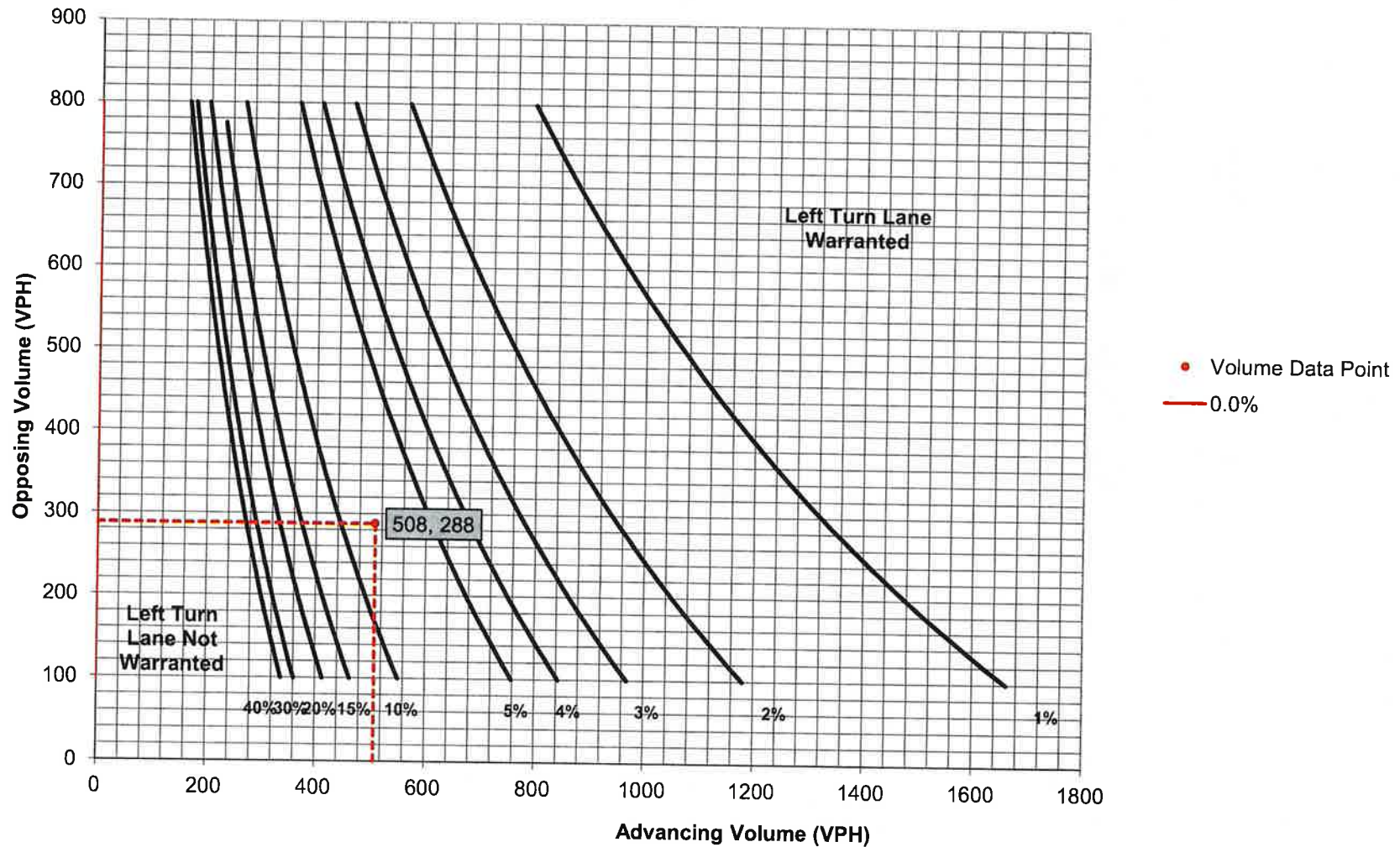
Condition C: #DIV/0! Feet

Required Left Turn Lane Storage Length: #DIV/0! Feet

Additional Findings: #DIV/0!

Additional Comments / Justifications:

**Figure 1. Warrant for left turn lanes on two-lane roadways**  
 (speeds to 35 mph, unsignalized and signalized intersections)  
 (L = % Left Turns in Advancing Volume)



# Turn Lane Warrant and Length Analysis Workbook

## STUDY LOCATION AND ANALYSIS INFORMATION

Municipality:	Hatfield Borough	Analysis Date:	10/17/2024
County:	Montgomery County	Conducted By:	MF
PennDOT Engineering District:	6	Checked By:	PHS
		Agency/Company Name:	Traffic Planning and Design, Inc.
Intersection & Approach Description: N. Main Street & Proposed Site Driveway			
Analysis Period:	2026 Projected Conditions	Number of Approach Lanes:	1
Design Hour:	Weekday A.M. Peak Hour	Undivided or Divided Highway:	Undivided
Intersection Control:	Unsignalized	Type of Analysis:	Left Turn Lane
Posted Speed Limit (MPH):	25	Left or Right-Turn Lane Analysis?:	Left Turn Lane
Type of Terrain:	Level		

## VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	0	2.0%	0
	Through	-	502	2.0%	508
	Right	No			N/A
Opposing	Left	No			N/A
	Through	-	279	5.0%	286
	Right	Yes	1	2.0%	2
Advancing Volume: 508					
Opposing Volume: 288					
Left Turn Volume: 0					
% Left Turns in Advancing Volume: 0.00%					
Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	No			N/A
	Through	-	279	5.0%	N/A
	Right	-	1	2.0%	N/A
Advancing Volume: N/A					
Right Turn Volume: N/A					

## TURN LANE WARRANT FINDINGS

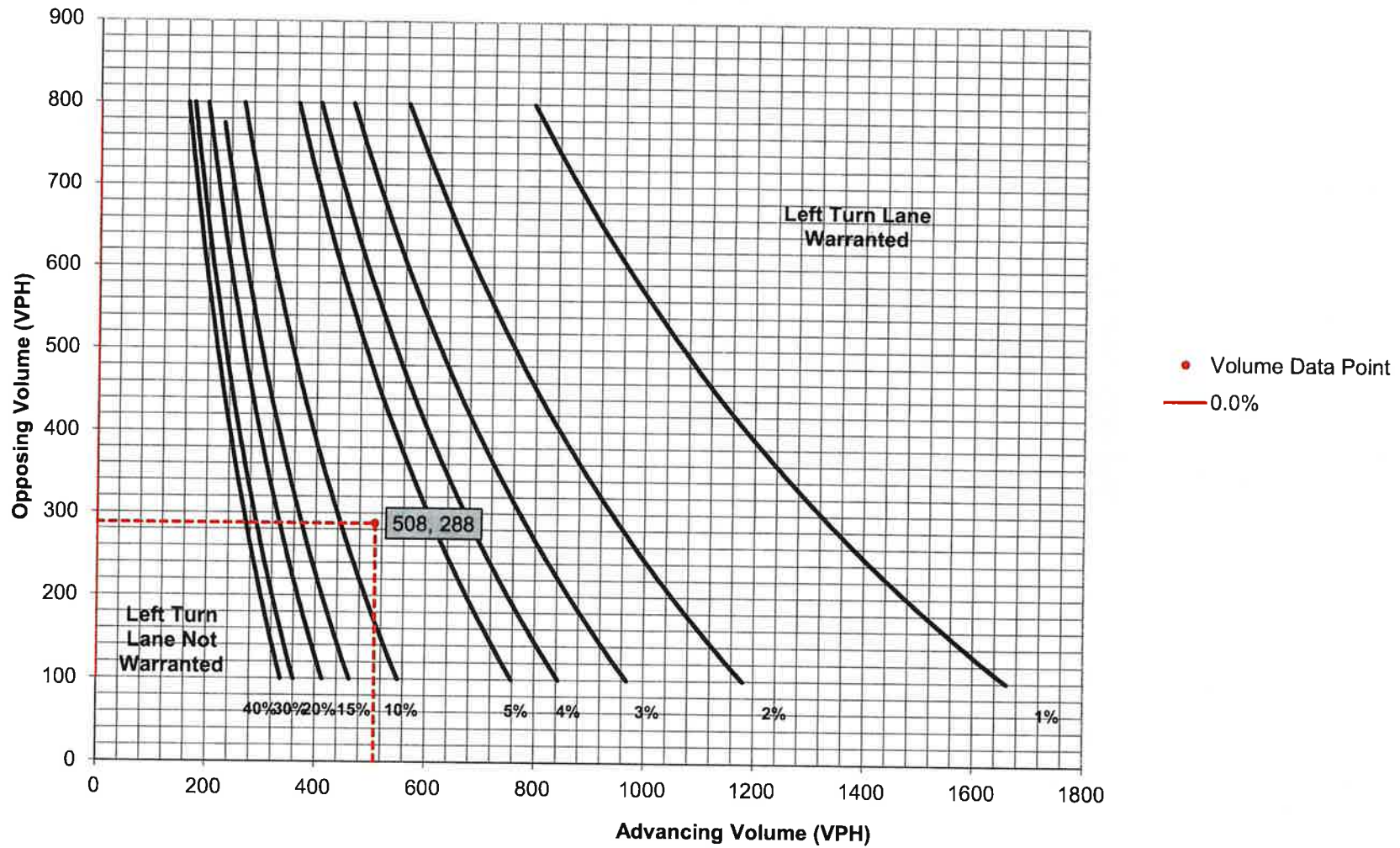
Left Turn Lane Warrant Findings	Right Turn Lane Warrant Findings
Applicable Warrant Figure: <b>Figure 1</b>	Applicable Warrant Figure: <b>N/A</b>
Warrant Met?: <b>#DIV/0!</b>	Warrant Met?: <b>N/A</b>

## TURN LANE LENGTH CALCULATIONS

Intersection Control:	Unsignalized	Average # of Vehicles/Cycle:	#DIV/0!			
Design Hour Volume of Turning Lane:	0					
Cycles Per Hour (Assumed):	60					
Cycles Per Hour (If Known):	60					
PennDOT Publication 46, Exhibit 11-6						
Type of Traffic Control	Speed (MPH)					
	25-35		40-45		50-60	
	Turn Demand Volume					
	High	Low	High	Low	High	Low
Signalized	A	A	B or C	B or C	B or C	B or C
Unsignalized	A	A	C	B	B or C	B
Left Turn Lane Storage Length, Condition A:		#DIV/0!	Feet			
Condition B:		#DIV/0!	Feet			
Condition C:		#DIV/0!	Feet			
Required Left Turn Lane Storage Length:		#DIV/0!	Feet			
Additional Findings:		#DIV/0!				
Additional Comments / Justifications:						



**Figure 1. Warrant for left turn lanes on two-lane roadways**  
**(speeds to 35 mph, unsignalized and signalized intersections)**  
 (L = % Left Turns in Advancing Volume)





# Turn Lane Warrant and Length Analysis Workbook

## STUDY LOCATION AND ANALYSIS INFORMATION

Municipality:	Hatfield Borough	Analysis Date:	10/17/2024
County:	Montgomery County	Conducted By:	MF
PennDOT Engineering District:	6	Checked By:	PHS
		Agency/Company Name:	Traffic Planning and Design, Inc.

Intersection & Approach Description: N. Main Street & Proposed Site Driveway

Analysis Period:	2026 Projected Conditions	Number of Approach Lanes:	1
Design Hour:	Weekday A.M. Peak Hour	Undivided or Divided Highway:	Undivided
Intersection Control:	Unsignalized		
Posted Speed Limit (MPH):	25	Type of Analysis:	Right Turn Lane
Type of Terrain:	Level	Left or Right-Turn Lane Analysis?:	Right Turn Lane

## VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	0	2.0%	N/A
	Through	-	502	2.0%	N/A
	Right	No			N/A
Opposing	Left	No			N/A
	Through	-	279	5.0%	N/A
	Right	Yes	1	2.0%	N/A

Advancing Volume: N/A  
Opposing Volume: N/A  
Left Turn Volume: N/A

% Left Turns in Advancing Volume: N/A

Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	No			N/A
	Through	-	279	5.0%	286
	Right	-	1	2.0%	2

Advancing Volume: 288  
Right Turn Volume: 2

## TURN LANE WARRANT FINDINGS

<b>Left Turn Lane Warrant Findings</b> Applicable Warrant Figure: N/A Warrant Met?: N/A	<b>Right Turn Lane Warrant Findings</b> Applicable Warrant Figure: Figure 9 Warrant Met?: No
---	--

## TURN LANE LENGTH CALCULATIONS

Intersection Control:	Unsignalized	Average # of Vehicles/Cycle:	N/A
Design Hour Volume of Turning Lane:	2		
Cycles Per Hour (Assumed):	60		
Cycles Per Hour (If Known):	60		

PennDOT Publication 46, Exhibit 11-6

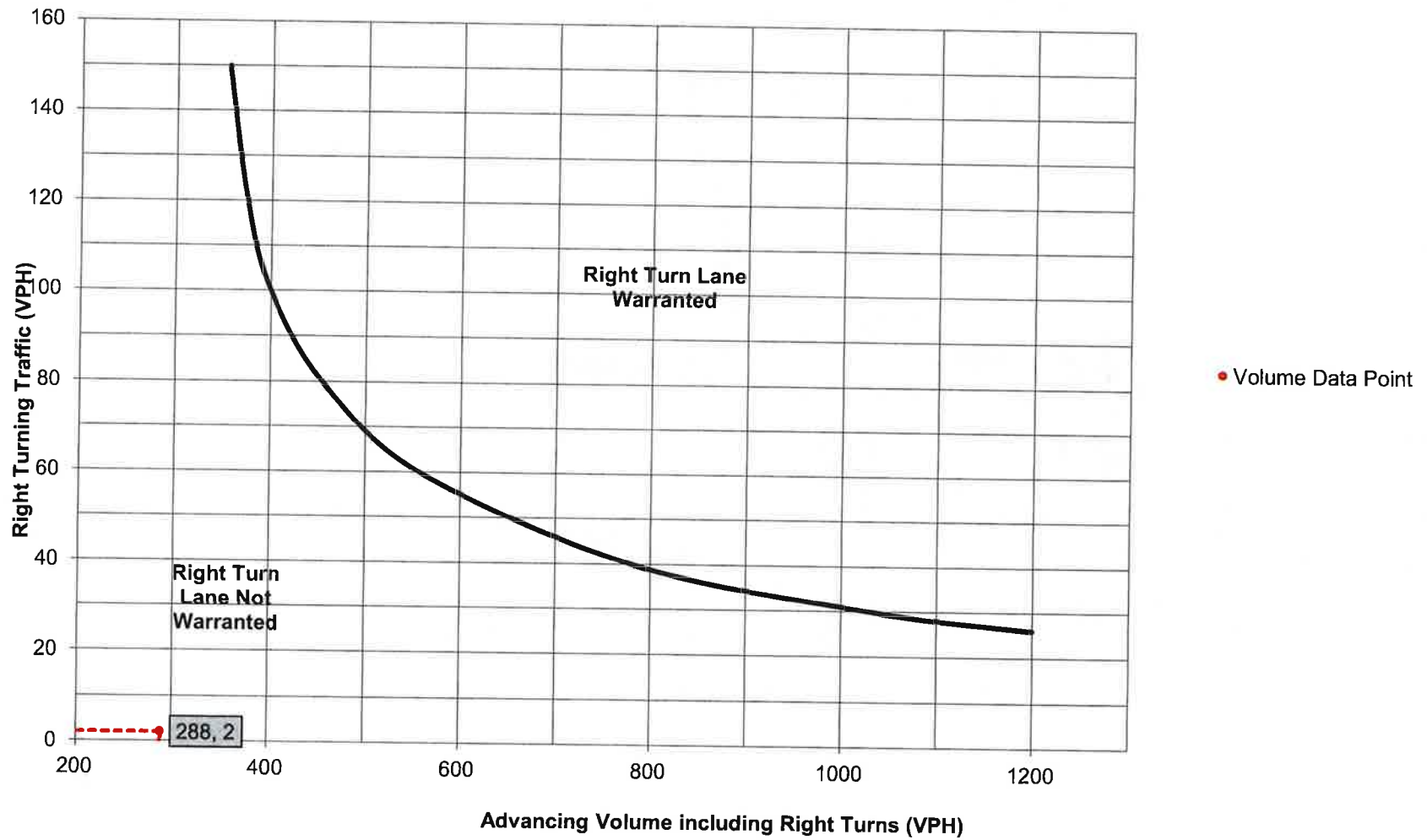
Type of Traffic Control	Speed (MPH)					
	25-35		40-45		50-60	
	High	Low	High	Low	High	Low
Signalized	A	A	B or C	B or C	B or C	B or C
Unsignalized	A	A	C	B	B or C	B

Right Turn Lane Storage Length, Condition A: N/A Feet  
Condition B: N/A Feet  
Condition C: N/A Feet  
Required Right Turn Lane Storage Length: N/A Feet

Additional Findings: N/A

Additional Comments / Justifications:

**Figure 9. Warrant for right turn lanes on two-lane roadways  
(40 mph or lower speeds, unsignalized and signalized intersections)**



# Turn Lane Warrant and Length Analysis Workbook

## STUDY LOCATION AND ANALYSIS INFORMATION

Municipality:	Hatfield Borough	Analysis Date:	10/17/2024
County:	Montgomery County	Conducted By:	MF
PennDOT Engineering District:	6	Checked By:	PHS
		Agency/Company Name:	Traffic Planning and Design, Inc.
Intersection & Approach Description: N. Main Street & Proposed Site Driveway			
Analysis Period:	2026 Projected Conditions	Number of Approach Lanes:	1
Design Hour:	Weekday A.M. Peak Hour	Undivided or Divided Highway:	Undivided
Intersection Control:	Unsignalized	Type of Analysis Right Turn Lane	
Posted Speed Limit (MPH):	25		
Type of Terrain:	Level		
		Left or Right-Turn Lane Analysis?:	Right Turn Lane

## VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	0	2.0%	N/A
	Through	-	502	2.0%	N/A
	Right	No			N/A
Opposing	Left	No			N/A
	Through	-	279	5.0%	N/A
	Right	Yes	1	2.0%	N/A

Advancing Volume: N/A  
Opposing Volume: N/A  
Left Turn Volume: N/A

% Left Turns in Advancing Volume: N/A

Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	No			N/A
	Through	-	279	5.0%	286
	Right	-	1	2.0%	2

Advancing Volume: 288  
Right Turn Volume: 2

## TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings	Right Turn Lane Warrant Findings
Applicable Warrant Figure: N/A	Applicable Warrant Figure: Figure 9
Warrant Met?: N/A	Warrant Met?: No

## TURN LANE LENGTH CALCULATIONS

Intersection Control:	Unsignalized
Design Hour Volume of Turning Lane:	2
Cycles Per Hour (Assumed):	60
Cycles Per Hour (If Known):	60

Average # of Vehicles/Cycle: N/A

PennDOT Publication 46, Exhibit 11-6

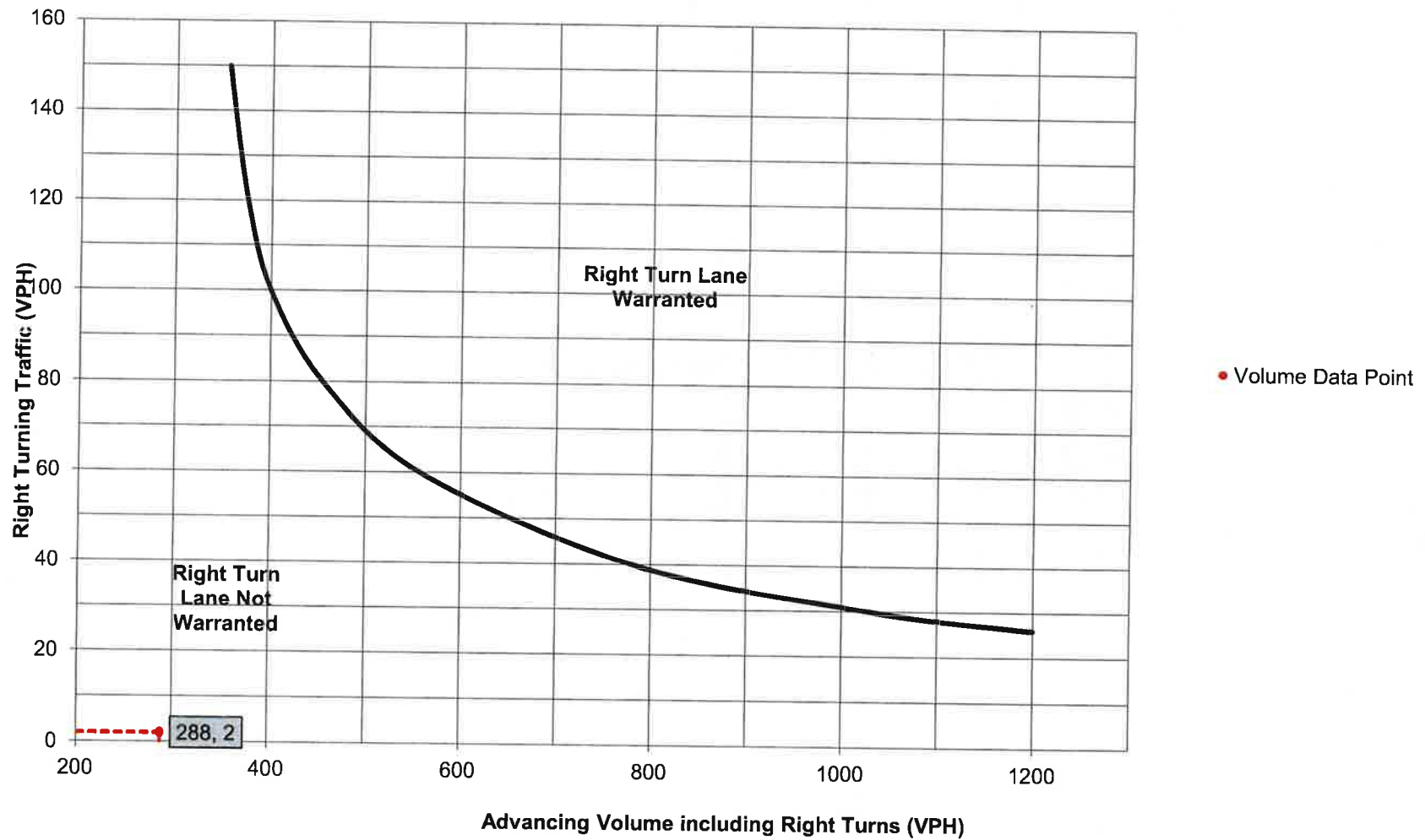
Type of Traffic Control	Speed (MPH)					
	25-35		40-45		50-60	
	High	Low	High	Low	High	Low
Signalized	A	A	B or C	B or C	B or C	B or C
Unsignalized	A	A	C	B	B or C	B

Right Turn Lane Storage Length, Condition A: N/A Feet  
Condition B: N/A Feet  
Condition C: N/A Feet  
Required Right Turn Lane Storage Length: N/A Feet

Additional Findings: N/A

Additional Comments / Justifications:

**Figure 9. Warrant for right turn lanes on two-lane roadways  
(40 mph or lower speeds, unsignalized and signalized intersections)**





# Turn Lane Warrant and Length Analysis Workbook

## STUDY LOCATION AND ANALYSIS INFORMATION

Municipality:	Hatfield Borough	Analysis Date:	10/17/2024
County:	Montgomery County	Conducted By:	MF
PennDOT Engineering District:	6	Checked By:	PHS
		Agency/Company Name:	Traffic Planning and Design, Inc.
Intersection & Approach Description: N, Main Street & Proposed Site Driveway			
Analysis Period:	2026 Projected Conditions	Number of Approach Lanes:	1
Design Hour:	Weekday P.M. Peak Hour	Undivided or Divided Highway:	Undivided
Intersection Control:	Unsignalized	Type of Analysis: <b>Left Turn Lane</b>	
Posted Speed Limit (MPH):	25		
Type of Terrain:	Level		
		Left or Right-Turn Lane Analysis?:	Left Turn Lane

## VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	1	2.0%	2
	Through	-	459	2.0%	464
	Right	No			N/A
Opposing	Left	No			N/A
	Through	-	496	1.0%	499
	Right	Yes	2	2.0%	3

Advancing Volume: 466  
Opposing Volume: 502  
Left Turn Volume: 2

% Left Turns in Advancing Volume: 0.43%

Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	No			N/A
	Through	-	496	1.0%	N/A
	Right	-	2	2.0%	N/A

Advancing Volume: N/A  
Right Turn Volume: N/A

## TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings		Right Turn Lane Warrant Findings	
Applicable Warrant Figure:	<b>Figure 1</b>	Applicable Warrant Figure:	<b>N/A</b>
Warrant Met?:	<b>No</b>	Warrant Met?:	<b>N/A</b>

## TURN LANE LENGTH CALCULATIONS

Intersection Control:	Unsignalized
Design Hour Volume of Turning Lane:	2
Cycles Per Hour (Assumed):	60
Cycles Per Hour (If Known):	60
Average # of Vehicles/Cycle:	N/A

Penndot Publication 46, Exhibit 11-6

Type of Traffic Control	Speed (MPH)					
	25-35		40-45		50-60	
	Turn Demand Volume					
	High	Low	High	Low	High	Low
Signalized	A	A	B or C	B or C	B or C	B or C
Unsignalized	A	A	C	B	B or C	B

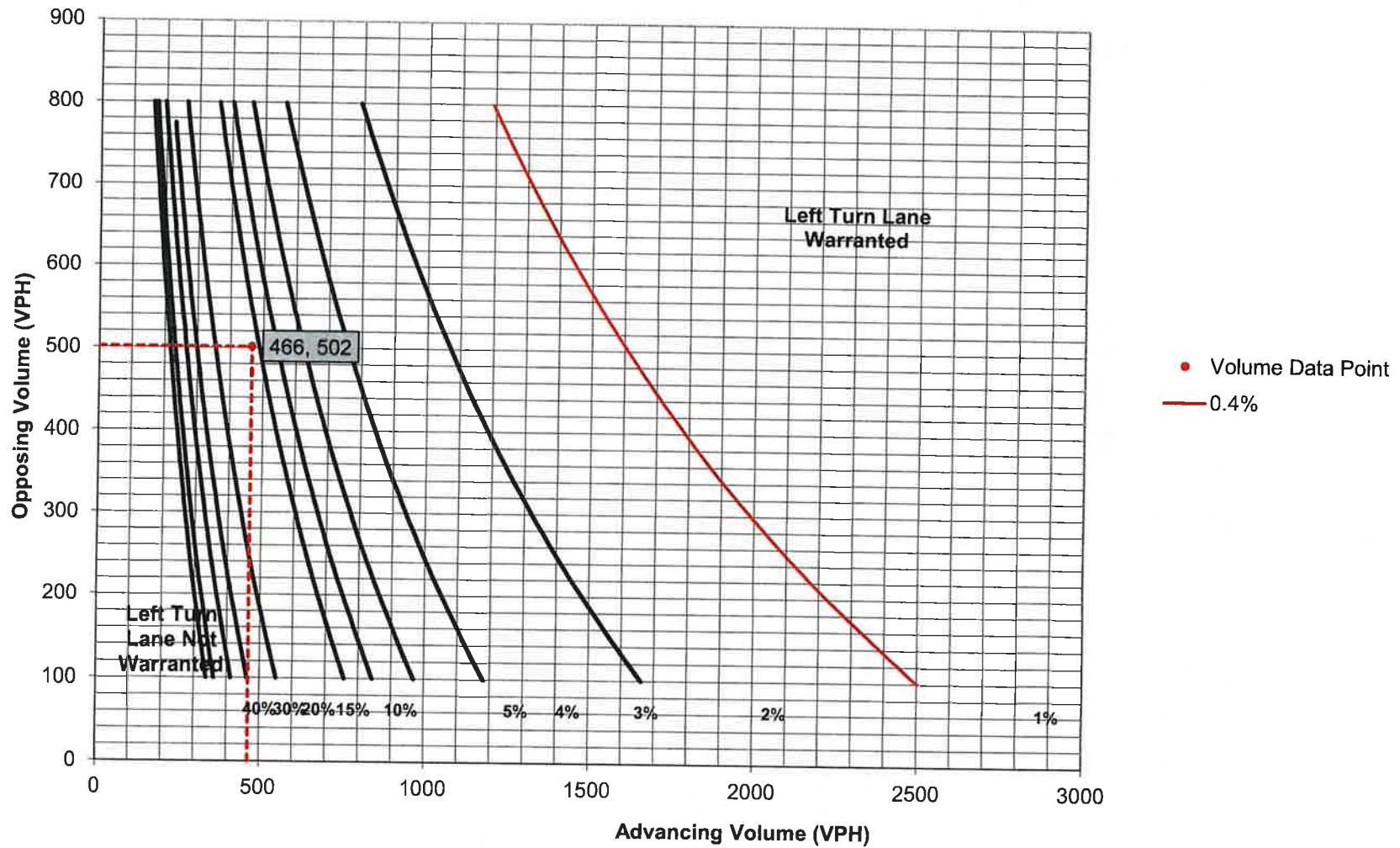
Left Turn Lane Storage Length, Condition A: **N/A** Feet  
Condition B: **N/A** Feet  
Condition C: **N/A** Feet  
Required Left Turn Lane Storage Length: **N/A** Feet

Additional Findings: **N/A**

Additional Comments / Justifications:



**Figure 1. Warrant for left turn lanes on two-lane roadways**  
 (speeds to 35 mph, unsignalized and signalized intersections)  
 (L = % Left Turns in Advancing Volume)



# Turn Lane Warrant and Length Analysis Workbook

## STUDY LOCATION AND ANALYSIS INFORMATION

Municipality:	Hatfield Borough	Analysis Date:	10/17/2024
County:	Montgomery County	Conducted By:	MF
PennDOT Engineering District:	6	Checked By:	PHS
		Agency/Company Name:	Traffic Planning and Design, Inc.
Intersection & Approach Description: N. Main Street & Proposed Site Driveway			
Analysis Period:	2026 Projected Conditions	Number of Approach Lanes:	1
Design Hour:	Weekday P.M. Peak Hour	Undivided or Divided Highway:	Undivided
Intersection Control:	Unsignalized	Type of Analysis Left Turn Lane	
Posted Speed Limit (MPH):	25		
Type of Terrain:	Level	Left or Right-Turn Lane Analysis?:	

## VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	1	2.0%	2
	Through	-	459	2.0%	454
	Right	No			N/A
Opposing	Left	No			N/A
	Through	-	496	1.0%	499
	Right	Yes	2	2.0%	3

Advancing Volume: 466  
Opposing Volume: 502  
Left Turn Volume: 2

% Left Turns in Advancing Volume: 0.43%

Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	No			N/A
	Through	-	496	1.0%	N/A
	Right	-	2	2.0%	N/A

Advancing Volume: N/A  
Right Turn Volume: N/A

## TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings		Right Turn Lane Warrant Findings	
Applicable Warrant Figure:	Figure 1	Applicable Warrant Figure:	N/A
Warrant Met?:	No	Warrant Met?:	N/A

## TURN LANE LENGTH CALCULATIONS

Intersection Control:	Unsignalized
Design Hour Volume of Turning Lane:	2
Cycles Per Hour (Assumed):	60
Cycles Per Hour (If Known):	60

Average # of Vehicles/Cycle: N/A

PennDOT Publication 46, Exhibit 11-6

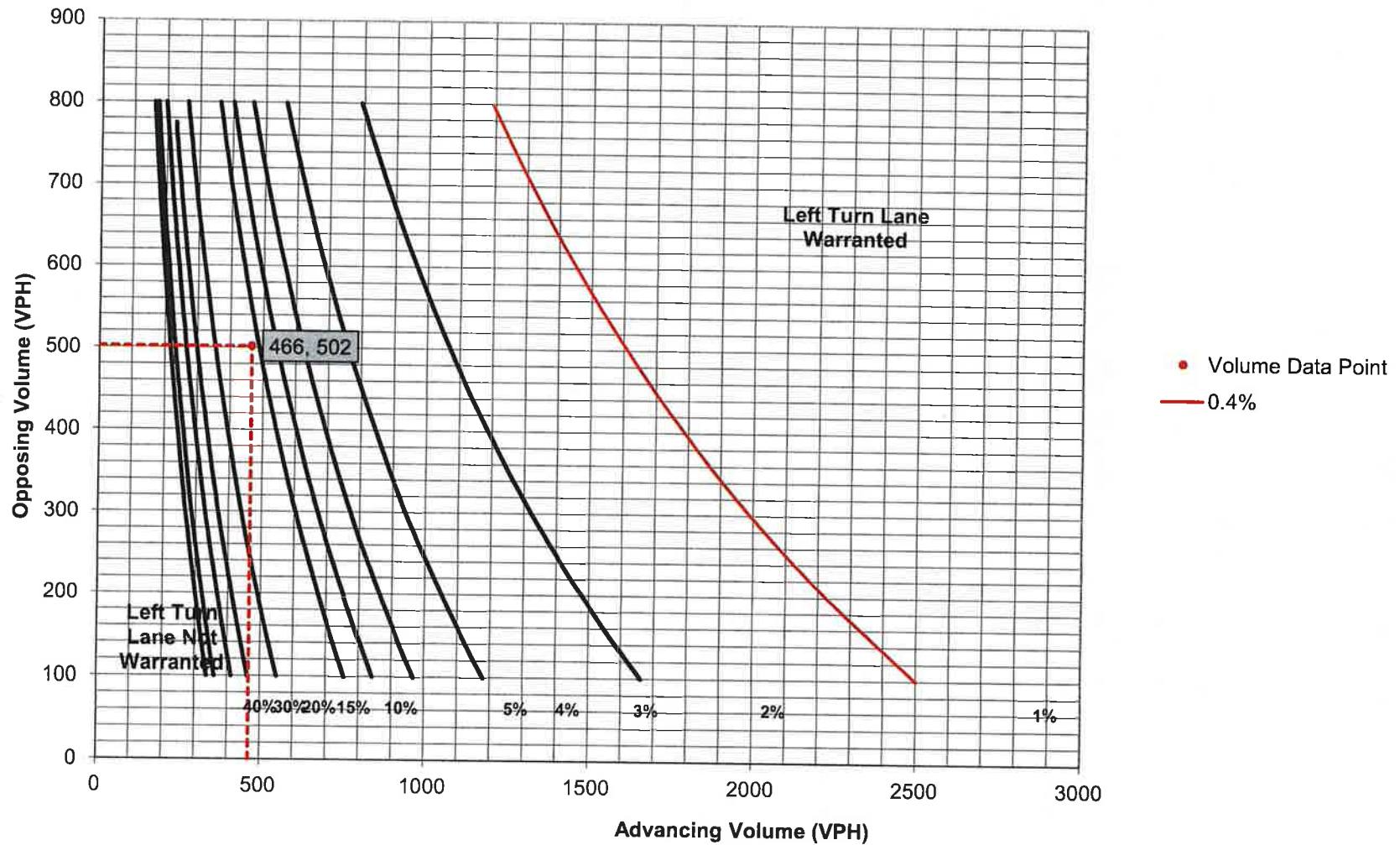
Type of Traffic Control	Speed (MPH)					
	25-35		40-45		50-60	
	High	Low	High	Low	High	Low
Signalized	A	A	B or C	B or C	B or C	B or C
Unsignalized	A	A	C	B	B or C	B

Left Turn Lane Storage Length, Condition A: N/A Feet  
Condition B: N/A Feet  
Condition C: N/A Feet  
Required Left Turn Lane Storage Length: N/A Feet

Additional Findings: N/A

Additional Comments / Justifications:

**Figure 1. Warrant for left turn lanes on two-lane roadways**  
 (speeds to 35 mph, unsignalized and signalized intersections)  
 (L = % Left Turns in Advancing Volume)





# Turn Lane Warrant and Length Analysis Workbook

## STUDY LOCATION AND ANALYSIS INFORMATION

Municipality:	Hatfield Borough	Analysis Date:	10/17/2024
County:	Montgomery County	Conducted By:	MF
PennDOT Engineering District:	6	Checked By:	PHS
		Agency/Company Name:	Traffic Planning and Design, Inc.
Intersection & Approach Description: N. Main Street & Proposed Site Driveway			
Analysis Period:	2026 Projected Conditions	Number of Approach Lanes:	1
Design Hour:	Weekday P.M. Peak Hour	Undivided or Divided Highway:	Undivided
Intersection Control:	Unsignalized		
Posted Speed Limit (MPH):	25	Type of Analysis	
Type of Terrain:	Level	Left or Right-Turn Lane Analysis?: Right Turn Lane	

## VOLUME CALCULATIONS

Left Turn Lane Volume Calculations						
Movement	Include?	Volume	% Trucks	PCEV		
Advancing	Left	Yes	1	2.0%	N/A	Advancing Volume: N/A
	Through	-	459	2.0%	N/A	
	Right	No			N/A	
Opposing	Left	No			N/A	Opposing Volume: N/A
	Through	-	496	1.0%	N/A	
	Right	Yes	2	2.0%	N/A	
% Left Turns in Advancing Volume: N/A						
Right Turn Lane Volume Calculations						
Movement	Include?	Volume	% Trucks	PCEV		
Advancing	Left	No			N/A	Advancing Volume: 502
	Through	-	496	1.0%	499	
	Right	-	2	2.0%	3	
Right Turn Volume: 3						

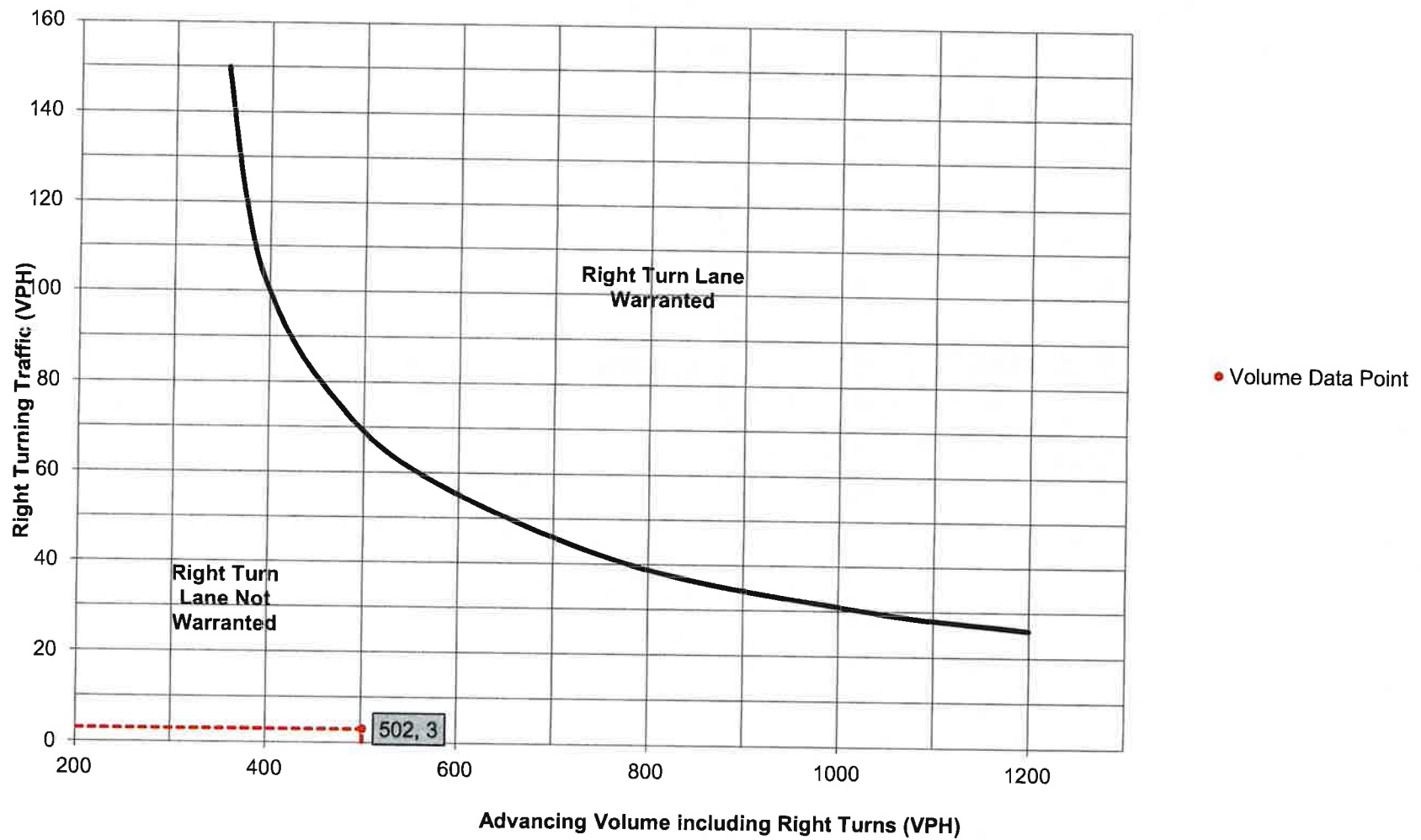
## TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings	Right Turn Lane Warrant Findings
Applicable Warrant Figure: N/A	Applicable Warrant Figure: Figure 9
Warrant Met?: N/A	Warrant Met?: No

## TURN LANE LENGTH CALCULATIONS

Intersection Control:	Unsignalized					
Design Hour Volume of Turning Lane:	3					
Cycles Per Hour (Assumed):	60					
Cycles Per Hour (If Known):	60					
Average # of Vehicles/Cycle:		N/A				
PennDOT Publication 46, Exhibit 11-6						
Type of Traffic Control	Speed (MPH)					
	25-35		40-45		50-60	
	Turn Demand Volume					
	High	Low	High	Low	High	Low
Signalized	A	A	B or C	B or C	B or C	B or C
Unsignalized	A	A	C	B	B or C	B
Right Turn Lane Storage Length, Condition A:		N/A	Feet			
Condition B:		N/A	Feet			
Condition C:		N/A	Feet			
Required Right Turn Lane Storage Length:		N/A	Feet			
Additional Findings:		N/A				
Additional Comments / Justifications:						

**Figure 9. Warrant for right turn lanes on two-lane roadways  
(40 mph or lower speeds, unsignalized and signalized intersections)**





# Turn Lane Warrant and Length Analysis Workbook

## STUDY LOCATION AND ANALYSIS INFORMATION

Municipality:	Hatfield Borough	Analysis Date:	10/17/2024
County:	Montgomery County	Conducted By:	MF
PennDOT Engineering District:	6	Checked By:	PHS
		Agency/Company Name:	Traffic Planning and Design, Inc.
Intersection & Approach Description: N. Main Street & Proposed Site Driveway			
Analysis Period:	2026 Projected Conditions	Number of Approach Lanes:	1
Design Hour:	Weekday P.M. Peak Hour	Undivided or Divided Highway:	Undivided
Intersection Control:	Unsignalized	Type of Analysis:	Right Turn Lane
Posted Speed Limit (MPH):	25	Left or Right-Turn Lane Analysis?:	Right Turn Lane
Type of Terrain:	Level		

## VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	1	2.0%	N/A
	Through	-	459	2.0%	N/A
	Right	No			N/A
Opposing	Left	No			N/A
	Through	-	496	1.0%	N/A
	Right	Yes	2	2.0%	N/A
Advancing Volume: N/A					
Opposing Volume: N/A					
Left Turn Volume: N/A					
% Left Turns in Advancing Volume: N/A					
Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	No			N/A
	Through	-	496	1.0%	499
	Right	-	2	2.0%	3
Advancing Volume: 502					
Right Turn Volume: 3					

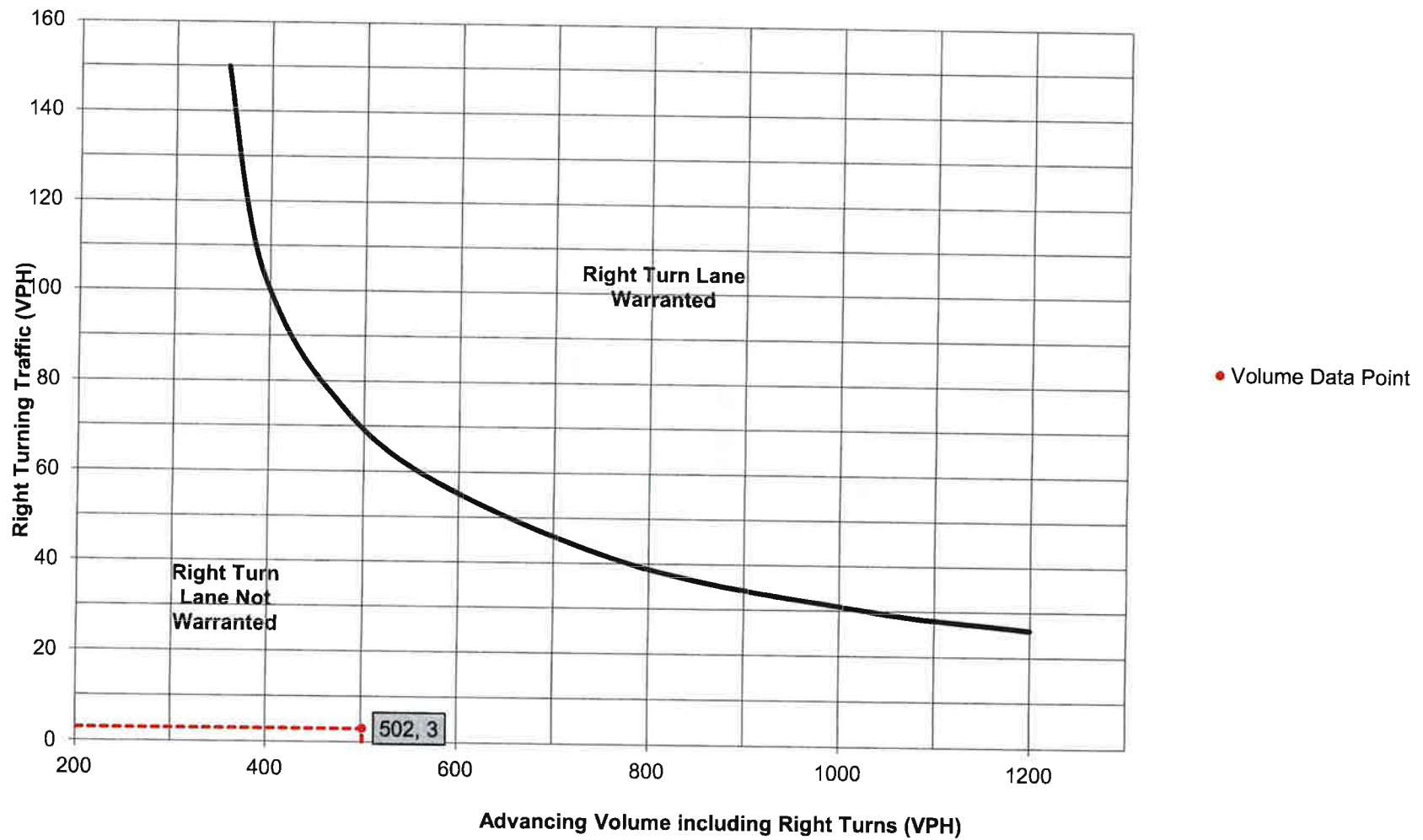
## TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings	Right Turn Lane Warrant Findings
Applicable Warrant Figure: N/A	Applicable Warrant Figure: Figure 9
Warrant Met?: N/A	Warrant Met?: No

## TURN LANE LENGTH CALCULATIONS

Intersection Control:	Unsignalized	Average # of Vehicles/Cycle:	N/A			
Design Hour Volume of Turning Lane:	3					
Cycles Per Hour (Assumed):	60					
Cycles Per Hour (If Known):	60					
PennDOT Publication 46, Exhibit 11-6						
Type of Traffic Control	Speed (MPH)					
	25-35		40-45		50-60	
	Turn Demand Volume					
	High	Low	High	Low	High	Low
Signalized	A	A	B or C	B or C	B or C	B or C
Unsignalized	A	A	C	B	B or C	B
Right Turn Lane Storage Length, Condition A: N/A Feet						
Condition B: N/A Feet						
Condition C: N/A Feet						
Required Right Turn Lane Storage Length: N/A Feet						
Additional Findings: N/A						
Additional Comments / Justifications:						

**Figure 9. Warrant for right turn lanes on two-lane roadways  
(40 mph or lower speeds, unsignalized and signalized intersections)**



**Response Letter to Review**  
**Letter from Bursich**  
**9.18.24**



Kristin Holmes, P.E., LEED AP  
Robert Cunningham, P.E., LEED AP

October 14, 2024

Jaime E. Snyder  
Borough Manager  
Hatfield Borough  
401 South Main Street  
P.O. Box 190  
Hatfield, PA 19440

**RE: 23 N. Main Street – Hatfield Walk  
Hatfield Borough, Montgomery County, PA  
HCE Project No.: 1727**

Dear Jaime:

We are in receipt of several review letters for the above-referenced project. Below please find responses to each of the comments contained in those letters.

Review Letter from Bursich Associates dated September 18, 2024.

Zoning Ordinance Comments

1. The following items must be revised to comply with the Zoning Decision:

A. The R-4 Zoning District standards shall be added to the record plan. The standards that are superseded by the Conditions of the Zoning Hearing Decision shall be noted. The proposed conditions must be related to the R-4 standards.

**Response: R-4 District Standards have been added to the table.**

B. The plans shall show 20-foot building setbacks rather than 10-foot and 2-foot setbacks, except along the northwestern line adjacent to the post office property.

**Response: The setbacks have been revised.**

C. The proposed sidewalk along the Renner Property shall be located adjacent to the proposed curbing for the access drive.

**Response: The sidewalk has been adjusted.**

D. The privacy fences along the driveway should extend to the faces of the buildings on the Renner and Hausmann properties unless the fences would conflict with the required sight triangle. The sight triangle shall be shown on the plan.

**Response: The fence has been revised and the sight triangle has been shown on the plan**

E. The existing Zoning District boundaries and labels for the affected and adjacent properties shall be added to the Record Plan.

**Response: Zoning District boundaries have been added and the property owners have been provided.**

F. Condition 1.c stipulates that Open Space shall be restricted from further development and shall be offered to the Borough for dedication. The Record Plan



shall label the proposed Open Space and provide metes and bounds of the boundary.

**Response: The open space area has been delineated and a note has been added to the record plan.**

2. §27-816.1.B.(3) – The Borough Council shall evaluate all applications relating to common driveways as to the location, placement, and alignment of such common driveways based upon the ease of accessibility to, and efficient maneuverability through, for protective services of fire and police.

**Response: Acknowledged.**

3. §27-2302.1 – The classification of the onsite stream shall be indicated on the plans. The source of the limit of the Riparian Corridor shown on the plans shall be clarified. Additionally, the Limit of Stream/Top of Bank along the southern side of the stream shall be clarified as it does not appear to match the topography between labels TOB-A6 and TOB-A4 on the Existing Features Plan.

**Response: The stream classification and riparian buffer have been noted on the plan.**

4. §27-2302.2 – Steep slopes shall be identified on the plans, and the Riparian Corridor boundary shall be updated if applicable.

**Response: The steep slopes have been added to the Existing Features Plan.**

#### Subdivision and Land Development Ordinance

1. §22-305 & §22-307 – The plans shall be revised to include or clarify the following information:

- A. The parcels subject to the application shall be labeled on the Record Plan, and the Lot Line to be Removed shall be more clearly labeled. Site Plan Note 3 on sheet 1 shall include both parcels.

**Response: On C1.0, a label has been added to each property containing the relevant Parcel ID numbers. The label regarding the Lot Line to be Removed has been moved into a more prominent position. Site Plan Notes No. 3 has been edited to include both Parcel ID numbers.**

- B. The street right-of-way line along the property frontage shall match the legend.

**Response: The plan and legend line type has been edited to match each other.**

- C. The Owner's Certification on the Record Plan must include all property owners party to the subdivision and land development.

**Response: On Sheet C1.0, the Owner's Certification has been updated to include the relevant property owners and parties.**

- D. The Existing Features (and Demolition) Plan shall label all features to be removed. The limit of tree clearing must be shown on the Existing Features and Grading and Drainage Plans.

**Response: On Sheet C1.1, additional labels reading "TO BE REMOVED" have been added to the existing features plan to better show the features and trees**



being removed. The proposed tree line/Limits of Tree Removal has been added to C1.1, C3.0 and C5.0.

- E. The bounds of the new site shall be labeled to the right-of-way line.

**Response: Boundary information to the right-of-way line has been added.**

- F. The Combined Lot Area in the Lot Area Calcs. Table on sheet 1 shall indicate "Net".

**Response: The table has been revised.**

- G. Dimensions shall be provided for the backup / turnaround area at the end of the parking row, the radii for all curves, sidewalk width, Community Area, distance between post office parking lot and underground basin / Community Area, driveway to property line.

**Response: Added dimension have been added to the plans.**

- H. The first-floor elevations shall be added to the plans. The ground outside the buildings must be at least 18-inches below finished floor, except at the garages.

**Response: First floor elevations have been added to the plans. The proposed project will be slab on grade construction and a minimum of 8-inch to outside grade has been provided as required by building code.**

- I. ADA ramps shall be designed at the end of the internal sidewalk and both sides of the driveway.

**Response: ADA ramps have been provided.**

- J. Sign symbols.

**Response: On Plan Sheet C1.0 and C2.0, the proposed signs symbol has been added to the legend and sign labels have been added to the plans to clarify the proposed signs.**

- K. Lights.

**Response: Lighting has been added.**

- L. The Location Map shall include the surrounding road names.

**Response: On Plan Sheet C1.0 and C1.1, a location map showing the names of surrounding roads has been included.**

- M. The soils line shall be shown differently for clarity and be included in the legend.

**Response: Across all plans, the Soils Boundary Line type has been changed, and the legend has been updated to clarify the Soils Boundary.**

- N. Existing features within 200 feet of the site are required to be included on the plans. Of particular importance are buildings, topography, vegetation, utilities, sidewalks, signs, etc. An aerial image may be appropriate.

**Response: A plan sheet C1.2 has been added with an Aerial Image to show existing features within 200 feet of the site.**

- O. The proposed building heights and number of stories shall be added to the plans.

**Response: On plan sheet C2.0, the building height and number of stories has been added.**

- P. The legend shall be more complete to clarify the lines and symbols on the plans, particularly on the Record Plan.

**Response: Across all plan sheets, legends have been updated to clarify lines and symbols.**

- Q. The proposed grades shall be shown on the plan view on sheet 14.

**Response: Grades have been added to the profile sheet.**

2. §22-410.E – The clear sight triangle shall be labeled on the plans, and all existing and proposed features within the sight triangle shall be labeled.

**Response: The clean sight triangle has been added to the plans.**

3. §22-413 – Sidewalks and Curbs

- A. The curbing within the N. Main St. right-of-way shall be concrete unless a waiver is granted.

**Response: Curbing within the right of way will be concrete.**

- B. A detail of the curb tapers shall be added to the plans.

**Response: On Plan Sheet C2.0, a Curb Taper Detail has been added.**

4. §22-414.B(2) – Parking areas shall not be located closer than 20 feet from any tract boundary line. These setback areas shall be landscaped in accordance with the requirements of §22-420, General Planting Requirements. Per §22-414.1.A.(3), "Parking" includes the driveway which provides direct access to the parking spaces.

**Response: A waiver from this section will be required since the proposed driveway is within 50 feet.**

5. §22-420.D.(2) – A 100 percent performance bond shall be posted to ensure replacement of landscape material that is removed, destroyed, damaged, or in ill-health within 15 months of installation. We also recommend an agreement be recorded perpetually requiring the Homeowner's Association to replace any landscaping that dies at any point in the future.

**Response: Acknowledged.**

6. §22-426 – The Applicant shall present evidence that water will be supplied by a certified public utility.

**Response: A water will serve letter will be provided.**

7. §22-427 – The Applicant shall present evidence that sewer service will be supplied by a certified public utility.

**Response: A sewer will serve letter will be provided.**

8. §22-428 – Compliance with Engineering & Construction Standards:

- A. §108.3.A – A letter of endorsement shall be required from the suppliers of utility services wherein the applicant acknowledges that underground utilities are feasible.

**Response: The letters of endorsement will be provided.**

- B. §108.3.D – Proposed lights shall be added to the plans along with footcandles showing safe lighting at the parking lots and along the sidewalks. The footcandles shall also illustrate that lighting will not spill across the tract lines. Details of the light fixtures and supporting bases shall be added to the plans.

**Response: The plan has been revised to depict the lights to be provided.**

- C. §110 – Fire hydrants shall be located at accessible points in the development and shall be located according to the Fire Marshal and Water Authority.

**Response: Fire hydrants have been coordinated with the Fire Marshal and Water Authority.**

- D. §112.1. – Concrete monuments shall be installed along the right-of-way lines where they meet adjoining properties. Property corner pins shall be installed. The pins and monuments shall be shown on the Record Plan. Existing monumentation shall be labeled as Found & Held where applicable.

**Response: Boundary monumentation has been added to the plan.**

9. §22-502.B – A cost estimate to establish financial security for the completion of the proposed improvements shall be provided.

**Response: Acknowledged. A cost estimate will be provided once all plan items have been addressed.**

#### Stormwater Comments

1. §26-132.2.B(3)(i) – The following signature block for the Design Engineer shall be added: “I, (Design Engineer), on this date (date of signature), hereby certify that the SWM Site Plan meets all design standards and criteria of The Neshaminy Creek Watershed Act 167 Stormwater Management Ordinance or Plan.”

**Response: On Plan Sheet C3.0, the Design Engineer Certification signature block has been edited to include the above note.**

2. §26-161 - For subdivisions and land developments, the applicant shall provide financial security acceptable to the Borough of Hatfield for the timely installation and proper construction of all stormwater management (SWM) facilities as specified in this section.

**Response: Acknowledged.**

3. §26-164 – A Stormwater Operation and Maintenance Agreement must be provided to the Borough Solicitor’s satisfaction.

**Response: Acknowledged.**

4. The Pre-Development Drainage Area Map shall illustrate the off-site area that is in the calculations.

**Response: On Figure 2, the EOS-1 “Existing Undisturbed” area is now shown.**

5. The Post-Development Drainage Area Map shall clarify how much runoff from Units 1-4 roofs are proposed to reach the storm basin. The roofdrains / downspouts shall be illustrated on the design plans.

**Response: On Plan Sheet C3.0, Roof Drains/downspouts and Roof Drain Collector Pipe has been added.**

6. The drainage area to the underground basin on the Post-Development Drainage Area Map does not appear to be accurate. The overland flow north of the driveways and access drive would not enter the basin based on the topography.

**Response: The grading has been revised to ensure the overland flow gets into the Underground Basin.**

7. We recommend a roofdrain pipe be installed to tie the downspouts from Units 5-8 into inlet box CB-5. This would keep runoff from the downspouts away from the building foundation.

**Response: On Plan Sheet C3.0, a Roof Drain Collector Pipe has been added, and it will connect directly into CB-5 to keep runoff away from the building foundation.**

8. The Tc paths must be shown on the Drainage Plans.

**Response: On Figures 2, 3 and 4, TC Paths are now shown.**

9. The Dekalb method of stormwater calculations shall use 3/3 limb factors to better estimate the anticipated volume of runoff.

**Response: The Stormwater Calculations have been revised to provided a 3/3 limb factor.**

10. The plans shall include the level spreader that is shown on the Detail Sheet. The detail shall be updated to reflect the proposed discharge pipe condition.

**Response: The application is no longer proposing a Level Spreader, and the detail has been removed.**

11. The storm sewer design calculations must consider the tailwater elevation in the storm basin.

**Response: The Storm Sewer Design Calculations have been revised to consider the tailwater elevation.**

12. Stormwater runoff from the neighboring properties to the south currently drains to, and across, the subject property. The plans proposed to raise the grade along the southern property line by over one foot in some locations. Additional topographic detail shall be provided along this property line to confirm the drainage from the neighboring properties will not be blocked. In particular, the Haque / Islam property contains a garage approximately two feet from the property line where the grade will be raised.

**Response: The grading has been revised to not trap runoff from many of the neighboring properties, however, in most cases the runoff will flow along the property line parallel to Board Street. The flow from the Haque/ Islam and Derstine properties will be conveyed around the property to the Walker property in a manner consistent with the existing conditions.**

#### Erosion and Sedimentation Control Comments

1. The proposed silt socks must be shown more clearly on sheet 10.

**Response: On Plan Sheet C5.0, the filter socks are now shown more clearly.**

2. Existing trees and Tree Protection Fencing must be added to the plan.



**Response: On Plan Sheet C5.0, the existing tree line, the proposed tree line and tree protection fencing has been added.**

3. Construction fencing shall be added along the limits of disturbance.

**Response: On Plan Sheet C5.0, a note has been added to the plans stating that construction fencing is to be added along the Limits of Disturbance and Sequence of Construction Note 3 has been edited to include Construction Fencing.**

4. If the plans are not being reviewed by the MCCD, then references to that agency can be removed from the notes on sheet 10.

**Response: On Plan Sheet C5.0, references to MCCD have been removed from the notes.**

5. The Sequence of Construction must indicate that no earth disturbance shall commence until Hatfield Borough inspects the E&S controls and authorizes earth disturbance activities to begin. The E&S controls shall not be removed until authorization is given by the Borough.

**Response: On Plan Sheet C5.0, the sequence of Construction Notes 4 and 13 has been edited to indicate that earth disturbance cannot commence, nor E&S controls can be removed, without authorization from Hatfield Borough.**

6. A topsoil stockpile location shall be added to the plans.

**Response: On plan sheet C5.0, a topsoil stockpile has been added.**

7. All lines and symbols representing E&S controls must match the Legend.

**Response: On plan sheet C5.0, lines and symbols have been updated to ensure plans and legends match.**

#### Sanitary Sewer Comments

1. The sanitary sewer design should be discussed with our office. In particular, the following will need to be coordinated:

- A. Illustrate the sanitary modifications being made in North Main Street
- B. Internal sanitary layout and depth of force main
- C. Locations of the grinder pumps and accessory panels and backup power supply
- D. Pump design / hydraulic capacity pump curve

**Response: The additional information has been provided. The pump design information is included with this submission.**

2. The following note shall be added to the Utility Plan:

"The sanitary sewer system in North Main Street is in the process of being replaced by Hatfield Borough during the design of these plans. The configuration of the sanitary lateral connection may be different than what is illustrated on these plans by the time the site is being developed."

**Response: On plan Sheet C4.0, a Utility Note 17 has been added.**



3. The plans include a label “See General Note 7” at the existing sanitary sewer manholes in North Main Street. General Note 7 is not applicable to sanitary sewer.  
**Response: The label has been removed.**

4. PADEP Sewage Facilities Planning shall be addressed.  
**Response: A copy of the planning module mailer is included.**

#### General Comments

1. The existing asphalt parking area for the Post Office encroaches approximately 12 feet onto the subject property. The Applicant shall indicate whether a parking easement exists on the property and illustrate the easement on the plans. If no easement exists, then one will need to be established, or the parking area will need to be removed.  
**Response: The existing parking easement/ lease area has been provided on the plans.**
2. The existing pull-in parking spaces for the Post Office are located approximately 8 feet from the proposed Community Area and underground storm basin. A barrier should be installed to stop vehicles from driving into this area.  
**Response: A post and rail fence has been added to the plans.**
3. The intention of the Community Area and any amenities shall be clarified.  
**Response: The community area is to be kept as lawn. A label has been added to the plans.**
4. The Belgian Block Curb detail indicates a curb reveal of 7-inches, and the spot grades indicate a 6-inch reveal. The curb reveal shall be clarified.  
**Response: The Belgian block curb detail has been revised.**
5. Some of the neighboring properties to the south contain two-story garages / potential living areas within one foot of the property line. The Applicant and Borough should consider the impact on these property owners to access the rear of their buildings when the privacy fence is installed along the property line.  
**Response: The fence has been adjusted closer to the proposed driveway to provide additional space.**
6. The proposed six-space pull-in parking is proposed to be located approximately 6 feet from the wall of Unit 1. We recommend a barrier, bumper blocks, and/or landscaping be provided to protect the building. Additionally, headlights and exhaust would likely be a nuisance to the occupants if windows are built on that wall.  
**Response: Landscape buffering has been added to the plans.**
7. The plans shall clarify if the site will contain community or individual mailboxes.  
**Response: A community mailbox pad has been added to the plan.**
8. Site Plan Note 20 on sheet 1 shall include sheets 1, 3, 6, and 7 to be recorded. These sheets shall also be noted to be recorded on the Drawing List.  
**Response: The note has been revised.**

9. Site Plan Note 9 on sheet 1 shall clarify that each unit will be responsible for trash pickup at their driveways rather than a community dumpster.

**Response: The note has been revised.**

10. We recommend a backup / turn-around area be provided in the access driveway for Unit 4 to back out of their driveway.

**Response: Additional backup area has been provided.**

11. Turning templates shall be provided for internal site movements.

**Response: A copy of the fire truck turning template has been provided in a separate plan included with this submission.**

12. Detail Sheet:

- A. The intent of the Street Sign shall be clarified since no sign is proposed on the plans.

**Response: The street sign has been removed.**

- B. Details shall be provided for concrete curb, ADA ramps at the intersection and lights.

**Response: The details have been added.**

13. Detailed design of the ADA ramps shall be provided prior to plan recording.

**Response: The additional ramp information has been provided on sheet #6.**

14. The proposed crosswalk and stop bar on the Detail Sheet shall be illustrated on the plans.

**Response: The crosswalk and stop bar have been added to the plans.**

15. Grading Note 6 on sheet 5 shall be revised to resolve the conflict in the horizontal to vertical slopes.

**Response: The note has been revised.**

16. The proposed Japanese Zelkova tree at the intersection of the driveway and N. Main St. shall be removed to avoid conflicts with sight distance, overhead utilities, neighboring driveway, and sidewalk. The three proposed Japanese Zelkova trees along the Renner property shall be replaced with trees that will not impact the Renner's property and the proposed sidewalk.

**Response: The landscaping has been revised.**

17. We recommend the privacy fence be extended along the property line between Unit 5 and the Post Office parking lot, at a minimum, for safety, security, and privacy.

**Response: The privacy fence has been extended.**

18. Homeowner's Association documents shall be provided to the satisfaction of the Borough Solicitor.

**Response: Acknowledged.**

19. Legal descriptions shall be provided for the overall tract, any defined easements, and areas to be offered for dedication to Hatfield Borough.

**Response: The legal descriptions will be provided once all engineering items have been satisfied.**

20. Reviews, approvals, permits required include, but are not limited to, the following:

- A. PaDEP Sewage Facilities Planning
- B. Montgomery County Planning Commission
- C. Borough Traffic Engineer
- D. Borough Fire Marshal
- E. Borough Electric Consultant
- F. Emergency Service providers
- G. NPWA – for service adequacy and design approval
- H. HTMA – for sewage treatment capacity

**Response: Acknowledged.**

21. Additional comments may be generated from subsequent submissions as a result of the plan and design revisions and additional information to be provided.

**Response: Acknowledged.**

Traffic Review Letter from Bowman dated September 20, 2024.

#### Site Access Study

1. The site access study should be revised to include a traffic analysis of the intersection of intersection of Main Street and Broad Street. The intersection currently experiences delay during the commuter peak hours and the queuing along Main Street may impact the operation of the site driveway during the commuter peak hours. A gap study along North Main Street at the proposed site driveway location should be conducted if necessary to confirm that there are an adequate number of gaps in the North Main Street traffic stream for vehicles to safely enter and exit the site.

**Response: As requested, the Main Street and Broad Street intersection has been included in the traffic analysis. Additionally, a gap study has been completed at the site driveway and is included in the revised traffic analysis.**

2. The site access study should be updated to include capacity/levels-of-service analysis for the intersection of North Main Street and the site driveway for the weekday morning and weekday afternoon peak hours under 2029 future with-development conditions.

**Response: As requested, capacity analysis has been included in the revised traffic analysis.**

3. The study should be revised so that the entering and exiting site trips for the weekday morning peak hour shown in Table 6 and on Figure 6 match the distribution percentages shown in Table 5. In addition, the turn lane warrant analysis shown in Appendix C should be revised accordingly.

**Response: As requested, the traffic analysis has been revised to address the above comment.**

#### Preliminary/Final Land Development Plans

1. The pavement markings along Main Street at the site access should be reviewed. Modifications to the pavement markings may be required to properly manage the

movements to \from the site, the left turn lane at the signalized intersection, and the existing pedestrian crossing and parking at the post office. It should be noted that the Borough has identified traffic calming\pedestrian improvements along North Main Street at the existing pedestrian crossing for the post office.

**Response: Acknowledged.**

2. Sight distance measurements must be shown on the plans for the intersection of North Main Street and the site driveway as required by Section 22-405.1 of the Subdivision and Land Development Ordinance.

**Response: Sight distances have been added to the plans.**

3. Turning templates should be provided with future plan submissions demonstrating the ability of a trash truck, emergency vehicle, and the largest expected delivery truck to maneuver into and out of the driveway along North Main Street and entirely through the site. The Borough Fire Marshal should review the emergency vehicle turning template for accessibility and circulation needs of emergency apparatus.

**Response: The fire truck turning template has been provided.**

4. A "Stop" sign and stop bar should be shown on the plans on the site driveway approach to North Main Street. "No Parking" signs should be shown on the plans along the eastern side of the site driveway from North Main Street to the northern end of the site driveway.

**Response: The additional signage has been provided.**

5. ADA ramps must be provided at the driveway along Main Street for the existing sidewalk. An ADA ramp should also be shown on the plans on the northern end of the sidewalk located on the western side of the site driveway at its intersection with the drive aisle leading to/from the townhomes.

**Response: Ramp information has been added to the plans.**

6. A back-up area should be provided on the western end of the drive aisle leading to/from the townhomes so that vehicles backing out of the driveways for lots 4 and 5 have adequate space to complete this maneuver.

**Response: Additional backup area has been provided**

*Fire Review Letter from Code Inspections, Inc. dated September 10, 2024.*

1. Due to the length of the proposed dead end fire lane a fire apparatus access road turnaround must be provided.
  - a. For approval a fire apparatus turning model shall be provided using the attached specifications for the Hatfield Fire Company Ladder Truck. The turning radius of the street and the apparatus turnaround shall be designed to accommodate the requirements for this apparatus.
  - b. The purpose of this model is to confirm that the fire apparatus will be able to enter and exit the property including using the provided fire apparatus access road without leaving the paved surface with minimal backing of apparatus.

**Response: The turning template has been provided on a supplemental plan sheet.**

If you have any questions or require additional information, please do not hesitate to contact us at 215-586-3330 or [rob@hcengineering.net](mailto:rob@hcengineering.net)

Very truly yours,  
**Holmes Cunningham Engineering**

A handwritten signature in black ink, appearing to read "Rob Cunningham", with a stylized flourish at the end.

Rob Cunningham, P.E., LEED AP  
Partner

O:\1727 - Arbor Grove Hatfield\Outbound\Twp Response Letter 2024-10-14.docx



# Engineer Review Letter

*[Faint signature]*

*[Faint text]*

November 11, 2024

Jaime E. Snyder  
Borough Manager  
Hatfield Borough  
401 South Main Street  
P.O. Box 190  
Hatfield PA 19440



RE: **Hatfield Walk Townhomes (23 N. Main St.)**  
Land Development Review Letter 2  
Bursich Project No: HAT-01 / 228290

Dear Jaime:

As requested, Van Cleef Engineering has reviewed the revised Preliminary / Final Land Development Plan submission for the Hatfield Walk Townhouse project. The submission consisted of the following information prepared by Holmes Cunningham Engineering:

- Plans titled Hatfield Walk, consisting of sheets 1 through 15 of 15, dated August 7, 2024 with latest revision date of October 11, 2024
- Plan titled Hatfield Walk – Fire Truck Turning Template, sheet 1 of 1, dated October 14, 2024 with no revision date
- Post Construction Stormwater Management Plan Narrative, dated August 7, 2024 with latest revision date of October 14, 2024
- Letter dated October 14, 2024 in response to Borough consultant review letters
- Letter dated Oct 10, 2024 from Site Specific Design, Inc. with Pressure Sewer Design Analysis

The site consists of two parcels: one contains an existing dwelling, fronts N. Main Street, and is located entirely in the CC – Core Commercial Zoning District; while the other is unimproved, is landlocked behind the first property and the Post Office property, and is split between the CC District and R-1 Residential District. The plan proposes eight townhouse units in two buildings, each with four units, separated by a paved access aisle. Each unit is proposed to include a two-car garage and driveway. Six parallel parking spaces are proposed along the access aisle, and a separate six-space lot is also proposed, for a total of twelve shared parking spaces. The existing dwelling on the N. Main Street parcel is to be demolished to construct the driveway, which will gain access from N. Main Street. The applicant intends to remove the common property line and join the properties into a common deed.

We offer the following for your consideration:

F:\Projects\HAT-01\228290\_Hatfield Walk (23 N. Main St.)\Land Development\Reviews\2024-11-11\_Hatfield Walk Townhomes-LD Rvw 2.docx

**OFFICE LOCATIONS**

[www.vancleefengineering.com](http://www.vancleefengineering.com)

Hillsborough, NJ  
908-359-8291  
  
Hamilton, NJ  
609-689-1100

Mt. Arlington, NJ  
862-284-1100  
  
Toms River, NJ  
732-573-0490

Phillipsburg, NJ  
908-454-3080  
  
Freehold, NJ  
732-303-8700

Doylestown, PA  
215-345-1876  
  
Bethlehem, PA  
610-332-1772

Pottstown, PA  
610-323-4040

### VARIANCES GRANTED

At a Hearing on April 24, 2024, the Hatfield Borough Zoning Hearing Board granted the following variances from the Borough's Zoning Ordinance, subject to seventeen conditions:

1. A variance from Section §27-1202 to allow townhouses in the R-1 Residential Zoning District.
2. A variance from Section §27-1204 to permit alternate dimensional standards in the R-1 Residential Zoning District.
3. A variance from Section §27-2101 to allow townhouses in the CC Core Commercial Zoning District.
4. A variance from Section §27-2108.1.G to permit alternate rear yard dimensional standards in the CC Core Commercial Zoning District.
5. A variance from Section §27-2108.1.H to permit alternate front yard dimensional standards in the CC Core Commercial Zoning District.

### WAIVERS REQUESTED

The following waivers have been requested. The Requested Waivers shall be listed on the Record Plan and in a letter to the Borough.

1. §22-414.B(2) – Parking areas shall not be located closer than 20 feet from any tract boundary line. These setback areas shall be landscaped in accordance with the requirements of §22-420, General Planting Requirements. Per §22-414.1.A.(3), "Parking" includes the driveway which provides direct access to the parking spaces. The driveway parking / driveway is proposed to be 5.5 feet from the eastern property line, 14 feet from the northern line, and 13 feet from the western line.
2. §22-420.1.C.(2) - A waiver to allow a six-foot high privacy fence along the Renner property rather than the required five shade trees, and a six-foot high privacy fence and shrubs along the southeastern property boundary rather than the required seven shade trees. There is not sufficient space for shade trees along these property lines.

### ZONING ORDINANCE COMMENTS

1. The following items must be revised to comply with the Zoning Decision:
  - A. We recommend the privacy fence along the driveway should extend to the face of the dwelling on the Renner property.

- B. Condition 1.c stipulates that Open Space shall be restricted from further development and shall be offered to the Borough for dedication.

The Record Plan includes a 0.467-acre area labeled "Open Space". The metes and bounds of the boundary shall be shown in larger vertical text for clarity and to indicate it is proposed rather than existing. A fee-simple dedication of this area would create a subdivision with a new lot (property), which would impact the proposed area and dimensional information as they apply to meeting Zoning requirements. The Borough should also consider its intent with this Open Space area. If the intent is to create access from N. Main Street to Centennial Park, then additional planning and easement agreements will be necessary for public access through the private townhouse property to the Borough-owned park property. The Borough should also consider if they wish the walkway to be ADA-compliant.

2. §27-816.1.B.(3) – The Borough Council shall evaluate all applications relating to common driveways as to the location, placement, and alignment of such common driveways based upon the ease of accessibility to, and efficient maneuverability through, for protective services of fire and police.
3. The following revisions shall be made to the Zoning Data Table on Sheet 1:
- A. The Required / Permitted Max. Building Coverage is 35%.
  - B. The Proposed Front Yard and Rear Yard setbacks appear to have been switched.

#### SUBDIVISION AND LAND DEVELOPMENT ORDINANCE

1. §22-305 & §22-307 – The plans shall be revised to include or clarify the following information:
- A. The Owner's Certification on the Record Plan indicates Pennington Property Group, LLC. is the owner of the properties, while the submitted deed indicates Kaler/Moyer is the owner. The legal owners of both properties must be represented on the plans.
  - B. The proposed bounds of the eastern property line must be for the combined property.
  - C. The northern adjoiner property line between the Hatfield Borough and Walker properties shall be made more clear.
  - D. Dimensions shall be provided for the backup / turnaround area between units 4 and 5, sidewalk width, distance between the buildings and sidewalks/curbs, driveway and fences to all property lines, fence lengths along the eastern property line including the gap for the fire hydrant.
  - E. Proposed spot elevations shall be provided at all corners of the buildings and along the sides of Units 5 and 8.
  - F. The limits of the curbing within the site shall be labeled.
  - G. A note shall be added to sheet 6 stating that an As-built Plan of the ADA ramps shall be submitted to Hatfield Borough after construction to confirm ADA compliance.
  - H. Lighting shall be provided for all parking spaces and walkways.



2. §22-420.D.(2) – A 100 percent performance bond shall be posted to ensure replacement of landscape material that is removed, destroyed, damaged, or in ill-health within 15 months of installation. We also recommend an agreement be recorded perpetually requiring the Homeowner's Association to replace any landscaping that dies at any point in the future.
3. §22-426 – The Applicant shall present evidence that water will be supplied by a certified public utility.
4. §22-427 – The Applicant shall present evidence that sewer service will be supplied by a certified public utility.
5. §22-428 – Compliance with Engineering & Construction Standards:
  - A. §108.3.A – A letter of endorsement shall be required from the suppliers of utility services wherein the applicant acknowledges that underground utilities are feasible.
  - B. §108.3.D – A detail of the light fixture bases shall be added to the plans.
  - C. §110 – The Fire Marshal should review the proximity of the proposed fences to the fire hydrant.
  - D. §112.1. –Existing monumentation shall be labeled as Found & Held where applicable.
6. §22-502.B – A cost estimate to establish financial security for the completion of the proposed improvements shall be provided.

#### STORMWATER COMMENTS

1. §26-161 - For subdivisions and land developments, the applicant shall provide financial security acceptable to the Borough of Hatfield for the timely installation and proper construction of all stormwater management (SWM) facilities as specified in this section.
2. §26-164 – A Stormwater Operation and Maintenance Agreement must be provided to the Borough Solicitor's satisfaction.
3. The grading along the eastern corner of the property may block stormwater from adjoining properties. Additional topographic detail shall be provided. Stormwater drainage facilities may be necessary to provide positive drainage away from the property line and existing buildings.
4. The elevation of the weir on the detail on sheet 7 shall be revised to 323.30 to match the design calculations. The references to a level spreader shall be removed from the details.
5. The storm inlet labels shall be added to the plan view on sheet 15. The sanitary force main crossing shall be removed from the CB-1 to CB-2 Profile, as the crossing will be eliminated by shifting the force main.



6. The proposed grading behind and along the sides of units 5 to 8 appears to be too flat. The grate elevation of Inlet CB-5 also appears to be higher than the ground around it.
7. The flow summary tables on page 6 of the stormwater report do not appear to be accurate. While the design calculations appear to be satisfactory, the summary tables shall be updated.

#### EROSION AND SEDIMENTATION CONTROL COMMENTS

1. Tree protection fencing shall be shown around the trees next to and behind the Renner property.
2. A minimum rock size for the riprap apron should be R-4.
3. The proposed post and rail fence along the post office parking lease area appears as compost filter sock on sheet 11.

#### SANITARY SEWER COMMENTS

1. The proposed force main shall be shifted to the south to avoid the crossing with the storm pipe leaving inlet CB-1. The force main profile shall be revised to eliminate the dip. The water line should be shifted accordingly to maintain a 10-foot spacing from the force main.
2. The accessory equipment and backup power for the grinder pumps is proposed to be installed within dwelling units 1 and 8. The community sanitary equipment must be installed in an accessible location.
3. Utility Note 8 on sheet 8 must be revised to eliminate "Municipal Authority" after Hatfield Borough.
4. PaDEP Sewage Facilities Planning shall be addressed.

#### GENERAL COMMENTS

1. The plans illustrate a Parking Lease Area on the site for use by the Post Office. The metes and bounds of the lease area shall be added to the Record Plan, a copy of the lease agreement shall be provided, and a note shall be added to the plan referencing the agreement.
2. A barrier should be installed to stop vehicles from driving into the Community Area / Underground Basin area.
3. The plans now show the fence to be installed approximately four feet from the eastern property line. A dimension shall be added to the plans.

4. Site Plan Note 20 on sheet 1 shall be revised to replace sheet 3 with sheet 4 to be recorded.
5. We recommend a larger backup / turn-around area in the access driveway for vehicles in Unit 4 to back out of their driveway. A dimension shall be added to the plans.
6. The proposed grading at the eastern corner of the property between the curb and Walker property does not appear to be shown correctly based on the top of curb elevations.
7. The Fire Marshal should review the Fire Truck Turning Template plan for maneuverability.
8. The Applicant and Borough should consider if a "street" name sign should be installed for the driveway.
9. The details shall be revised to specify 4,000 psi for all curbs, sidewalks, and ramps.
10. The proposed crosswalk on the plans and Detail Sheet shall match the Borough's standard crosswalk pattern, which can be seen at the intersection of Broad St. and Main St.
11. Homeowner's Association documents shall be provided to the satisfaction of the Borough Solicitor.
12. Legal descriptions shall be provided for the overall tract, any defined easements, and areas to be offered for dedication to Hatfield Borough.
13. Reviews, approvals, permits required include, but are not limited to, the following:
  - A. PaDEP Sewage Facilities Planning
  - B. Montgomery County Planning Commission
  - C. Borough Traffic Engineer
  - D. Borough Fire Marshal
  - E. Borough Electric Consultant
  - F. Emergency Service providers
  - G. NPWA – for service adequacy and design approval
  - H. HTMA – for sewage treatment capacity
14. Additional comments may be generated from subsequent submissions as a result of the plan and design revisions and additional information to be provided.

The comments are made with the understanding that all existing features and topography are accurately represented on the plans, and that all designs, calculations and surveys are accurate and have been prepared in accordance with current laws, regulations, and currently accepted Professional Land Surveying and Engineering practices.

Should you have any questions or need further information, please feel free to contact me at 484-941-0418 or [ccamburn@vancleefengineering.com](mailto:ccamburn@vancleefengineering.com).

Very Truly Yours,  
Van Cleef Engineering Associates, LLC



Chad E. Camburn, P.E.  
Senior Technical Manager

Pc: Katie Vlahos, Assistant to the Borough Manager (*via email*)  
Kate Harper, Borough Solicitor (*via email*)  
Bob Heil, Hatfield Borough Zoning Officer (*via email*)  
Ben Goldthorp, Pennington Property Group, LLC., Applicant (*via email*;  
[ben@penningtonpropertygroup.com](mailto:ben@penningtonpropertygroup.com))  
Rob Cunningham, P.E., Holmes Cunningham LLC, Applicant's Engineer (*via email*;  
[rob@hcengineering.net](mailto:rob@hcengineering.net))

# Traffic Engineer Review Letter

1. The Traffic Engineer has reviewed the proposed project and has determined that the project meets the requirements of the Traffic Engineering Review Letter.

2. The Traffic Engineer has determined that the project meets the requirements of the Traffic Engineering Review Letter.

3. The Traffic Engineer has determined that the project meets the requirements of the Traffic Engineering Review Letter.

4. The Traffic Engineer has determined that the project meets the requirements of the Traffic Engineering Review Letter.

5. The Traffic Engineer has determined that the project meets the requirements of the Traffic Engineering Review Letter.

6. The Traffic Engineer has determined that the project meets the requirements of the Traffic Engineering Review Letter.

7. The Traffic Engineer has determined that the project meets the requirements of the Traffic Engineering Review Letter.

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10. The Traffic Engineer has determined that the project meets the requirements of the Traffic Engineering Review Letter.

11. The Traffic Engineer has determined that the project meets the requirements of the Traffic Engineering Review Letter.

12. The Traffic Engineer has determined that the project meets the requirements of the Traffic Engineering Review Letter.

13. The Traffic Engineer has determined that the project meets the requirements of the Traffic Engineering Review Letter.

November 7, 2024

Ms. Jaime E. Snyder  
Borough of Hatfield  
401 South Main Street  
P.O. Box 190  
Hatfield, PA 19440



**RE: Traffic Engineering Review #4**  
Proposed Residential Development – Hatfield Walk  
23 North Main Street  
Hatfield, PA 19440  
Project No. 311304-01-001

Dear Jaime:

Per your request, Bowman Consulting Group (Bowman) has completed a traffic engineering review of the proposed residential development to be located at 23 North Main Street in the Borough of Hatfield, Montgomery County, PA. It is our understanding that the proposed development will consist of the development of eight (8) townhomes. Access to the proposed development will be provided via a full-movement driveway along North Main Street.

The following documents were reviewed and/or referenced in preparation of our comments:

- Transportation Impact Assessment – Proposed Hatfield Homes Residential, prepared by TPD, Inc., dated October 18, 2024.
- Preliminary/Final Land Development Plans – Hatfield Walk, prepared by Holmes Cunningham Engineering, last revised October 11, 2024.

Bowman continues to offer the following comments pertaining to the land development plans for consideration by the Borough and action by the applicant.

1. Bowman finds that all outstanding traffic-related technical comments associated with the transportation impact assessment (TIA) have been satisfactorily addressed and we have no additional comments pertaining to the TIA at this time. It should be noted that based on information provided in Table 10 of the study, the queues along North Main Street, from its intersection with Broad Street, will extend past the site access during both peak hours. Driveway and traffic signal operations should be monitored after the development is open and operating at full occupancy.
2. It should be evaluated to revise the pavement markings along North Main Street at the site access to provide a painted\gored taper for the existing southbound left-turn lane at Broad Street. Also, a painted median\center turn lane area should be provided along North Main Street encompassing the site driveway and the church driveway. The median\center turn lane should taper to the existing conditions at the pedestrian crossing for the post office.



3. Turning templates should be provided with future plan submissions demonstrating the ability of a trash truck, emergency vehicle, and the largest expected delivery truck to maneuver into and out of the driveway along North Main Street and entirely through the site. The Borough Fire Marshal should review the emergency vehicle turning template for accessibility and circulation needs of emergency apparatus.
4. A back-up area should be provided on the western end of the drive aisle leading to/from the townhomes so that vehicles backing out of the driveways for lots 4 and 5 have adequate space to complete this maneuver.
5. The white stripe pavement marking shown on the plans on the center of the driveway at its intersection with North Main Street should be replaced with a double yellow line pavement marking.
6. The plans should include details for the proposed ADA ramps on both sides of the site access along North Main Street.
7. Review of the on-site ADA ramps has not been completed by our office, but these ramps must be designed by the applicant's engineers to comply with Federal/PennDOT design standards for ADA facilities.
8. A response letter must be provided with the resubmission detailing how each comment below has been addressed, and where each can be found in the resubmission materials (i.e., page number(s)) to assist in the re-review process. Additional comments may follow upon review of any resubmitted and more detailed plans during the land development process.

We trust that this review letter responds to your request, and satisfactorily addresses the traffic issues related to the proposed development at this time. If the Borough has any questions, or requires further clarification, please contact me.

Sincerely,



Anton Kuhner, P.E.  
Regional Service Lead - Signals

AKK/BMJ

cc: Chad Camburn, P.E., Bursich Associates, Inc  
Catherine M. Harper, Borough Solicitor  
Bob Heil, Borough of Hatfield  
Rob Cunningham, P.E., Holmes Cunningham Engineering (Applicant's Engineer)  
Matt Hammond, P.E., TPD, Inc. (Applicant's Traffic Engineer)

# Fire Marshal Review Letter

# Code Inspections, Inc.

603 Horsham Road  
Horsham, PA 19044

*A Full Service Agency Providing  
Professional Inspection Services*

Phone: 215-672-9400  
Fax: 215-672-9736

November 11, 2024

Re: Preliminary and Final Land Development Review for Hatfield Walk proposed at 23 North Main Street

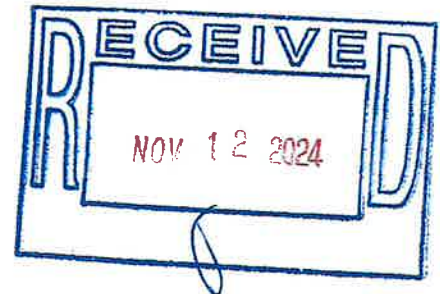
To Whom It May Concern:

The review of the plan referenced above for compliance with the 2018 International Building Code and the 2018 International Fire Code as adopted by the Pennsylvania Uniform Construction Code as well as the 2012 International Fire Code as amended and adopted by the Borough of Hatfield. The review has been completed and items in the previous submittal review letter have been addressed and approved.

Yours in safety,

  
Daniel Azeff

Fire Marshal  
Borough of Hatfield



# **Montgomery County Planning Commission Review Letter**

**MONTGOMERY COUNTY  
BOARD OF COMMISSIONERS**

JAMILA H. WINDER, CHAIR  
NEIL K. MAKHIJA, VICE CHAIR  
THOMAS DIBELLO, COMMISSIONER

WWW.MONTGOMERYCOUNTYPA.GOV



**MONTGOMERY COUNTY  
PLANNING COMMISSION**

MONTGOMERY COUNTY • PO Box 311  
NORRISTOWN, PA 19404-0311

610-278-3722  
PLANNING@MONTGOMERYCOUNTYPA.GOV

SCOTT FRANCE, AICP  
EXECUTIVE DIRECTOR

October 2, 2024

Ms. Jaime Snyder, Borough Manager  
Hatfield Borough  
401 S. Main Street  
Hatfield, PA 19440

Re: MCPC #24-0003-002  
Plan Name: Hatfield Walk  
(8 du/1 lot on approximately 1.5 acres)  
Situate: Main Street (N); northwest of Broad Street  
Hatfield Borough

Dear Ms. Snyder:

We have reviewed the above-referenced subdivision and land development plan in accordance with Section 502 of Act 247, "The Pennsylvania Municipalities Planning Code," as you requested on August 21, 2024. We forward this letter as a report of our review.

## BACKGROUND

The applicant, Pennington Property Group, has submitted a preliminary land development plan dated August 7, 2024 showing the construction of 8 new townhomes. In addition to garages and driveways for each townhome unit, 12 surface parking spaces are shown. The planned development also involves the construction of stormwater management facilities, a community area, and sidewalks. The proposal shows the consolidation of two properties and the removal of an existing home on the parcel fronting North Main Street. This property is located in the borough's Core Commercial zoning district. Public water and sewer serve the site.

The Montgomery County Planning Commission (MCPC) previously reviewed both a sketch plan on August 7, 2023 (MCPC # 23-0133-001), and a conceptual plan with a zoning text amendment on February 7, 2024 (MCPC # 24-0003-001) for the subject tract.

According to Sheet 1 of the Record Plan provided to MCPC on August 21, 2024, the applicant has received the following variances from the Hatfield Borough's zoning ordinance.

- From §27-1202 *Permitted Uses*
- From §27-1204 *Dimensional Standards*
- From §27-2101 *Statement of Intent*
- From §27-2108.1.G *Minimum Rear Setback*
- From §27-2108.1.H *Front Yard Depth*





## CONSISTENCY WITH COMPREHENSIVE PLANS

The proposed plan is generally consistent with the Montgomery County Comprehensive Plan, *Montco 2040: A Shared Vision*, and, in particular, its objectives to support growth and development in appropriate areas with existing infrastructure. The applicant's site lies within a "designated growth area" per the Growth and Preservation component of the county comprehensive plan. Furthermore, our Future Land Use Map categorizes the subject tract as a "Town Residential Area". The Plan advises that these areas are oriented towards pedestrians more than automobiles. A primary use in these areas are townhouses, though Town Residential Areas can have a variety of housing types mingled within blocks or small neighborhoods.

## RECOMMENDATION

The Montgomery County Planning Commission (MCPC) generally supports the applicant's proposal, however, in the course of our review we have identified the following issues that the applicant and borough may wish to consider prior to final plan approval. Our review comments are as follows:

## REVIEW COMMENTS

### CIRCULATION

- A. Pedestrian Facilities. The Walk Score® (<https://www.walkscore.com/score/23-n-main-st-hatfield-pa-19440>) of the development site is rated 50 (out of 100). For the provision of new sidewalks, which shall link this residential development to North Main Street, the borough may wish to consider if additional measures can be taken to facilitate walking to and from local destinations, such as Hatfield Elementary School and the central business district. We recommend that the large driveway curb cut have ADA curb ramps and a marked crosswalk.
- B. Dead-End Street. The access driveway delineated on the plan may be deemed a dead end street. It is uncertain how cars will adequately maneuver within the area at the end of the access drive. There appears to be a lack of backup space for Unit 4 in particular. We defer to the Borough Engineer to determine if the layout, as proposed, meets applicable municipal standards. A turnaround area may need to be considered. We recommend that future plan submissions include a truck turning template showing how trash trucks or emergency vehicles could enter/exit the site.

### LANDSCAPING

The proposed plan removes two existing trees, while adding four new trees and eight shrubs. Section 22-420.1.C(3)(a) of the Subdivision and Land Development Ordinance (SALDO) requires a ratio of at least two trees for each 100 feet of property line. While the applicant has requested a waiver of the tree requirements, we recommend additional trees to provide appropriate shade and aesthetic where feasible. All shade trees should be from the list provided in § 22-421 of the SALDO. Native trees can be planted within the Riparian Corridor Conservation Overlay District in accordance with § 22-433 of the SALDO.

## STORMWATER MANAGEMENT

We recommend that the developer provide some guidance for maintenance of the underground stormwater basin, as the HOA will likely be responsible.

## PROPOSED COMMUNITY AREA

A “community area” is delineated on the plan in the rear portion of the development parcel. It is unclear how this open space area will function. We suggest that this open space could be furnished with various amenities, including enhanced landscaping, and, possibly, an area for sitting or passive recreation.

## MISCELLANEOUS

### A. Highway Occupancy Permit

As shown on the submitted plan, the applicant proposes a point of ingress/egress along Main Street (SR 0463), which is a state road maintained by the Pennsylvania Department of Transportation (PennDOT). We defer to the borough and applicant to coordinate with PennDOT concerning any issues regarding a highway occupancy permit, if applicable.

### B. Building Better Townhouse Communities

The Montgomery County Planning Commission has published a report titled Building Better Townhouse Communities, which offers suggestions, recommendations and best practices related to townhouse developments. We invite municipal officials and the applicant to download this document from our website (<https://www.montgomerycountypa.gov/1459/Publications>) to gain insight on County land development policies regarding this development type.

Of particular importance are the sections related to Townhouse Design Elements and Best Practices and open space (pg. 11), garage design standards (pg. 29), parking standards (pg. 30), and garage design options (pg. 33).

## CONCLUSION

We wish to reiterate that MCPC generally supports the applicant’s proposal but we believe that our suggested revisions will better achieve the borough’s planning objectives for residential development.

Please note that the review comments and recommendations contained in this report are advisory to the municipality and final disposition for the approval of any proposal will be made by the municipality.

Should the governing body approve a final plat of this proposal, the applicant must present the plan to our office for seal and signature prior to recording with the Recorder of Deeds office. A paper copy bearing the municipal seal and signature of approval must be supplied for our files.

Please print the assigned MCPC number (24-0003-002) on any plans submitted for final recording.

Sincerely,

A handwritten signature in black ink, appearing to read "Adam Schantz". The signature is fluid and cursive, with the first name "Adam" and last name "Schantz" clearly distinguishable.

Adam Schantz, Community Planner II  
[adam.schantz@montgomerycountypa.gov](mailto:adam.schantz@montgomerycountypa.gov) – 610-278-3722

cc: Pennington Property Group, Applicant  
Katie Vlahos, Assistant Borough Manager  
Scott Burton, PennDOT  
Paul Lutz, PennDOT  
Fran Hanney, PennDOT

Attachment A: Aerial Image of Site  
Attachment B: Reduced Copy of Applicant's Proposed Site Plan

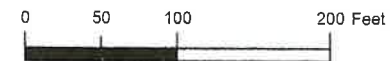




Hatfield Walk  
MCPC#240003002

Montgomery  
County  
Planning  
Commission

Montgomery County Courthouse - Planning Commission  
PO Box 311 Norristown PA 19404-0311  
(p) 610 278-3722 (f) 610 278-3941  
[www.montcopa.org/plancom](http://www.montcopa.org/plancom)  
Aerial photography provided by Nearmap









# ZHB Decision

**BEFORE THE ZONING HEARING BOARD  
OF HATFIELD BOROUGH**

**IN RE: THE APPLICATION OF  
ARBOR GROVE DEVELOPMENT COMPANY, LLC**

**DECISION AND ORDER**

**FINDINGS OF FACT**

1. On or about February 15, 2024, Arbor Grove Development Company, LLC (the "Applicant") submitted an Appeal (the "Application") to the Hatfield Borough Zoning Hearing Board (the "Board") requesting Variances to Sections 27-1202, 27-1204, 27-2102, 27-2108.1.G and 27-2108.1.H of the Borough's Zoning Ordinance ("Zoning Ordinance") proposing the consolidation of two separate parcels into one parcel for the development of a nine unit townhouse community.<sup>1</sup>

2. The properties which are the subject of the Application (collectively the "Subject Property") are owned by Robert L. Kaler, III and Joanne E. Moyer (Parcel No. 09-00-01012-00-5) and Barry V. Moyer and Joanne E. Moyer (Parcel No. 09-00-01006-00-2) located at N. Main Street and 23 N. Main Street.

3. The Subject Property is split zoned with a portion being in the Borough's CC-Core Commercial Zoning District and the remainder in the R-1 Residential Zoning District. The Board was unsure whether the zoning line followed the existing property lines.

4. The Applicant was authorized by the owners of the Subject Property to submit the Application and request the relief set forth therein as evidenced by the Owners' signature on the Application.

5. The Subject Property consists of two parcels. Parcel No. 09-00-01012-00-5 is a vacant landlocked lot identified as N. Main Street consisting of 55,067 square feet. Parcel No. 09-00-01006-00-2 is identified as 23 N. Main Street consisting of approximately 10,000 square feet (+/-) and is improved with a house.

---

<sup>1</sup> The Application, as submitted, stated the Zoning Districts as Core Commercial and R2. Applicant revised its Application to amend and correct the Zoning Districts to Core Commercial and R1. This amendment was completed prior to advertising the hearing.

6. The Subject Property is surrounded by single family homes, the post office, a borough park, and a commercial business. See Exhibit A-2.

7. A hearing on the Application (the "Hearing") occurred before the Board on March 27, 2024. At the Hearing, Board members James Rudolph, Chairman, John Pedrazzani, and Paul Mullin, Esquire were present. Dan Ruch, Alternate Member, was also present. The Board was represented by its Solicitor, Eric C. Frey, Esquire, of the law firm of Dischell, Bartle & Dooley, P.C. The Borough Manager, Jaime Snyder and Zoning Officer, Robert Heil, were also present.

8. At the Hearing, the Applicant provided testimony in support of the Application. The Applicant presented the testimony of:

- (a) Michael Amoroso, Managing Member, of Applicant; and
- (b) Robert Cunningham, P.E., Applicant's Engineer.

The Applicant was represented by Michael Meginniss, Esquire of Begley, Carlin & Mandio, LLP.

9. Two members of the public entered their appearance, without objection, as parties to the Application, as follows:

- (a) Douglas S. Renner, 25 N. Main Street; and
- (b) Janet L. McCarthy, 13 E. Broad Street.

While various other members of the public asked questions related to the Application, no other person or property owner requested party status before the Board.

10. The following documents were entered into the record as Board Exhibits:

- Exhibit B-1 - Revised Application Package;
- Exhibit B-2 - Legal Notice;
- Exhibit B-3 - Proof of Publication (published in The Reporter on March 5 and March 12, 2024); and
- Exhibit B-4 - Affidavit of Zoning Officer.

11. The following documents were entered into the record as Applicant Exhibits:

- Exhibit A-1 - Color Plan of Subject Property; and
- Exhibit A-2 - colored Aerial,

12. Neither the Borough nor the other parties offered any exhibits.

13. As set forth in the Application, the Applicant desires to consolidate the two Subject Parcels into one parcel to permit the development of a nine-unit townhouse community as shown on the plan ("Plan") marked as part of Exhibit B-1 during the Hearing.

14. All or a majority of the proposed development of the Subject Property is within the portion zoned CC-Core Commercial.

15. The Subject Property, as a combined tract, will have 62 feet of frontage on North Main Street with the largest portion of the Subject Property being a land locked tract behind the Post Office.

16. The northwest portion of the Subject Property is not developable as it contains an intermittent stream and associated floodplains and/or wetlands.

17. Prior to the current Application, the Applicant proposed multiple other proposals to the Borough, as follows:

- (a) mixed use apartments and commercial with 6,800 square feet of office with 22 apartments;
- (b) twins consisting of more than 9 units; and
- (c) Townhomes with 10 units.

18. The commercial development of the Subject Property is not practical due to the fact that there is limited road frontage.

19. The limited frontage and access would impair visibility and access for a commercial use. Further, the frontage and shape of the Subject Property presented issues for fire safety.

20. The current proposal is for a residential development consisting of nine townhomes with associated access parking and stormwater improvements ("Project")

21. The Project has proper access for fire safety and emergency vehicles.

22. The current proposal has 12 overflow parking spaces as shown on the Plan.

23. Each townhome, as shown on Exhibit A-1, would meet the following:

- (a) be 20 feet wide by 40 feet deep;
- (b) have a two car garage;

(c) have 2 surface parking spaces in a dedicated driveway;

(d) be 3 stories high;

(e) contain three bedrooms; and

(f) offer a 10 feet by 10 feet second story deck.

24. While not finally determined, it is anticipated that the proposed townhomes will sell for over \$500,000 each.

25. Each townhome is proposed to be 20 feet from rear of the townhome to a property line, with decks being 10 feet from a property line.

26. The closest townhome (townhome no. 6 on Exhibit A-1) will be 10 feet from the side of a townhome to a property line.

27. As shown on the Plan, the development of the Subject Property will include an underground detention basin and a community area.

28. The detention basin and community area are not fully designed but would be designed as required by the Borough during the Borough's subdivision and land development approval process.

29. A homeowners' association will be created to manage the roadway, parking areas, stormwater controls and other common areas as shown on the Plan.

30. The emergency access for the Project will be approved by the Fire Marshal.

31. The Applicant will not develop the area of the Subject Property next to the Borough Park and will offer the same for dedication to the Borough during the Borough's review and approval of the subdivision and land development plans for the Subject Property.

32. The access has not been approved by the Borough but will be reviewed and approved by the Borough during the Borough's review and approval of the subdivision and land development plans for the Subject Property.

33. The proposed townhomes will have less traffic impact than many if not most of the uses permitted by the Zoning Ordinance in the CC District.

34. A cul-de-sac with individual lot singles will not work on the Subject Property as the bulb would need to be 100 feet wide which would take up most of the developable area.

35. Applicant will comply with the Borough's landscaping requirements and will supplement the same to the satisfaction of the Borough as determined during the



Borough's review and approval of the subdivision and land development plans for the Subject Property.

36. The proposed townhomes are more in line than the uses permitted in the CC Zoning District and will have less impacts on the neighborhood than the permitted uses.

37. Provided the conditions set forth in the below Order are strictly enforced, the improvement and use of the Subject Property as requested will be in no way detrimental to the public health, safety, and welfare.

### DISCUSSION

Applicant has requested Variances from Section 27-1202, 27-1204, 27-2101, 27-2108.1.G. and 27-2108.1.H of the Zoning Ordinance to permit the consolidation of two lots and the development of the same as a nine unit townhouse community.

In order to qualify for the grant of a variance, Applicant is required to show that they have met the criteria set forth in Section 910.2 of the Pennsylvania Municipalities Planning Code ("MPC"), as follows:

(1) That there are unique physical circumstances or conditions, including irregularity, narrowness, or shallowness of lot size or shape, or exceptional topographical or other physical conditions peculiar to the particular property, and that the unnecessary hardship is due to such conditions, and not the circumstances or conditions generally created by the provisions of the Zoning Ordinance in the neighborhood or district in which the property is located;

(2) That because of such physical circumstances or conditions, there is no possibility that the property can be developed in strict conformity with the provisions of the Zoning Ordinance and that the authorization of a variance is therefore necessary to enable the reasonable use of the property;

(3) That such unnecessary hardship has not been created by Applicant;

(4) That the variance, if authorized, will not alter the essential character of the neighborhood or district in which the property is located, nor substantially or permanently impair the appropriate use or development of adjacent property, nor be detrimental to the public welfare; and

(5) That the variance, if authorized, will represent the minimum variance that will afford relief and will represent the least modification possible of the regulation in issue.

Applicant has established that the Subject Property possesses certain unique physical characteristics. Specifically, the Applicant identified the following hardships: (a) landlocked parcel; (b) split zoned parcel; (c) odd shape; (d) environmental conditions; (e) limited frontage. Because of the hardships, presented, the Board determined that the Subject Property cannot be

used or developed in strict conformity with the Zoning Ordinance. The Board is satisfied that the unnecessary hardship facing the use of the Subject Property, as set forth above, was not created by Applicant.

The Board has determined that the Applicant's requested variance relief will not alter the essential character of the neighborhood or district in which the Subject Property is located, nor substantially or permanently impair the appropriate use or development of adjacent property. The Board finds that the townhomes are more in line with the existing residential uses than the uses permitted in the CC Zoning District. So long as the conditions set forth in the Order below are met, the Board is satisfied that the grant of the variance relief requested will not be detrimental to the public health, safety, or welfare. The impacts of the proposed relief are mitigated by the conditions set forth in the Order.

Further, The Board has determined that Applicant has requested the minimum relief from the Zoning Ordinance necessary to effectuate a reasonable use of the Subject Property.

#### CONCLUSIONS OF LAW

1. Pursuant to Section 909.1 of the Pennsylvania Municipalities Planning Code, the Board has exclusive jurisdiction to hear and render a final adjudication relative to the Application.
2. As set forth in the Application, Applicant has standing to request the variance relief related to the Subject Property.
3. The requirements for a variance in Pennsylvania are clear and are specifically stated in Section 910.2 of the MPC. Given the testimony presented at the Hearing, a careful review of the record evidence offered in support of the requested variance relief, and with no substantive proof offered to the contrary, the Board finds that Applicant has established an entitlement to Applicant's requested variance relief so long as the conditions set forth in the Order below are met.
4. Particularly noteworthy, this Board concludes that Applicant's requested variance relief is consistent with and will not be adverse to the public health, safety, or welfare and that Applicant's requested variance relief is the minimum relief necessary so long as the conditions set forth in the Order below are met.
5. Accordingly, this Board issues the following Order.

{ ORDER ON NEXT PAGE }

## **ORDER**

**AND NOW**, this 24<sup>th</sup> day of April, 2024, the Application of Arbor Grove Development Company, LLC is hereby **GRANTED** subject to the stated conditions below. The Board **GRANTS** Variances from Sections 27-1202, 27-1204, 27-2101, 27-2108.1.G. and 27-2108.1.H of the Zoning Ordinance to permit the consolidation of two lots and the development of the same as a nine unit townhouse community as shown in the Application (Exhibit B-1) and the Plan (Exhibit A-1).

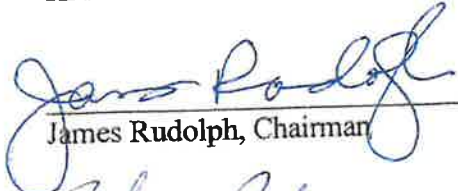
The relief is granted in accordance with the Application and plans submitted and subject to the following conditions:

1. A Homeowners' Association (HOA) shall be established to the satisfaction of the Borough during the land development process and shall include provisions for the following:
  - a. The HOA shall be responsible for, at a minimum, for roads, parking areas, stormwater, sidewalks, trails, open space, community area, landscaping and snow plowing;
  - b. The two lots making up the Subject Property shall be merged and developed as a united Project/property; and
  - c. Open Space shall be restricted from further development and shall be offered to the Borough for dedication.
2. Landscaping and buffering shall be designed to the satisfaction of the Borough during the land development process and shall be provided between the Subject Property and all adjoining residentially used tracts;
3. There shall be a maximum of nine (9) townhome units;
4. All townhomes shall be a minimum of twenty feet (20') from all property lines with the exceptions that:
  - a. patios and decks may be a minimum of ten feet (10') from any property line; and
  - b. the townhouse unit next to the post office property (Parcel No. 09-00-01015-00-2) shall have a minimum side yard setback of ten (10) feet.
5. There shall be a minimum of twelve (12) shared parking spaces as shown on the plans;
6. Any stormwater basin shall be a minimum of fifteen feet (15') from any building;

7. All townhouse units shall have a minimum of two garage parking spaces and two surface driveway parking spaces;
8. The final design of the Project shall be approved by the Borough Fire Marshal prior to the final land development approval of the Project;
9. The site access shall be designed to the satisfaction of the Borough and PennDOT, if necessary;
10. The Project shall conform to the density and dimensional standards of the R-4 Zoning District as set forth in the Table 27-15-3 of the Zoning Ordinance, with the following exceptions:
  - a. Minimum rear yard shall be reduced to 20 feet (decks/patios may be ten (10) feet as set forth above);
  - b. The minimum lot width for any end unit shall be 25 feet;
11. Refuse collection facilities must be provided as set forth in the Borough's Code;
12. The Project shall conform to the regulations of the Flood Plain Conservation District including any riparian buffer requirements. Clearing of existing vegetation, parking lots and stormwater management facilities, among other improvements, are prohibited in the Riparian Corridor District;
13. Lighting levels and fixtures shall not create a nuisance on adjacent properties;
14. A privacy fence shall be installed along the Renner Property (Parcel No. 09-00-01009-00-8) and along the entire southern property line to shield adjacent residentially used properties from vehicles;
15. The proposed sidewalk along the Renner Property (Parcel No. 09-00-01009-00-8) shall be located adjacent to the proposed curbing for the access drive to the fullest extent practical;
16. All conditions of approval shall be complied with prior to the final Land Development and/or Subdivision approval by the Borough; and
17. The Development and the use of the Subject Property shall be substantially consistent with the testimony and exhibits presented at the Hearing on the Application.

The Foregoing Findings of Facts, Discussion, Conclusions of Law and Order, are hereby approved as the Decision and Order of the Board.

ZONING HEARING BOARD OF  
HATFIELD BOROUGH

  
James Rudolph, Chairman

  
John Pedrazzani, Secretary

  
Paul Mullin, Esquire, Member

Written Decision mailed: 4-25-2024

**NOTE TO APPLICANT**

There is a thirty (30) day period after the date of a decision for an aggrieved person to file an appeal in the Court of Common Pleas of Montgomery County to contest an approval or denial by the Zoning Hearing Board. If the Application has been granted by the Zoning Hearing Board, the Applicant may act on said approval during this thirty (30) day appeal period; however, the Applicant will do so at his/her own risk. If the Applicant received Zoning Hearing Board approval, the Applicant must still secure all necessary and applicable permits from Hatfield Borough within twelve (12) months of the date of the approval of the Zoning Hearing Board.



## **4. Old Business:**

**A. Bennetts Court Update**

**B. Didden Greenhouses Update**

**C. 43 Roosevelt Avenue Update**

**6. NEW BUSINESS / DISCUSSION ITEMS:**

**A. 2025 Preliminary Budget Update  
Discussion**



# Borough of Hatfield

Montgomery County, Pennsylvania

## *Memorandum*

### **BOROUGH OF HATFIELD**

To: Borough Council & Mayor Girard

From: Jaime E. Snyder, Borough Manager

Date: November 20, 2024

Subject: 2025 Advertising Requirements

- Notice that the Budget is available for inspection must run ONCE at least TEN days before its adoption
- RE Tax Ordinance for INCREASES must be run ONCE at least SEVEN days before it is adopted
- Electric Fee / Rate Increase is adopted by Resolution and no advertising is required
- Sewer Fee / Rate Increase is adopted by Resolution and no advertising is required
- If NO T RE Tax increase, a Resolution is adopted, and no advertising is required

### **EXAMPLE TIME FRAME WITH PROPOSED BUDGET:**

Motion to Advertise December 4, 2024, the proposed 2025 budget (can still be adjusted) and any required Ordinances.

At the December 18, 2024, meeting these will need to be adopted, if incorporated in the proposed 2025 budget:

- 2025 Budget (appropriating specific funds)
- RE Tax Ordinance (if increase)
- RE Tax Resolution (if no increase)
- Electric Rate Resolution (if increase)
- Sewer Rate Resolution (if increase)

401 S. Main Street  
P.O. Box 190  
Hatfield, PA 19440

**Phone:**  
215-855-0781

**Fax:**  
215-855-2075

**Email:**  
admin@  
hatfieldborough.com

**Website:**  
[www.hatfieldborough.com](http://www.hatfieldborough.com)

# 2025 BUDGET

**DRAFT**

FUND	REVENUES	EXPENDITURES	SUBTOTAL	BEGINNING FUND BALANCE
01-GENERAL	\$2,244,709.64	\$2,226,931.11	\$17,778.53	\$0.00
03-FIRE PROTECTION	\$61,244.72	\$61,101.59	\$143.52	\$0.00
07-ELECTRIC	\$4,110,415.00	\$4,109,925.40	\$489.60	\$0.00
08-SEWER	\$834,300.00	\$808,564.26	\$25,735.74	\$0.00
18-CAPITAL PROJECTS	\$3,491,095.00	\$3,471,249.57	\$19,845.43	\$0.00
30-CAPITAL RESERVE	\$0.00	\$0.00	\$0.00	\$0.00
31-SEWER CAPITAL RESERVE	\$220,900.00	\$107,550.00	\$113,350.00	\$0.00
35-HIGHWAY AID	\$83,923.61	\$115,400.00	(\$31,476.39)	\$62,000.00
<b>TOTAL</b>	<b>\$11,046,587.97</b>	<b>\$10,900,721.93</b>	<b>\$145,866.43</b>	<b>\$62,000.00</b>

**DRAFT**

# GENERAL FUND REVENUES

## FUND #01

Catagory	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Notes
<b>REAL ESTATE AND LOCAL TAXES</b>										
Real Prop	301.100	Real Estate Tax: Curnt Yr	\$268,611	\$311,860	\$407,093	\$387,228	\$413,750	\$413,750	\$558,146	.004 Mils
Tax	301.300	RE Taxes Delinquent	\$0	\$0	\$0	\$0	\$1,000	\$1,000	\$1,000	
	301.500	Real Estate Tax: Del/Lien	\$768	\$2,130	\$5,764	\$4,366	\$1,000	\$1,000	\$1,000	
		<b>SUBTOTAL</b>	<b>\$269,379</b>	<b>\$313,990</b>	<b>\$412,857</b>	<b>\$391,594</b>	<b>\$415,750</b>	<b>\$415,750</b>	<b>\$560,146</b>	
Local	310.100	Real Estate Transfer Tax	\$68,146	\$88,578	\$89,798	\$30,248	\$60,495	\$50,000	\$50,000	.005% of sale
Taxes										
	310.210	Earned Inc Tax: Curnt Yr		\$496,504	\$512,441	\$281,629	\$563,258	\$455,000	\$500,000	
	310.220	Earned Inc Tax: Prior Yr	\$0	\$0	\$0	\$0	\$0	\$1,000	\$1,000	
	310.510	LST TAX Current Year	\$41,997	\$51,428	\$58,552	\$32,774	\$65,548	\$45,000	\$55,000	
	310.520	LST TAX Prior Year		\$0	\$0	\$0	\$0	\$0	\$0	
	310.530	LST TAX Delinquent		\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$110,142</b>	<b>\$636,510</b>	<b>\$660,790</b>	<b>\$344,651</b>	<b>\$623,753</b>	<b>\$551,000</b>	<b>\$606,000</b>	
<b>LICENSES, PERMITS AND FINES</b>										
Bus. Lic &	321.620	Plumbers License	\$230	\$0	\$0	\$140	\$280	\$87	\$87	
Permits	321.630	Electricians License	\$50	\$0	\$0	\$0	\$0	\$169	\$169	
	321.700	Amusement License	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	321.800	Comcast TV Franchise	\$20,896	\$20,306	\$18,683	\$8,428	\$16,856	\$20,000	\$20,000	
	321.810	Verizon Franchise	\$19,788	\$18,868	\$17,585	\$8,106	\$16,212	\$20,000	\$20,000	
	321.900	Pole Rental Fee	\$975	\$975	\$975	\$63,250	\$63,250	\$975	\$975	
		<b>SUBTOTAL</b>	<b>\$41,939</b>	<b>\$40,149</b>	<b>\$37,243</b>	<b>\$79,924</b>	<b>\$96,599</b>	<b>\$41,231</b>	<b>\$41,231</b>	
Non-Bus	322.820	Street Encroach Permit	\$0	\$1,214	\$310	\$0	\$0	\$438	\$438	
Permits		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$1,214</b>	<b>\$310</b>	<b>\$0</b>	<b>\$0</b>	<b>\$438</b>	<b>\$438</b>	
Fines	331.110	Vehicle Code Violations	\$934	\$2,878	\$3,437	\$1,825	\$3,651	\$722	\$722	
	331.120	Ord./State Pol. Fines	\$1,121	\$1,160	\$1,142	\$604	\$1,208	\$1,306	\$1,306	



# GENERAL FUND REVENUES

## FUND #01

Catagory	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Notes
		<b>SUBTOTAL</b>	<b>\$2,056</b>	<b>\$4,038</b>	<b>\$4,579</b>	<b>\$2,429</b>	<b>\$4,859</b>	<b>\$2,028</b>	<b>\$2,028</b>	
<b>INTEREST AND RENTAL EARNINGS</b>										
Interest	341.000	Interest Income - Invmts	\$0	\$1,594	\$8,111	\$6,986	\$13,972	\$5,000	\$5,000	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$1,594</b>	<b>\$8,111</b>	<b>\$6,986</b>	<b>\$13,972</b>	<b>\$5,000</b>	<b>\$5,000</b>	
Rental	342.300	Rent from Cell Tower	\$48,271	\$47,444	\$48,423	\$28,448	\$56,896	\$50,000	\$50,000	
Earnings	342.310	Rent for SEPTA Prop	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$48,271</b>	<b>\$47,444</b>	<b>\$48,423</b>	<b>\$28,448</b>	<b>\$56,896</b>	<b>\$50,000</b>	<b>\$50,000</b>	
<b>STATE GRANTS &amp; SHARED REVENUES</b>										
State	354.030	PennDOT - Winter Mainte	\$6,847	\$6,984	\$8,120	\$0	\$5,045	\$8,120	\$8,120	New Agreeme
Grants	354.150	Act 101 Recycling Grant	\$0	\$12,865	\$0	\$5,126	\$8,500	\$5,000	\$5,000	Ten year
		<b>SUBTOTAL</b>	<b>\$6,847</b>	<b>\$19,849</b>	<b>\$8,120</b>	<b>\$5,126</b>	<b>\$13,545</b>	<b>\$13,120</b>	<b>\$13,120</b>	
State Shar	355.010	Public Utility Prop Taxes	\$770	\$822	\$958	\$0	\$615	\$615	\$615	
Revenues	355.050	Mun. Pen. Sys. State Aid	\$47,973	\$48,968	\$58,495	\$0	\$35,000	\$49,235	\$49,235	
	355.080	Alcoholic Beverage Tax	\$0	\$600	\$600	\$200	\$600	\$600	\$600	
		<b>SUBTOTAL</b>	<b>\$48,743</b>	<b>\$50,390</b>	<b>\$60,053</b>	<b>\$200</b>	<b>\$36,215</b>	<b>\$50,450</b>	<b>\$50,450</b>	
	358.400	Dividends from DVIT	\$10,762	\$7,024	\$4,918	\$0	\$10,000	\$10,762	\$10,762	True # Checks
	358.410	Grants from DVIT/DWCT	\$0	\$0	\$0	\$0	\$3,300	\$2,200	\$1,500	WC & Liabili
		<b>SUBTOTAL</b>	<b>\$10,762</b>	<b>\$7,024</b>	<b>\$4,918</b>	<b>\$0</b>	<b>\$13,300</b>	<b>\$12,962</b>	<b>\$12,262</b>	
<b>GENERAL GOVERNMENT &amp; PUBLIC SAFETY</b>										
Gen. Gov'	361.300	Subdivision/Devel Fee	\$0	\$2,500	\$0	\$0	\$0	\$1,200	\$1,200	
	361.330	Zoning Hearing Fee	\$2,800	\$3,650	\$3,303	\$4,925	\$9,850	\$4,609	\$4,609	
	361.340	Rezoning/Cond Use Fee	\$0	\$0	\$0	\$0	\$0	\$500	\$500	
	361.350	BOCA Hearing Fees	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	361.500	Sale of Maps/Pub/Codes	\$0	\$0	\$0	\$0	\$0	\$0	\$0	

# GENERAL FUND REVENUES

## FUND #01

Catagory	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Notes
	361.710	Reproduction of records -	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	361.750	Transfer Cert & Inspection	\$3,545	\$2,145	\$980	\$220	\$440	\$2,000	\$2,000	
		<b>SUBTOTAL</b>	<b>\$6,345</b>	<b>\$8,295</b>	<b>\$4,283</b>	<b>\$5,145</b>	<b>\$10,290</b>	<b>\$8,309</b>	<b>\$8,309</b>	
Pub. Safet	362.410	Building Permits	\$27,039	\$22,301	\$16,742	\$42,044	\$84,087	\$17,500	\$25,000	
	362.420	Electrical Permits	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	362.430	Plumbing Permits	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	362.440	Fire/Prop Insp. Fees	\$25,790	\$28,390	\$26,820	\$22,410	\$44,820	\$32,500	\$92,500	32500 Fire
	362.450	Use & Occup Permits	\$0	\$415	\$2,260	\$1,285	\$2,570	\$2,000	\$3,000	60000 Rental
	362.470	Sign Permits	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	362.480	Other Fire Safety Permits	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$52,829</b>	<b>\$51,106</b>	<b>\$45,821</b>	<b>\$65,739</b>	<b>\$131,477</b>	<b>\$52,000</b>	<b>\$120,500</b>	
Sanitation	364.500	Recycling Energy	\$0	\$0	\$0	\$199	\$0	\$0	\$0	
	364.512	Sale of Leaf Bags	\$64	\$48	\$102	\$0	\$75	\$87	\$87	
		<b>SUBTOTAL</b>	<b>\$64</b>	<b>\$48</b>	<b>\$102</b>	<b>\$199</b>	<b>\$75</b>	<b>\$87</b>	<b>\$87</b>	
<b>MISCELLANEOUS &amp; INTERFUND TRANSFERS</b>										
Misc. Rev	380.000	Miscellaneous Revenue	\$205,678	\$294,596	\$37,199	\$1,243,478	\$15,000	\$17,389	\$17,389	See Attached
		<b>SUBTOTAL</b>	<b>\$205,678</b>	<b>\$294,596</b>	<b>\$37,199</b>	<b>\$1,243,478</b>	<b>\$15,000</b>	<b>\$17,389</b>	<b>\$17,389</b>	
Proceeds of	391.100	Sales of Gen Fixed Assets	\$1,893	\$51,649	\$0	\$27,870	\$55,740	\$25,000	\$5,000	
Sales										
	391.200	Compensation for Losses	\$0	\$0	\$0	\$0	\$0	\$2,750	\$2,750	
		<b>SUBTOTAL</b>	<b>\$1,893</b>	<b>\$51,649</b>	<b>\$0</b>	<b>\$27,870</b>	<b>\$55,740</b>	<b>\$27,750</b>	<b>\$7,750</b>	
Interfund	392.070	Trans from Elec Fund	\$1,280,000	\$1,245,000	\$1,350,000	\$350,000	\$700,000	\$975,000	\$750,000	
Transfers	392.080	Trans from Sewer Fund	\$0	\$0	\$0	\$0	\$0	\$0	\$0	

# GENERAL FUND REVENUES

## FUND #01

Catagory	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Notes
	392.300	Trans from Cap Res Fund	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$1,280,000</b>	<b>\$1,245,000</b>	<b>\$1,350,000</b>	<b>\$350,000</b>	<b>\$700,000</b>	<b>\$975,000</b>	<b>\$750,000</b>	
	398.000	Trans From Invested Fund	\$1,051,899	\$503,878	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$1,051,899</b>	<b>\$503,878</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
<b>TOTAL REVENUES</b>			<b>\$3,136,846</b>	<b>\$3,276,774</b>	<b>\$2,682,808</b>	<b>\$2,551,789</b>	<b>\$2,187,470</b>	<b>\$2,222,514</b>	<b>\$2,244,710</b>	

# GENERAL FUND EXPENSES

## FUND #01

Catagory	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
<b>GENERAL GOVERNMENT</b>										
Governing	400.105	Council Mayor Comp.	\$3,250	\$3,250	\$3,250	\$1,950	\$3,900	\$3,250	\$3,250	\$6,500
Body	400.199	GF - Council/Mayor Life	\$200	\$214	\$204	\$121	\$242	\$348	\$348	\$738
	400.420	Dues/Subscr/Memberships	\$855	\$2,204	\$1,371	\$833	\$1,666	\$1,000	\$750	\$1,500
	400.460	Meetings/Conferences	\$1,651	\$5,285	\$805	\$359	\$717	\$2,000	\$500	\$1,000
		<b>SUBTOTAL</b>	<b>\$5,955</b>	<b>\$10,953</b>	<b>\$5,630</b>	<b>\$3,263</b>	<b>\$6,525</b>	<b>\$6,598</b>	<b>\$4,848</b>	
Executive	401.110	Salary: Manager	\$76,498	\$49,542	\$51,942	\$25,539	\$51,077	\$50,960	\$52,015	\$104,030
	401.116	Administrative Assist		\$0	\$0	\$0	\$0	\$0	\$0	\$0
	401.187	General Compensation	\$963	\$881	\$756	\$389	\$778	\$750	\$750	\$1,500
	401.211	Newsletter Printing	\$2,175	\$3,658	\$8,120	\$2,902	\$5,804	\$6,000	\$6,500	\$13,000
	401.215	Newsletter Postage	\$3,000	\$1,500	\$2,000	\$2,000	\$4,000	\$1,500	\$0	\$0
	401.337	Automobile Allowance		\$0	\$3,000	\$1,500	\$0	\$3,000	\$3,000	\$6,000
	401.340	Legal Advertising/Printing	\$6,554	\$8,659	\$3,638	\$1,540	\$3,079	\$9,000	\$8,000	\$16,000
	401.353	Bonding Fee	\$125	\$338	\$0	\$0	\$0	\$150	\$150	\$257
	401.420	Dues & Memberships	\$1,026	\$2,149	\$1,323	\$855	\$1,711	\$1,000	\$1,030	\$2,030
	401.460	Conferences & Training	\$1,992	\$1,450	\$1,439	\$18	\$36	\$2,706	\$2,500	\$5,000
		<b>SUBTOTAL</b>	<b>\$92,332</b>	<b>\$68,177</b>	<b>\$72,219</b>	<b>\$34,742</b>	<b>\$66,485</b>	<b>\$75,066</b>	<b>\$73,945</b>	
Financial	402.112	Wages: Finance Staff	\$31,610	\$35,929	\$34,806	\$18,718	\$37,437	\$34,957	\$36,003	\$72,007
Admin	402.180	Overtime Pay	\$1,788	\$2,759	\$2,339	\$1,134	\$2,269	\$1,180	\$1,227	\$1,679
	402.187	General Compensation	\$875	\$884	\$881	\$250	\$500	\$875	\$875	\$1,750
	402.210	Office Supplies	\$4,179	\$4,155	\$3,958	\$2,858	\$5,716	\$4,179	\$4,433	\$10,225
	402.215	Postage	\$4,094	\$6,535	\$3,973	\$3,460	\$6,919	\$3,000	\$5,000	\$10,000
	402.238	Clothing & Uniforms	\$0	\$0		\$0	\$0	\$1,030	\$250	\$500
	402.310	Payroll Service Fees	\$1,947	\$1,924	\$2,248	\$982	\$1,963	\$2,150	\$2,215	\$7,706
	402.311	Auditing Services Fees	\$12,850	\$10,050	\$23,350	\$525	\$1,050	\$12,500	\$13,261	\$27,180
	402.312	Professional Services	\$0	\$3,500	\$4,867	\$1,953	\$3,906	\$2,500	\$2,652	\$5,152
	402.321	Telephone Monthly Charge	\$5,060	\$5,600	\$4,958	\$2,828	\$5,657	\$4,057	\$4,179	\$7,780

# GENERAL FUND EXPENSES

## FUND #01

Catagory	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
	402.331	Travel Mileage Reimburse	\$0	\$0	\$0	\$0	\$0	\$500	\$530	\$1,030
	402.353	Bonding Fee	\$125	\$125	\$125	\$0	\$0	\$129	\$133	\$274
	402.374	Office Equip: Repair/Maint	\$0	\$0	\$0	\$0	\$0	\$289	\$298	\$1,079
	402.384	Office Equip: Lease	\$3,814	\$3,394	\$3,880	\$2,250	\$4,499	\$3,939	\$3,027	\$6,942
	402.420	Dues, Subscriptions, Mem.	\$213	\$411	\$178	\$0	\$0	\$500	\$530	\$1,061
	402.460	Conferences & Training	\$10	\$107	\$409	\$88	\$175	\$750	\$796	\$1,546
		<b>SUBTOTAL</b>	<b>\$66,565</b>	<b>\$75,374</b>	<b>\$85,973</b>	<b>\$35,046</b>	<b>\$70,091</b>	<b>\$72,535</b>	<b>\$75,409</b>	
Tax	403.105	Salary: Tax Collector	\$5,000	\$5,000	\$5,000	\$2,500	\$5,000	\$5,000	\$5,000	
Collection	403.210	Office Supplies	\$611	\$695	\$831	\$915	\$1,829	\$250	\$250	
	403.353	Bonding Fee	\$560	\$174	\$0	\$0	\$0	\$289	\$298	
	403.420	Conferences/Training	\$601	\$200	\$200	\$0	\$0	\$289	\$298	
	403.430	TCC Fees	\$0	\$0	\$0	\$0	\$0	\$530	\$530	
	403.460	Memberships	\$100	\$0	\$0	\$0	\$0	\$109	\$109	
		<b>SUBTOTAL</b>	<b>\$6,872</b>	<b>\$6,069</b>	<b>\$6,031</b>	<b>\$3,415</b>	<b>\$6,829</b>	<b>\$6,468</b>	<b>\$6,485</b>	
Legal	404.314	Legal Services	\$10,475	\$9,777	\$10,194	\$5,092	\$10,183	\$17,500	\$12,500	\$25,000
	404.450	Special Legal Services	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	404.451	Code Update Services	\$9,013	\$7,632	\$1,195	\$1,195	\$2,390	\$2,000	\$2,000	
		<b>SUBTOTAL</b>	<b>\$19,488</b>	<b>\$17,409</b>	<b>\$11,389</b>	<b>\$6,287</b>	<b>\$12,573</b>	<b>\$19,500</b>	<b>\$14,500</b>	
Secretary/Clerk										
	405.112	Assistant to the Manager	\$41,490	\$26,603	\$24,640	\$14,019	\$28,038	\$28,122	\$28,965	\$57,930
	405.187	General Compensation	\$875	\$631	\$631	\$250	\$500	\$750	\$750	\$1,500
	405.420	Dues, Subscriptions, Memb	\$333	\$80	\$302	\$150	\$299	\$500	\$500	\$1,000
	405.460	Conferences & Training	\$880	\$0	\$80	\$0	\$0	\$1,500	\$500	\$1,000
			<b>\$43,578</b>	<b>\$27,313</b>	<b>\$25,652</b>	<b>\$14,419</b>	<b>\$28,538</b>	<b>\$30,872</b>	<b>\$30,715</b>	
General Government Admin										
	406.112	Utility Billing/Admin	\$25,915	\$27,305	\$27,654	\$15,032	\$30,065	\$29,843	\$29,848	\$99,493
	406.180	Utility Billing/Admin OT	\$545	\$263	\$337	\$246	\$492	\$383	\$383	\$1,037



# GENERAL FUND EXPENSES

## FUND #01

Catagory	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
	406.187	General Compensation	\$1,538	\$1,527	\$1,637	\$523	\$1,045	\$1,500	\$1,500	\$3,000
			<b>\$27,997</b>	<b>\$29,095</b>	<b>\$29,628</b>	<b>\$15,801</b>	<b>\$31,602</b>	<b>\$31,726</b>	<b>\$31,731</b>	
Data	407.241	Computer Software	\$0	\$0	\$29	\$567	\$1,133	\$2,319	\$1,000	\$2,000
Processing	407.252	Computer Repair & Parts	\$0	\$1,027	\$0	\$0	\$0	\$597	\$615	\$1,167
	407.310	Software Maintenance Fee	\$4,142	\$3,226	\$1,460	\$66	\$132	\$4,150	\$4,275	\$8,893
	407.325	Internet Fees	\$1,651	\$2,177	\$2,029	\$1,010	\$2,021	\$1,612	\$1,660	\$4,479
	407.329	Document Retention	\$0	\$5,026	\$12,244	\$0	\$0	\$7,500	\$1,500	\$3,000
	407.331	Cloud Services	\$0	\$0	\$3,667	\$0	\$0	\$0	\$0	\$0
	407.452	Contract IT Services	\$3,629	\$4,618	\$4,327	\$1,768	\$3,535	\$5,000	\$5,000	\$10,000
	407.453	Web Design/Maintenance	\$2,409	\$1,209	\$1,200	\$1,200	\$2,400	\$1,200	\$1,800	\$3,600
	407.610	HTV Equipment	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	407.750	Computer Equipment	\$776	\$6	\$1,543	\$0	\$0	\$1,000	\$1,000	\$2,000
		<b>SUBTOTAL</b>	<b>\$12,606</b>	<b>\$17,289</b>	<b>\$26,499</b>	<b>\$4,610</b>	<b>\$9,221</b>	<b>\$23,378</b>	<b>\$16,851</b>	
Engineer	408.313	Engineering Services	\$57,333	\$102,739	\$61,685	\$43,392	\$86,784	\$62,500	\$62,500	\$125,000
	408.450	Special Engineer Mun Com	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
		<b>SUBTOTAL</b>	<b>\$57,333</b>	<b>\$102,739</b>	<b>\$61,685</b>	<b>\$43,392</b>	<b>\$86,784</b>	<b>\$62,500</b>	<b>\$62,500</b>	
Bldgs & Plant	409.366	Public Utility Services	\$922	\$1,206	\$1,305	\$774	\$1,548	\$2,000	\$2,000	\$3,000
	409.367	Waste Disposal Services	\$965	\$905	\$1,041	\$527	\$1,055	\$2,000	\$2,000	\$3,887
	409.370	Bldg Repair/Maint	\$13,969	\$10,771	\$19,240	\$6,092	\$12,185	\$12,500	\$11,250	\$22,500
	409.372	Scout Cabin Repair/ Maint	\$0	\$286	\$0	\$0	\$0	\$2,000	\$0	
	409.375	SEPTA Property	\$0	\$0	\$10,949	\$30,000	\$60,000	\$60,000	\$0	
	409.430	Property Taxes	\$3,313	\$3,314	\$3,676	\$825	\$1,650	\$298	\$298	
	409.450	Contracted Services: Clean	\$3,515	\$2,750	\$2,781	\$2,015	\$4,030	\$4,030	\$4,030	\$7,030
		<b>SUBTOTAL</b>	<b>\$22,683</b>	<b>\$19,231</b>	<b>\$38,992</b>	<b>\$40,234</b>	<b>\$80,467</b>	<b>\$82,828</b>	<b>\$19,578</b>	
<b>PUBLIC SAFETY</b>										
	410.310	Police Protection Services	\$925,000	\$925,000	\$950,000	\$ 497,502	\$995,000	\$995,000	\$ 1,045,000	Thru 2026

# GENERAL FUND EXPENSES

## FUND #01

Catagory	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
	410.311	Police Station Debt							\$ 50,000	
	410.315	VMSC							\$ 15,000	
	410.241	Operating Supplies - Camer	\$218	\$5,000	\$1,695	\$0	\$0	\$5,000	\$3,000	
		<b>SUBTOTAL</b>	<b>\$ 925,218</b>	<b>\$ 930,000</b>	<b>\$ 951,695</b>	<b>\$497,502</b>	<b>\$995,000</b>	<b>\$1,000,000</b>	<b>\$ 1,113,000</b>	
Fire	411.240	Supplies	\$0	\$0	\$361	\$0	\$0	\$250	\$250	
	411.310	Fire Safety Inspection Svcs.	\$0	\$20,976	\$52,934	\$18,468	\$36,936	\$15,450	\$15,914	
	411.317	Fire Marshall Svcs.	\$0	\$0	\$6,004	\$0	\$0	\$4,030	\$4,151	
	411.318	EMC Services	\$0	\$0	\$0	\$0	\$0	\$7,000	\$0	
	411.363	Hydrant Service	\$4,796	\$4,801	\$4,865	\$4,870	\$4,870	\$5,857	\$6,033	
		<b>SUBTOTAL</b>	<b>\$4,796</b>	<b>\$25,777</b>	<b>\$64,164</b>	<b>\$23,338</b>	<b>\$41,806</b>	<b>\$32,587</b>	<b>\$26,347</b>	
Code	413.240	Supplies	\$0	\$0	\$207	\$248	\$495	\$0	\$1,000	
Enforceme	413.310	Code Enforcement Svcs	\$38,380	\$27,563	\$30,932	\$17,632	\$35,264	\$30,796	\$31,720	
	413.317	Building Inspection Svcs	\$10,602	\$12,768	\$19,378	\$10,964	\$21,928	\$14,405	\$14,837	
	413.340	Advertising/Printing	\$0	\$0	\$0	\$0	\$0	\$270	\$278	
	413.420	Dues/Memb/Subscr	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	413.460	Conferences/Training	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$48,982</b>	<b>\$40,331</b>	<b>\$50,517</b>	<b>\$28,844</b>	<b>\$57,687</b>	<b>\$45,471</b>	<b>\$47,835</b>	
Zoning	414.313	Engineering Services	\$0	\$0	\$0	\$0	\$0	\$2,500	\$2,500	
	414.314	Legal Services	\$3,336	\$3,717	\$8,530	\$2,941	\$5,882	\$5,000	\$5,150	
	414.317	Zoning Officer Svcs	\$0	\$0	\$209	\$0	\$0	\$0	\$0	
	414.318	Transcription Services	\$1,126	\$2,763	\$1,050	\$300	\$600	\$2,500	\$2,575	
	414.340	Advertising/Printing	\$0	\$0	\$0	\$790	\$1,581	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$4,462</b>	<b>\$6,480</b>	<b>\$9,789</b>	<b>\$4,031</b>	<b>\$8,063</b>	<b>\$9,999</b>	<b>\$10,225</b>	
<b>PUBLIC WORKS</b>										
Recycling	426.244	Operating Supplies	\$2,163	\$0	\$0	\$0	\$0	\$773	\$796	

# GENERAL FUND EXPENSES

## FUND #01

Catagory	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
	426.368	Leaf & Wood Chip Recycli	\$5,826	\$7,123	\$7,610	\$2,714	\$5,428	\$6,695	\$6,896	
		<b>SUBTOTAL</b>	<b>\$7,989</b>	<b>\$7,123</b>	<b>\$7,610</b>	<b>\$2,714</b>	<b>\$5,428</b>	<b>\$7,468</b>	<b>\$7,692</b>	
Roads &	430.110	Salary: Public Works Dir	\$45,318	\$36,125	\$34,867	\$23,222	\$46,445	\$36,369	\$37,460	\$74,920
Streets	430.111	General Compensation	\$1,000	\$884	\$881	\$250	\$500	\$875	\$875	\$1,750
	430.112	Wages: Maintenance Crew	\$85,306	\$95,102	\$97,146	\$45,244	\$90,488	\$97,126	\$98,790	\$197,280
	430.115	Wages: Summer Help	\$5,933	\$0	\$0	\$0	\$0	\$5,500	\$0	\$0
	430.180	OT Wages: Maintenance	\$6,340	\$5,084	\$1,236	\$3,323	\$6,646	\$2,508	\$2,660	\$6,390
	430.220	Operating Supplies	\$4,755	\$3,675	\$3,636	\$1,803	\$3,605	\$4,000	\$4,000	\$8,000
	430.222	Chemicals	\$313	\$346	\$150	\$97	\$195	\$597	\$615	\$1,284
	430.231	Operating Fuel	\$5,457	\$7,433	\$5,497	\$2,730	\$5,461	\$6,250	\$6,250	\$12,500
	430.245	Street Materials	\$1,743	\$3,381	\$3,048	\$1,814	\$3,629	\$4,059	\$4,180	\$8,438
	430.260	Small Tools/Minor Equip	\$1,904	\$2,443	\$405	\$581	\$1,161	\$2,089	\$2,152	\$4,500
	430.316	Drug/Alcohol Testing	\$296	\$219	\$141	\$274	\$547	\$406	\$643	\$1,285
	430.324	Cell phones	\$2,763	\$2,448	\$2,816	\$863	\$1,725	\$2,089	\$2,152	\$3,987
	430.384	Equipment Rental	\$330	\$51	\$46	\$942	\$1,883	\$1,273	\$750	\$1,500
	430.386	Uniform	\$1,706	\$580	\$738	\$762	\$1,523	\$750	\$750	\$1,500
	430.420	Dues/Subscr/Memberships	\$30	\$18	\$97	\$68	\$135	\$250	\$250	\$500
	430.460	Conferences & Training	\$258	\$788	\$898	\$695	\$1,390	\$1,000	\$1,000	\$1,950
	430.740	Equipment Purchase	\$0	\$36,633	\$0	\$61,729	\$61,729	\$61,729	\$61,729	\$61,729
	430.750	Materials Purchase	\$0	\$165	\$1,457	\$339	\$678	\$2,500	\$750	\$1,500
		<b>SUBTOTAL</b>	<b>\$163,452</b>	<b>\$195,375</b>	<b>\$153,060</b>	<b>\$144,735</b>	<b>\$227,740</b>	<b>\$229,370</b>	<b>\$225,007</b>	
Snow &	432.450	Contracted Snow Removal	\$0	\$0	\$0	\$714	\$3,000	\$2,000	\$0	
Ice		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$714</b>	<b>\$3,000</b>	<b>\$2,000</b>	<b>\$0</b>	
Signs &	433.245	Signs and Posts	\$2,082	\$4,828	\$4,085	\$2,479	\$4,958	\$5,000	\$4,000	
Signals	433.246	Traffic Signal Supplies	\$0	\$0	\$1,359	\$0	\$0	\$4,000	\$0	
	433.450	Contracted Services	\$1,100	\$2,133	\$1,249	\$6,713	\$13,427	\$2,000	\$6,000	
		<b>SUBTOTAL</b>	<b>\$3,182</b>	<b>\$6,961</b>	<b>\$6,693</b>	<b>\$9,192</b>	<b>\$18,385</b>	<b>\$11,000</b>	<b>\$10,000</b>	

# GENERAL FUND EXPENSES

## FUND #01

Catagory	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
Equipment	437.251	Vehicle & Equipment Parts	\$5,604	\$4,169	\$2,921	\$0	\$0	\$0	\$0	\$0
& Truck	437.241	Vehicle Tires	\$706	\$191	\$330	\$0	\$0	\$0	\$0	\$0
Repair	437.451	Contracted Services - Vehic	\$2,940	\$14,415	\$4,574	\$0	\$0	\$0	\$0	\$0
	437.458	Contracted Services - Other	\$719	\$42	\$975	\$0	\$0	\$0	\$0	\$0
	437.460	Vehicle Maintenance	\$0	\$0	\$1,284	\$8,345	\$16,690	\$15,000	\$12,500	\$25,000
		<b>SUBTOTAL</b>	<b>\$9,968</b>	<b>\$18,816</b>	<b>\$10,084</b>	<b>\$8,345</b>	<b>\$16,690</b>	<b>\$15,000</b>	<b>\$12,500</b>	
Roads	438.250	Maintenance of Roads & B	\$6,603	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$6,603</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
Parks	454.246	Supplies: Repair/Maint	\$17,361	\$35,078	\$14,391	\$7,562	\$15,123	\$25,000	\$13,000	Tot Lot/OS/Be
	454.450	Contracted Services	\$3,800	\$500	\$865	\$0	\$0	\$1,500	\$0	
	454.458	Contracted Services - Mow	\$0	\$0	\$0	\$270	\$540	\$0	\$0	
	454.459	Lawn Mowing Equipment	\$43	\$30	\$0	\$0	\$1,000	\$750	\$500	\$1,000
	454.531	Community Pool Contrib.	\$26,944	\$27,776	\$30,000	\$0	\$30,000	\$30,000	\$30,000	Year 20 of 20
		<b>SUBTOTAL</b>	<b>\$48,148</b>	<b>\$63,384</b>	<b>\$45,256</b>	<b>\$7,832</b>	<b>\$46,663</b>	<b>\$57,250</b>	<b>\$43,500</b>	9/28/2005
Library	456.530	MontCo Library Contributi	\$3,290	\$3,291	\$0	\$0	\$3,290	\$3,290	\$3,290	
		<b>SUBTOTAL</b>	<b>\$3,290</b>	<b>\$3,291</b>	<b>\$0</b>	<b>\$0</b>	<b>\$3,290</b>	<b>\$3,290</b>	<b>\$3,290</b>	
Misc. Expe	480.000	Miscellaneous Expenditures	\$178,571	\$5,824	\$18,161	\$3,000	\$6,000	\$5,196	\$20,196	\$49,237
		<b>SUBTOTAL</b>	<b>\$178,571</b>	<b>\$5,824</b>	<b>\$18,161</b>	<b>\$3,000</b>	<b>\$6,000</b>	<b>\$5,196</b>	<b>\$20,196</b>	
<b>EMPLOYER PAID BENEFITS AND WITHHOLDING ITEMS</b>										
Withholdir	481.100	FICA Employer Tax	\$20,661	\$16,953	\$17,061	\$9,379	\$18,759	\$12,072	\$12,434	\$31,027
Items	481.200	Medicare Employer Tax	\$4,900	\$4,116	\$4,786	\$2,221	\$4,442	\$5,662	\$5,832	\$10,148
	481.300	Unemployment Comp Tax	\$1,710	\$2,163	\$1,994	\$1,425	\$2,850	\$4,196	\$4,322	\$7,950
		<b>SUBTOTAL</b>	<b>\$27,271</b>	<b>\$23,232</b>	<b>\$23,840</b>	<b>\$13,025</b>	<b>\$26,051</b>	<b>\$21,930</b>	<b>\$22,587</b>	



# GENERAL FUND EXPENSES

## FUND #01

Catagory	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
Pension	483.300	Pension Contribution DC	\$9,999	\$15,584	\$14,292	\$7,752	\$15,503	\$11,800	\$15,250	\$30,500
	483.301	Pension Contribution DB	\$72,072	\$24,484	\$29,248	\$0	\$0	\$18,300	\$33,550	\$67,100
	483.302	Pension Contribution Mgr	\$0	\$0	\$0	\$0	\$0	\$2,850	\$2,850	\$5,700
	483.319	Pension Investment Consult	\$2,350	\$3,000	\$2,700	\$0	\$0	\$2,500	\$2,500	\$5,000
		<b>SUBTOTAL</b>	<b>\$84,421</b>	<b>\$43,069</b>	<b>\$46,239</b>	<b>\$7,752</b>	<b>\$15,503</b>	<b>\$35,450</b>	<b>\$54,150</b>	
	484.000	Worker's Compensation	\$19,758	\$19,939	\$25,948	\$16,454	\$32,909	\$22,500	\$18,000	\$36,000
		<b>SUBTOTAL</b>	<b>\$19,758</b>	<b>\$19,939</b>	<b>\$25,948</b>	<b>\$16,454</b>	<b>\$32,909</b>	<b>\$22,500</b>	<b>\$18,000</b>	
Employee	485.152	Non-Union Life Insurance	\$317	\$317	\$328	\$0	\$0	\$750	\$750	\$1,500
Benefits	485.153	Union Disability Ins - STD	\$4,002	\$4,397	\$4,638	\$2,554	\$5,108	\$5,238	\$5,395	\$10,522
	485.182	Union Shoe Allowance	\$515	\$348	\$540	\$88	\$175	\$588	\$588	\$1,175
	485.183	Union Severance Fund	\$2,724	\$2,600	\$2,600	\$1,300	\$2,600	\$3,997	\$4,117	\$7,392
	485.184	Union Scholarship Fund	\$94	\$110	\$104	\$52	\$104	\$185	\$191	\$337
		<b>SUBTOTAL</b>	<b>\$7,652</b>	<b>\$7,772</b>	<b>\$8,210</b>	<b>\$3,994</b>	<b>\$7,987</b>	<b>\$10,758</b>	<b>\$11,040</b>	
<b>INSURANCE - CASUALTY &amp; EMPLOYEE HEALTH</b>										
Insurance	486.100	Property/Liability/Auto Ins	\$13,487	\$17,582	\$11,486	\$7,472	\$14,944	\$23,041	\$30,500	\$61,000
		<b>SUBTOTAL</b>	<b>\$13,487</b>	<b>\$17,582</b>	<b>\$11,486</b>	<b>\$7,472</b>	<b>\$14,944</b>	<b>\$23,041</b>	<b>\$30,500</b>	
Emp Bens	487.152	Dental Insurance	\$1,000	\$0	\$187	\$0	\$0	\$1,000	\$500	\$1,000
	487.156	Health Ins: Non-Union	\$7,500	\$6,977	\$5,769	\$3,000	\$6,000	\$5,000	\$5,000	\$10,000
	487.157	Union Health & Welfare Fu	\$82,956	\$93,037	\$101,635	\$55,448	\$110,896	\$105,000	\$104,000	\$208,000
		<b>SUBTOTAL</b>	<b>\$91,457</b>	<b>\$100,014</b>	<b>\$107,591</b>	<b>\$58,448</b>	<b>\$116,896</b>	<b>\$111,000</b>	<b>\$109,500</b>	
Intrafund	492.030	Transfer to Cap. Reserve	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
Transfer	492.070	Transfer to Electric Fund	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	492.080	Transfer to Sewer			\$0	\$25,000		\$50,000	\$0	



# GENERAL FUND EXPENSES

## FUND #01

Catagory	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
	492.180	Transfer to Capital Projects	\$0	\$0	\$138,000	\$1,281,000	\$562,000	\$140,000	\$125,000	
	492.030	Transfer to Fire Fund	\$0	\$0	\$0	\$0	\$12,000	\$0	\$0	
	492.031	Transfer to ARPA Fund		\$174,668	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$174,668</b>	<b>\$138,000</b>	<b>\$1,306,000</b>	<b>\$574,000</b>	<b>\$190,000</b>	<b>\$125,000</b>	
		<b>TOTAL EXENDITURES</b>	<b>\$1,917,948</b>	<b>\$2,063,287</b>	<b>\$2,042,040</b>	<b>\$2,344,599</b>	<b>\$2,617,157</b>	<b>\$2,244,780</b>	<b>\$2,226,931</b>	
		TOTAL REVENUES	\$3,136,846	\$3,276,774	\$2,682,808	\$2,551,789	\$2,187,470	\$2,222,514	\$2,244,710	
		TOTAL EXENDITURES	\$1,917,948	\$2,063,287	\$2,042,040	\$2,344,599	\$2,617,157	\$2,244,780	\$2,226,931	
		<b>FUND BALANCE</b>	<b>\$1,218,898</b>	<b>\$1,213,487</b>	<b>\$640,768</b>	<b>\$207,189</b>	<b>(\$429,687)</b>	<b>(\$22,266)</b>	<b>\$17,779</b>	

# FIRE PROTECTION FUND REVENUES

## FUND #03

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Notes
Real Estate Tax	301.100	Real Estate Tax: Current Yr	\$33,633	\$32,634	\$33,724	\$25,058	\$34,004	\$34,479	\$34,884	0.25 mils
		<b>SUBTOTAL</b>	<b>\$33,633</b>	<b>\$32,634</b>	<b>\$33,724</b>	<b>\$25,058</b>	<b>\$34,004</b>	<b>\$34,479</b>	<b>\$34,884</b>	
Shared Revenue										
	355.130	Foreign Fire Ins Prem Tax	\$16,091	\$20,361	\$20,508	\$0	\$20,508	\$20,361	\$20,508	
		<b>SUBTOTAL</b>	<b>\$16,091</b>	<b>\$20,361</b>	<b>\$20,508</b>	<b>\$0</b>	<b>\$20,508</b>	<b>\$20,361</b>	<b>\$20,508</b>	
Interfund Transfers										
	392.040	Transfer from Electric	\$0	\$0	\$0	\$0	\$26,361	\$26,361	\$5,853	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$26,361</b>	<b>\$26,361</b>	<b>\$5,853</b>	
<b>TOTAL REVENUES</b>			<b>\$49,724</b>	<b>\$52,995</b>	<b>\$54,232</b>	<b>\$25,058</b>	<b>\$80,873</b>	<b>\$81,201</b>	<b>\$61,245</b>	

# FIRE PROTECTION FUND EXPENSES

## FUNF #03

Category	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025
Fire	411.242	Real Estate Taxes	\$32,234	\$32,634	\$33,395	\$25,058	\$34,004	\$34,479	\$34,741
	411.352	Hatfield VFC Liability Ins	\$7,917	\$8,910	\$5,355	\$0	\$6,000	\$6,000	\$6,000
	411.550	Fire Relief Tax Fund	\$17,921	\$20,361	\$20,508	\$0	\$20,361	\$20,361	\$20,361
		<b>SUBTOTAL</b>	<b>\$58,072</b>	<b>\$61,904</b>	<b>\$59,258</b>	<b>\$25,058</b>	<b>\$60,365</b>	<b>\$60,840</b>	<b>\$61,102</b>
<b>TOTAL EXPENDITURES</b>			<b>\$58,072</b>	<b>\$61,904</b>	<b>\$59,258</b>	<b>\$25,058</b>	<b>\$60,365</b>	<b>\$60,840</b>	<b>\$61,102</b>
TOTAL REVENUES			\$49,724	\$52,995	\$54,232	\$25,058	\$80,873	\$81,201	\$61,245
TOTAL EXPENDITURES			\$58,072	\$61,904	\$59,258	\$25,058	\$60,365	\$60,840	\$61,102
<b>FUND BALANCE</b>			<b>(\$8,349)</b>	<b>(\$8,910)</b>	<b>(\$5,026)</b>	<b>\$0</b>	<b>\$20,508</b>	<b>\$20,361</b>	<b>\$144</b>

# ELECTRIC FUND REVENUES

## FUND #07

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Notes
Int. Income	341.000	Interest Income - Invest.	\$0	\$3,440	\$8,692	\$2,974	\$5,949	\$11,000	\$11,000	
	341.100	Interests Income-HSB S	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$3,440</b>	<b>\$8,692</b>	<b>\$2,974</b>	<b>\$5,949</b>	<b>\$11,000</b>	<b>\$11,000</b>	
Culture/	367.140	Pavillion Rental Fees	\$0	\$0	\$0	\$0	\$0	\$100	\$100	
Recreation	367.300	Founder's Day	\$0	\$1,650	\$3,250	\$0	\$0	\$0	\$0	
	367.301	Event Sponsorships	\$0	\$0	\$0	\$4,850	\$8,000	\$6,800	\$6,800	
	367.310	Movie Night	\$0	\$0	\$150	\$0	\$0	\$0	\$0	
	367.311	Fall Festival	\$0	\$4,750	\$1,300	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$6,400</b>	<b>\$4,700</b>	<b>\$4,850</b>	<b>\$8,000</b>	<b>\$6,900</b>	<b>\$6,900</b>	
Elect Syster	372.410	Metered Sales	\$3,652,229	\$3,788,929	\$3,754,477	\$1,821,283	\$3,642,566	\$4,010,340	\$4,056,965	.01 Cent Inc
	372.520	Misc. Service Revenues	\$3,050	\$2,705	\$2,625	\$1,150	\$2,300	\$3,350	\$3,350	
	372.530	Transfer Settlement Fees	\$1,650	\$1,075	\$2,275	\$550	\$1,100	\$1,000	\$1,000	
	372.600	Penalty Income	\$27,553	\$28,071	\$30,880	\$19,619	\$39,237	\$31,200	\$31,200	
		<b>SUBTOTAL</b>	<b>\$3,684,483</b>	<b>\$3,820,779</b>	<b>\$3,790,257</b>	<b>\$1,842,602</b>	<b>\$3,685,203</b>	<b>\$4,045,890</b>	<b>\$4,092,515</b>	
	398.000	Transfer from Invested F	\$0	\$0	\$8,765	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$8,765</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
Misc. Revs	380.000	Miscellaneous Revenues	\$272	\$557	\$304	\$9,271	\$18,542	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$272</b>	<b>\$557</b>	<b>\$304</b>	<b>\$9,271</b>	<b>\$18,542</b>	<b>\$0</b>	<b>\$0</b>	
<b>TOTAL REVENUES</b>			<b>\$3,684,755</b>	<b>\$3,831,176</b>	<b>\$3,812,718</b>	<b>\$1,859,697</b>	<b>\$3,717,695</b>	<b>\$4,063,790</b>	<b>\$4,110,415</b>	

# ELECTRIC FUND EXPENDITURES

## FUND #07

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Total of G-E-S
Governing	400.105	Council Mayor Comp	\$2,600	\$2,600	\$2,600	\$1,300	\$1,950	\$2,600	\$2,600	\$6,500
Body	400.199	Council Life Insurance	\$160	\$171	\$163	\$81	\$163	\$295	\$295	\$738
	400.420	Dues, Subscrips, Membe	\$636	\$1,763	\$1,097	\$714	\$1,428	\$800	\$600	\$1,500
	400.460	Conferences & Training	\$1,158	\$3,652	\$644	\$178	\$356	\$1,600	\$400	\$1,000
		<b>SUBTOTAL</b>	<b>\$4,553</b>	<b>\$8,187</b>	<b>\$4,504</b>	<b>\$2,274</b>	<b>\$3,897</b>	<b>\$5,295</b>	<b>\$3,895</b>	
Executive	401.110	Salary: Manager	\$60,489	\$38,845	\$41,554	\$20,754	\$41,508	\$40,768	\$41,612	\$104,030
	401.116	Administrative Assist	\$4,338	\$789	\$0		\$0	\$0	\$0	\$0
	401.187	General Compensation	\$770	\$705	\$605	\$0	\$0	\$600	\$600	\$1,500
	401.211	Newsletter Printing	\$1,746	\$3,452	\$6,496	\$2,271	\$4,542	\$4,800	\$5,200	\$13,000
	401.215	Newsletter Postage	\$0	\$400	\$0	\$0	\$0	\$2,400	\$0	\$0
	401.217	Special Newsletter/Mail	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	401.337	Automobile Allowance	\$0	\$0	\$0	\$3,000	\$6,000	\$2,400	\$2,400	\$6,000
	401.340	Advertising/Printing	\$5,243	\$6,455	\$2,911	\$2,465	\$4,930	\$7,200	\$6,400	\$16,000
	401.353	Bonding Fee	\$100	\$270	\$0	\$0	\$0	\$75	\$75	\$257
	401.420	Dues/Memberships	\$711	\$1,719	\$1,058	\$785	\$1,571	\$800	\$800	\$2,030
	401.460	Conferences/Training	\$1,594	\$1,160	\$1,161	\$466	\$932	\$2,165	\$2,000	\$5,000
		<b>SUBTOTAL</b>	<b>\$74,991</b>	<b>\$53,794</b>	<b>\$53,785</b>	<b>\$29,742</b>	<b>\$59,483</b>	<b>\$61,208</b>	<b>\$59,087</b>	
Fin Admin	402.112	Wages: Finance Crew	\$24,874	\$29,532	\$27,845	\$14,400	\$28,800	\$27,966	\$28,803	\$72,007
	402.180	Overtime Pay	\$1,431	\$2,194	\$1,871	\$911	\$1,823	\$944	\$200	\$1,679
	402.187	General Compensation	\$700	\$707	\$705	\$102	\$205	\$700	\$700	\$1,750
	402.210	Office Supplies	\$4,397	\$4,636	\$4,882	\$3,255	\$6,510	\$4,455	\$4,589	\$10,225
	402.215	Postage, Electric Bills	\$4,275	\$5,274	\$3,229	\$2,425	\$4,850	\$4,052	\$4,000	\$10,000
	402.238	Clothing & Uniforms	\$0	\$0	\$0	\$0	\$0	\$849	\$200	\$500
	402.310	Payroll Service Fees	\$1,558	\$1,539	\$1,798	\$766	\$1,532	\$4,838	\$4,983	\$7,706
	402.311	Auditing Services Fees	\$10,280	\$8,040	\$18,680	\$5,280	\$10,560	\$10,134	\$10,134	\$27,180
	402.312	Professional Services	\$0	\$2,800	\$2,885	\$525	\$1,050	\$2,000	\$2,000	\$5,152



# ELECTRIC FUND EXPENDITURES

## FUND #07

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Total of G-E-S
	402.321	Telephone Monthly Chrg	\$4,048	\$4,486	\$3,966	\$1,877	\$3,753	\$2,610	\$2,688	\$7,780
	402.331	Travel Mileage Reimbur	\$0	\$0	\$0	\$0	\$0	\$400	\$400	\$1,030
	402.353	Bonding Fee (Mgr & Tr	\$100	\$100	\$100	\$0	\$0	\$110	\$113	\$274
	402.374	Office Equip: Repair/Ma	\$0	\$0	\$0	\$0	\$0	\$694	\$715	\$1,079
	402.384	Office Equip: Lease	\$3,051	\$2,716	\$3,104	\$1,615	\$3,230	\$3,041	\$3,132	\$6,942
	402.420	Dues Subscrips Member	\$178	\$342	\$143	\$7	\$15	\$400	\$400	\$1,061
	402.460	Conferences & Training	\$0	\$85	\$327	\$40	\$80	\$600	\$600	\$1,546
	402.490	Allowance for Uncollect	\$0	\$0	\$0	\$0	\$0	\$5,000	\$1,000	
		<b>SUBTOTAL</b>	<b>\$54,891</b>	<b>\$62,452</b>	<b>\$69,536</b>	<b>\$31,203</b>	<b>\$62,407</b>	<b>\$68,793</b>	<b>\$64,657</b>	
Law	404.314	Legal Services	\$6,250	\$15,652	\$12,559	\$6,520	\$13,041	\$14,000	\$10,000	\$25,000
	404.317	Electric Dues	\$7,695	\$7,695	\$7,720	\$7,720	\$15,440	\$7,670	\$7,670	\$7,670
		<b>SUBTOTAL</b>	<b>\$13,945</b>	<b>\$23,347</b>	<b>\$20,279</b>	<b>\$14,240</b>	<b>\$28,481</b>	<b>\$21,670</b>	<b>\$17,670</b>	
Secretary/Clerk										
	405.112	Assistant to the Manager	\$33,192	\$21,282	\$20,997	\$12,927	\$25,854	\$22,497	\$23,172	\$57,930
	405.187	General Compensation	\$700	\$505	\$605	\$102	\$205	\$600	\$600	\$1,500
	405.331	Travel Mileage Reimbur	\$0	\$0	\$0	\$0	\$0	\$493	\$200	\$800
	405.420	Dues Subscrips Member	\$267	\$64	\$242	\$242	\$483	\$400	\$400	\$1,000
	405.460	Conferences & Training	\$704	\$0	\$64	\$0	\$0	\$800	\$400	\$1,000
		<b>SUBTOTAL</b>	<b>\$34,863</b>	<b>\$21,851</b>	<b>\$21,907</b>	<b>\$13,271</b>	<b>\$26,542</b>	<b>\$24,790</b>	<b>\$24,772</b>	
General Government Admin										
	406.112	Utility Billing/Admin	\$51,829	\$53,546	\$55,023	\$26,939	\$53,877	\$57,948	\$59,696	\$99,493
	406.180	Utility Billing/Admin O	\$273	\$131	\$168	\$114	\$228	\$344	\$355	\$1,037
	406.187	General Compensation	\$1,230	\$1,222	\$1,284	\$205	\$410	\$1,200	\$1,200	\$3,000
		<b>SUBTOTAL</b>	<b>\$53,332</b>	<b>\$54,900</b>	<b>\$56,476</b>	<b>\$27,257</b>	<b>\$54,515</b>	<b>\$59,492</b>	<b>\$61,251</b>	

# ELECTRIC FUND EXPENDITURES

## FUND #07

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Total of G-E-S
Process	407.241	Computer Software	\$0	\$0	\$23	\$23	\$47	\$2,460	\$800	\$2,000
	407.252	Computer Repair & Part	\$0	\$822	\$0	\$0	\$0	\$522	\$538	\$1,167
	407.310	Software Maint Fees	\$3,314	\$2,581	\$1,168	\$0	\$0	\$3,628	\$3,737	\$8,893
	407.325	Internet Fees	\$1,752	\$1,843	\$1,623	\$819	\$1,637	\$2,610	\$2,688	\$4,479
	407.329	Document Retention	\$0	\$4,020	\$2,961	\$2,961	\$5,923	\$6,000	\$1,200	\$3,000
	407.331	Cloud Services		\$0	\$0	\$0	\$0	\$1,920	\$0	\$0
	407.452	Contract IT Services	\$2,830	\$3,695	\$3,462	\$1,487	\$2,974	\$4,000	\$4,000	\$10,000
	407.453	Web Design/Maint	\$80	\$967	\$960	\$960	\$1,920	\$960	\$1,440	\$3,600
	407.750	Computer Equipment	\$549	\$5	\$1,234	\$1,234	\$2,469	\$800	\$800	\$2,000
		<b>SUBTOTAL</b>	<b>\$8,524</b>	<b>\$13,933</b>	<b>\$11,431</b>	<b>\$7,485</b>	<b>\$14,969</b>	<b>\$22,900</b>	<b>\$15,204</b>	
Engineer	408.313	Engineering Services	\$48,377	\$100,030	\$51,258	\$24,697	\$49,393	\$50,000	\$50,000	\$125,000
	408.450	Special Elect Engineering	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
		<b>SUBTOTAL</b>	<b>\$48,377</b>	<b>\$100,030</b>	<b>\$51,258</b>	<b>\$24,697</b>	<b>\$49,393</b>	<b>\$50,000</b>	<b>\$50,000</b>	
Bldgs &	409.366	Public Utility Services	\$737	\$964	\$1,044	\$692	\$1,384	\$800	\$800	\$3,000
	409.367	Waste Disposal Services	\$772	\$724	\$833	\$433	\$866	\$1,600	\$1,600	\$3,887
	409.370	Bldg/Repair Maint	\$11,175	\$8,616	\$15,561	\$10,077	\$20,154	\$10,000	\$9,000	\$22,500
	409.450	Contracted Services: Cle	\$2,812	\$2,200	\$2,225	\$784	\$1,568	\$2,400	\$2,400	\$7,030
		<b>SUBTOTAL</b>	<b>\$15,496</b>	<b>\$12,505</b>	<b>\$19,663</b>	<b>\$11,986</b>	<b>\$23,972</b>	<b>\$14,800</b>	<b>\$13,800</b>	
Electric	430.110	Salary: Public Works Di	\$35,717	\$28,900	\$27,893	\$13,905	\$27,811	\$29,095	\$29,968	\$74,920
Utility	430.111	General Compensation	\$800	\$707	\$705	\$102	\$205	\$700	\$700	\$1,750
	430.112	Wages: Maintenance Cr	\$68,245	\$76,082	\$77,717	\$39,484	\$78,968	\$77,700	\$78,732	\$197,280
	430.115	Wages: Summer Help	\$4,746	\$0	\$0	\$0	\$0	\$4,400	\$0	\$0
	430.180	OT Wages: Maintenanc	\$2,682	\$2,233	\$1,320	\$910	\$1,821	\$3,132	\$3,226	\$6,390
	430.220	Operating Supplies	\$4,215	\$3,032	\$2,792	\$1,330	\$2,660	\$3,200	\$3,200	\$8,000

# ELECTRIC FUND EXPENDITURES

## FUND #07

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Total of G-E-S
	430.222	Chemicals	\$110	\$277	\$120	\$0	\$0	\$522	\$538	\$1,284
	430.231	Operating Fuel	\$4,366	\$5,947	\$4,398	\$1,034	\$2,069	\$5,000	\$5,000	\$12,500
	430.245	Street Materials	\$1,682	\$2,688	\$2,278	\$1,129	\$2,258	\$3,247	\$3,344	\$8,438
	430.260	Small Tools/Minor Equi	\$1,682	\$1,954	\$347	\$155	\$311	\$1,773	\$1,826	\$4,500
	430.316	CDL Drug/Alcohol Test	\$237	\$175	\$113	\$113	\$225	\$335	\$514	\$1,285
	430.324	Cell Phones	\$2,211	\$1,958	\$2,207	\$1,063	\$2,126	\$1,338	\$1,378	\$3,987
	430.384	Equipment Rental	\$1,749	\$41	\$36	\$0	\$0	\$1,018	\$600	\$1,500
	430.386	Uniform	\$1,470	\$464	\$591	\$158	\$316	\$600	\$600	\$1,500
	430.420	Dues, Subscrips, Membe	\$24	\$14	\$78	\$64	\$128	\$200	\$200	\$500
	430.460	Conferences & Training	\$206	\$630	\$719	\$273	\$545	\$800	\$800	\$1,950
	430.740	Equipment Purchase	\$0	\$29,518	\$0	\$0	\$0	\$0	\$0	
	430.750	Materials Purchase	\$664	\$915	\$2,061	\$2,061	\$4,122	\$2,000	\$600	\$1,500
		<b>SUBTOTAL</b>	<b>\$130,805</b>	<b>\$155,536</b>	<b>\$123,374</b>	<b>\$61,782</b>	<b>\$123,563</b>	<b>\$135,060</b>	<b>\$131,226</b>	
Lighting	434.240	Bulbs & Fixtures	\$10,350	\$9,987	\$12,852	\$11,228	\$22,456	\$11,500	\$10,000	25 LED Cob
	434.450	Contracted Services - Re	\$880	\$1,863	\$2,151	\$357	\$714	\$2,500	\$2,000	Lights
		<b>SUBTOTAL</b>	<b>\$11,230</b>	<b>\$11,850</b>	<b>\$15,002</b>	<b>\$11,585</b>	<b>\$23,170</b>	<b>\$14,000</b>	<b>\$12,000</b>	
Repairs	437.241	Vehicle Tires	\$565	\$152	\$264	\$0	\$0	\$0	\$0	\$0
& Truck	437.251	Vehicle & Equipment Pa	\$4,481	\$3,295	\$2,300	\$0	\$0	\$0	\$0	\$0
	437.451	Contracted Services-Vel	\$2,352	\$11,208	\$3,659	\$0	\$0	\$0	\$0	\$0
	437.458	Contracted Services-Oth	\$575	\$33	\$780	\$0	\$0	\$0	\$0	\$0
	437.460	Vehicle Maintenance	\$0	\$0	\$1,027	\$6,676	\$13,352	\$12,400	\$10,000	\$25,000
		<b>SUBTOTAL</b>	<b>\$7,972</b>	<b>\$14,689</b>	<b>\$8,030</b>	<b>\$6,676</b>	<b>\$13,352</b>	<b>\$12,400</b>	<b>\$10,000</b>	
Power	442.361	Purchase of Electricity	\$1,584,468	\$2,238,697	\$2,005,587	\$954,321	\$1,908,642	\$1,728,986	\$2,257,130	
Purchase		<b>SUBTOTAL</b>	<b>\$1,584,468</b>	<b>\$2,238,697</b>	<b>\$2,005,587</b>	<b>\$954,321</b>	<b>\$1,908,642</b>	<b>\$1,728,986</b>	<b>\$2,257,130</b>	

# ELECTRIC FUND EXPENDITURES

## FUND #07

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Total of G-E-S
Power	442.320	Data Collection	\$17,085	\$36,875	\$44,370	\$30,321	\$60,643	\$37,500	\$37,500	AMI
Operations	442.430	PA Sales Tax	\$40,051	\$42,224	\$43,094	\$18,039	\$36,078	\$42,000	\$42,000	
	442.452	Contract Serv - System M	\$35,854	\$49,665	\$3,485	\$1,800	\$3,600	\$30,000	\$14,000	Tree Trim
	442.455	Contract Serv - Emerg. I	\$126,506	\$29,119	\$20,015	\$10,960	\$21,920	\$20,000	\$20,000	
	442.640	Capital construction	\$7,200	\$2,260	\$91,507	\$8,050	\$16,100	\$100,000	\$60,000	Pole Replace
	442.650	Install Section Fuses	\$0	\$0	\$41,072	\$0	\$0	\$0	\$0	
	442.740	Capital Equip Purchases	\$21,262	\$47,420	\$13,713	\$7,419	\$14,838	\$10,500	\$0	
		<b>SUBTOTAL</b>	<b>\$247,958</b>	<b>\$207,563</b>	<b>\$257,256</b>	<b>\$76,589</b>	<b>\$153,178</b>	<b>\$240,000</b>	<b>\$173,500</b>	
Culture &	451.500	Contributions	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
Recreation	451.501	HERC Contribution	\$0	\$0	\$0	\$0	\$0	\$5,000	\$5,000	
	451.502	Recognitions	\$0	\$0	\$175	\$0	\$0	\$1,000	\$1,000	
	453.239	Earth Day	\$0	\$100	\$0	\$0	\$0	\$0	\$0	
	453.450	Park Concerts	\$0	\$0	\$2,000	\$2,000	\$4,000	\$2,000	\$2,000	
	453.457	Movie Night	\$4,172	\$1,895	\$1,989	\$0	\$0	\$0	\$0	
	453.458	Founders Day Event	\$490	\$6,740	\$21,438	\$20,236	\$40,472	\$0	\$0	
	453.459	Fall Festival	\$3,635	\$8,908	\$5,495	\$0	\$0	\$0	\$0	
	453.460	Event Sponsorships	\$0	\$0	\$0	\$0	\$0	\$26,500	\$24,500	
	453.461	HMHS	\$10,000	\$0	\$0	\$0	\$0	\$6,000	\$3,000	
		<b>SUBTOTAL</b>	<b>\$18,297</b>	<b>\$17,642</b>	<b>\$31,097</b>	<b>\$22,236</b>	<b>\$44,472</b>	<b>\$40,500</b>	<b>\$35,500</b>	
Parks &	454.459	Lawn Mowing Equip	\$0	\$24	\$0	\$0	\$0	\$600	\$400	\$1,000
Rec			\$0	\$24	\$0	\$0	\$0	\$600	\$400	
Small Borrowing										
	472.400	Small Borrowing Repay	\$99,449	\$0	\$99,449	\$49,725	\$99,449	\$99,449	\$99,449	
			\$99,449	\$0	\$99,449	\$49,725	\$99,449	\$99,449	\$99,449	
Misc.	480.000	Miscellaneous Expenses	\$9,078	\$9,922	\$9,687	\$1,813	\$3,626	\$10,000	\$25,000	\$49,237



# ELECTRIC FUND EXPENDITURES

## FUND #07

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Total of G-E-S
Expenses		<b>SUBTOTAL</b>	<b>\$9,078</b>	<b>\$9,922</b>	<b>\$9,687</b>	<b>\$1,813</b>	<b>\$3,626</b>	<b>\$10,000</b>	<b>\$25,000</b>	
Withholdin	481.100	FICA Employer Tax	\$17,916	\$15,048	\$15,415	\$8,149	\$16,298	\$14,758	\$15,201	\$31,027
Items	481.200	Medicare Employer Tax	\$4,245	\$3,653	\$4,278	\$1,887	\$3,773	\$3,443	\$3,546	\$10,148
	481.300	Unemployment Comp T	\$1,368	\$1,731	\$1,595	\$1,140	\$2,280	\$2,952	\$3,041	\$7,950
		<b>SUBTOTAL</b>	<b>\$23,528</b>	<b>\$20,431</b>	<b>\$21,288</b>	<b>\$11,175</b>	<b>\$22,351</b>	<b>\$21,153</b>	<b>\$21,788</b>	
Pension	483.300	Pension Contribution DC	\$13,298	\$12,467	\$11,433	\$5,105	\$10,209	\$9,440	\$12,200	\$30,500
	483.301	Pension Contribution DI	\$57,658	\$19,587	\$23,398	\$0	\$0	\$14,640	\$26,840	\$67,100
	483.302	Pension Contribution M	\$0	\$0	\$0	\$0	\$0	\$2,280	\$2,280	\$5,700
	483.319	Pension Investment Con	\$1,880	\$2,400	\$2,160	\$2,160	\$4,320	\$2,000	\$2,000	\$5,000
		<b>SUBTOTAL</b>	<b>\$72,836</b>	<b>\$34,455</b>	<b>\$36,991</b>	<b>\$7,265</b>	<b>\$14,529</b>	<b>\$28,360</b>	<b>\$43,320</b>	
	484.000	Worker's Compensation	\$15,806	\$15,951	\$20,786	\$9,749	\$19,498	\$18,000	\$14,400	\$36,000
		<b>SUBTOTAL</b>	<b>\$15,806</b>	<b>\$15,951</b>	<b>\$20,786</b>	<b>\$9,749</b>	<b>\$19,498</b>	<b>\$18,000</b>	<b>\$14,400</b>	
Employee	485.152	Non Union/Union Life I	\$253	\$253	\$262	\$0	\$0	\$600	\$600	\$1,500
Benefits	485.153	Disability Ins - STD/LTI	\$3,202	\$3,517	\$3,710	\$1,855	\$3,710	\$4,052	\$4,173	\$10,522
	485.182	Union Shoe Allowance	\$412	\$278	\$432	\$84	\$169	\$470	\$470	\$1,175
	485.183	Union Serverance Fund	\$2,180	\$2,080	\$2,080	\$1,000	\$2,000	\$2,587	\$2,665	\$7,392
	485.184	Union Scholarship Fund	\$75	\$78	\$85	\$40	\$80	\$113	\$116	\$337
		<b>SUBTOTAL</b>	<b>\$6,122</b>	<b>\$6,207</b>	<b>\$6,570</b>	<b>\$2,980</b>	<b>\$5,959</b>	<b>\$7,822</b>	<b>\$8,024</b>	
Insurance	486.100	Property/Liability/Auto	\$10,790	\$14,065	\$9,189	\$5,007	\$10,015	\$12,020	\$24,400	\$61,000
Empl.	487.152	Non Union Dental Reim	\$800	\$0	\$150	\$0	\$1,697	\$800	\$400	\$1,000
	487.156	Non-Union Health Insur	\$3,750	\$3,754	\$2,885	\$1,500	\$3,000	\$4,000	\$4,000	\$10,000
	487.157	Health & Welfare Fund	\$66,365	\$74,430	\$81,308	\$40,654	\$81,308	\$84,000	\$83,200	\$208,000



# ELECTRIC FUND EXPENDITURES

## FUND #07

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Total of G-E-S
		<b>SUBTOTAL</b>	<b>\$81,705</b>	<b>\$92,249</b>		<b>\$47,162</b>	<b>\$96,020</b>	<b>\$100,820</b>	<b>\$112,000</b>	
Intrfrnd	492.010	Transfer to General Fund	\$1,280,000	\$1,245,000	\$1,350,000	\$405,000	820,000	\$975,000	\$750,000	
Transfer	492.018	Transfer to Fire Fund	\$0	\$0	\$0	\$0	0	\$26,361	\$5,853	
	492.022	Transfer to Sinking Fund	\$0	\$0	\$0	\$0	0	\$0	\$0	
	492.092	Transfer to HERC	\$0	\$5,000	\$5,000	\$5,000	0	\$0	\$0	
	492.220	Transfer to Cap Proj Sin	\$50,000	\$10,000	\$378,300	\$70,000	180,000	\$180,000	\$100,000	
	492.300	Transfer to Cap Res Fund		\$0	\$0			\$0	\$0	
	492.990	Transfer to Investments	\$0	\$0	\$0	\$0	0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$1,330,000</b>	<b>\$1,260,000</b>	<b>\$1,733,300</b>	<b>\$480,000</b>	<b>\$1,000,000</b>	<b>\$1,181,361</b>	<b>\$855,853</b>	
<b>TOTAL EXPENSES</b>			<b>\$3,948,227</b>	<b>\$4,436,214</b>	<b>\$4,677,258</b>	<b>\$1,855,486</b>	<b>\$3,670,963</b>	<b>\$3,967,460</b>	<b>\$4,109,925</b>	
TOTAL REVENUES			\$3,684,755	\$3,831,176	\$3,812,718	\$1,859,697	\$3,717,695	\$4,063,790	\$4,110,415	
TOTAL EXENDITURES			\$3,948,227	\$4,436,214	\$4,677,258	\$1,855,486	\$3,670,963	\$3,967,460	\$4,109,925	
<b>FUND BALANCE</b>			<b>(\$263,472)</b>	<b>(\$605,038)</b>	<b>(\$864,540)</b>	<b>\$4,211</b>	<b>\$46,732</b>	<b>\$96,330</b>	<b>\$490</b>	

# SEWER FUND REVENUES

## FUND #08

Category	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Notes
Interest Earn	341.000	Interest Income - Investment	\$0	\$1,134	\$4,340	\$1,553	\$3,106	\$3,000	\$3,000	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$1,134</b>	<b>\$4,340</b>	<b>\$1,553</b>	<b>\$3,106</b>	<b>\$3,000</b>	<b>\$3,000</b>	
	364.120	Sewer Use Charges	\$680,575	\$669,103	\$700,786	\$367,908	\$735,816	\$738,980	\$718,000	
	364.121	Penalty Income	\$8,453	\$7,835	\$8,139	\$5,944	\$11,887	\$5,000	\$5,000	
	364.122	Transfer Settlement Fees	\$1,525	\$1,025	\$775	\$575	\$1,150	\$500	\$500	
		<b>SUBTOTAL</b>	<b>\$690,552</b>	<b>\$677,964</b>	<b>\$709,699</b>	<b>\$374,427</b>	<b>\$748,854</b>	<b>\$744,480</b>	<b>\$723,500</b>	
Misc. Revenues	380.000	Miscellaneous Revenues	\$0	\$0	\$66,600	\$400	\$800	\$250	\$250	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$66,600</b>	<b>\$400</b>	<b>\$800</b>	<b>\$250</b>	<b>\$250</b>	
Intrfnd Tran	392.100	Transfer from General				\$25,000	\$50,000	\$50,000	\$0	
	392.310	Transfer from Sewer Mgd	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$25,000</b>	<b>\$50,000</b>	<b>\$50,000</b>	<b>\$0</b>	
	398.000	Transfer from Inv Funds	\$0	\$0	\$0	\$0	\$0	\$0	\$0	HTMA, HB
	398.001	Transfer of Interest	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	398.100	Transfer from Inv Sewer	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	398.200	Transfer from Sewer Res	\$0	\$0	\$0			\$0	\$107,550	EDU Fees 23 N M
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$107,550</b>	43 Roosevelt 1/3
<b>TOTAL REVENUES</b>			<b>\$690,552</b>	<b>\$679,098</b>	<b>\$780,639</b>	<b>\$401,380</b>	<b>\$802,760</b>	<b>\$797,730</b>	<b>\$834,300</b>	

# SEWER FUND EXPENDITURES

## FUND #08

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
	400.105	Council Mayor	\$650	\$650	\$650	\$325	\$650	\$650	\$650	\$6,500
	400.199	Council Life Insurance	\$40	\$43	\$41	\$20	\$41	\$95	\$95	\$738
	400.420	Dues/Memberships	\$159	\$441	\$274	\$179	\$357	\$200	\$150	\$1,500
	400.460	Conferences & Training	\$289	\$913	\$161	\$45	\$89	\$400	\$100	\$1,000
		<b>SUBTOTAL</b>	<b>\$1,138</b>	<b>\$2,047</b>	<b>\$1,126</b>	<b>\$568</b>	<b>\$1,137</b>	<b>\$1,345</b>	<b>\$995</b>	
Executive	401.110	Salary: Manager	\$15,221	\$9,908	\$10,388	\$5,188	\$10,377	\$10,192	\$10,403	\$104,030
	401.116	Administrative Assist.	\$1,085	\$0		\$0	\$0	\$0	\$0	\$0
	401.187	General Compensation	\$193	\$176	\$151	\$0	\$0	\$150	\$150	\$1,500
	401.211	Newsletter Printing	\$437	\$732	\$1,624	\$568	\$1,136	\$1,200	\$1,300	\$13,000
	401.215	Newsletter Postage	\$0	\$100	\$0	\$0	\$0	\$300	\$0	\$0
	401.337	Automobile Allowance	\$0	\$0	\$300	\$0	\$0	\$600	\$600	\$6,000
	401.340	Advertising/Legal	\$1,311	\$1,614	\$670	\$616	\$1,232	\$1,800	\$1,600	\$16,000
	401.353	Bonding Fee	\$25	\$68	\$0	\$0	\$0	\$31	\$32	\$257
	401.420	Due/Membership	\$178	\$430	\$265	\$196	\$393	\$200	\$200	\$2,030
	401.460	Mtgs/Confer/Training	\$398	\$290	\$278	\$117	\$233	\$591	\$500	\$5,000
		<b>SUBTOTAL</b>	<b>\$18,846</b>	<b>\$13,317</b>	<b>\$13,676</b>	<b>\$6,685</b>	<b>\$13,371</b>	<b>\$15,064</b>	<b>\$14,785</b>	
Fin Admin	402.112	Wages: Finance Crew	\$6,276	\$7,273	\$6,961	\$3,600	\$7,200	\$7,075	\$7,201	\$72,007
	402.180	Overtime Pay	\$358	\$550	\$468	\$228	\$456	\$245	\$253	\$1,679
	402.187	General Compensation	\$175	\$177	\$176	\$26	\$51	\$175	\$175	\$1,750
	402.210	Office Supplies	\$2,380	\$1,465	\$1,606	\$1,076	\$2,152	\$2,660	\$1,203	\$10,225
	402.215	Postage Sewer Bills	\$868	\$1,319	\$807	\$606	\$1,213	\$1,114	\$1,000	\$10,000
	402.238	Clothing & Uniforms	\$0	\$0	\$0	\$0	\$0	\$212	\$50	\$500
	402.310	Payroll Services Fees	\$359	\$385	\$450	\$192	\$383	\$494	\$509	\$7,706
	402.311	Auditing Fees	\$2,570	\$2,010	\$4,670	\$1,320	\$2,640	\$3,674	\$3,784	\$27,180
	402.312	Professional Services	\$0	\$0	\$670	\$131	\$263	\$500	\$500	\$5,152
	402.321	Telephone Monthly Chgs	\$1,012	\$700	\$946	\$469	\$938	\$887	\$913	\$7,780

# SEWER FUND EXPENDITURES

## FUND #08

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
	402.331	Travel Mileage Reimb	\$0	\$1,099	\$0	\$0	\$0	\$100	\$100	\$1,030
	402.353	Bonding Fees	\$25	\$25	\$25	\$0	\$0	\$27	\$28	\$274
	402.374	Office Equip: Repair/Mai	\$0	\$0	\$0	\$0	\$0	\$63	\$65	\$1,079
	402.384	Office Equip: Lease	\$763	\$679	\$776	\$404	\$807	\$760	\$783	\$6,942
	402.420	Dues, Subscrips, Member	\$43	\$62	\$36	\$2	\$4	\$131	\$131	\$1,061
	402.450	Shut Off Fees from NPW/	\$0	\$0	\$0	\$0	\$0	\$240	\$240	
	402.460	Conferences & Training	\$2	\$21	\$82	\$10	\$20	\$150	\$150	\$1,546
			<b>\$14,830</b>	<b>\$15,766</b>	<b>\$17,673</b>	<b>\$8,063</b>	<b>\$16,126</b>	<b>\$18,507</b>	<b>\$17,085</b>	
Law	404.314	Legal Services	\$1,299	\$3,810	\$2,663	\$1,211	\$2,422	\$3,500	\$2,500	\$25,000
	404.450	Special Legal Services	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$1,299</b>	<b>\$3,810</b>	<b>\$2,663</b>	<b>\$1,211</b>	<b>\$2,422</b>	<b>\$3,500</b>	<b>\$2,500</b>	
Secretary/Clerk										
	405.112	Assistant to the Manager	\$8,298	\$5,321	\$5,071	\$3,053	\$6,106	\$5,624	\$5,793	\$57,930
	405.187	General Compensation	\$175	\$126	\$151	\$26	\$51	\$150	\$150	\$1,500
	405.420	Dues Subscrips Member	\$67	\$16	\$60	\$60	\$121	\$100	\$100	\$1,000
	405.460	Conferences & Training	\$176	\$0	\$16	\$0	\$0	\$200	\$100	\$1,000
			<b>\$8,716</b>	<b>\$5,463</b>	<b>\$5,298</b>	<b>\$3,139</b>	<b>\$6,278</b>	<b>\$6,074</b>	<b>\$6,143</b>	
General Government Admin										
	406.112	Utility Billing/Admin	\$8,654	\$8,983	\$9,186	\$4,490	\$8,980	\$9,659	\$9,949	\$99,493
	406.180	Utility Billing/Admin OT	\$91	\$44	\$56	\$38	\$76	\$300	\$300	\$1,037
	406.187	General Compensation	\$308	\$305	\$227	\$51	\$102	\$300	\$300	\$3,000
			<b>\$9,053</b>	<b>\$9,333</b>	<b>\$9,470</b>	<b>\$4,579</b>	<b>\$9,158</b>	<b>\$10,259</b>	<b>\$10,549</b>	
	407.241	Computer Software	\$0	\$0	\$6	\$6	\$12	\$760	\$200	\$2,000
	407.252	Computer Repair/Parts	\$0	\$205	\$0	\$0	\$0	\$13	\$13	\$1,167
	407.310	Software Maint Fees	\$828	\$645	\$292	\$0	\$0	\$855	\$881	\$8,893
	407.325	Internet Fees	\$330	\$435	\$382	\$205	\$409	\$127	\$131	\$4,479

# SEWER FUND EXPENDITURES

## FUND #08

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
	407.329	Document Retention	\$0	\$1,005	\$740	\$740	\$0	\$1,500	\$300	\$3,000
	407.331	Cloud Services/Upgrade S	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
	407.452	Contract IT Services	\$707	\$924	\$810	\$372	\$743	\$1,000	\$1,000	\$10,000
	407.453	Web Design/Maint	\$20	\$242	\$240	\$240	\$480	\$240	\$360	\$3,600
	407.750	Computer Equipment	\$137	\$0	\$309	\$309	\$617	\$200	\$200	\$2,000
		<b>SUBTOTAL</b>	<b>\$2,023</b>	<b>\$3,456</b>	<b>\$2,778</b>	<b>\$1,871</b>	<b>\$2,262</b>	<b>\$4,695</b>	<b>\$3,085</b>	
Engineer	408.313	Engineering Services	\$12,094	\$24,993	\$11,867	\$6,174	\$12,348	\$12,500	\$12,500	\$125,000
	408.450	Special Eng Services			\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$12,094</b>	<b>\$24,993</b>	<b>\$11,867</b>	<b>\$6,174</b>	<b>\$12,348</b>	<b>\$12,500</b>	<b>\$12,500</b>	
Bldgs & Pla	409.366	Public Utility Services	\$184	\$241	\$238	\$173	\$346	\$200	\$200	\$3,000
	409.367	Waste Disposal Services	\$193	\$181	\$208	\$108	\$217	\$279	\$287	\$3,887
	409.370	Bldg Repair Maint.	\$2,794	\$2,154	\$3,890	\$2,519	\$5,039	\$2,500	\$2,250	\$22,500
	409.450	Contracted Services Clean	\$703	\$550	\$479	\$196	\$392	\$600	\$600	\$7,030
		<b>SUBTOTAL</b>	<b>\$3,874</b>	<b>\$3,126</b>	<b>\$4,815</b>	<b>\$2,996</b>	<b>\$5,993</b>	<b>\$3,579</b>	<b>\$3,337</b>	
Sanitary	429.360	Wastewater Treatment	\$433,562	\$460,391	\$439,146	\$279,233	\$535,356	\$602,061	\$620,123	
Treatment	429.361	Meter Electricty Charges	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	429.372	Lateral Repairs	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	429.373	Sewer Repairs	\$2,267	\$23,767	\$18,888	\$20,830	\$41,660	\$9,000	\$10,000	Sewer Repairs
	429.374	Sewer Interceptor	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	429.375	Sewer Replacements	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	429.454	Contracted Serv Sanitary	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$435,829</b>	<b>\$484,157</b>	<b>\$458,034</b>	<b>\$300,063</b>	<b>\$577,016</b>	<b>\$611,061</b>	<b>\$630,123</b>	
Sewer	430.110	Salary: Public Works Dir	\$9,004	\$7,225	\$6,973	\$3,476	\$6,953	\$7,274	\$7,492	\$74,920
Utility	430.111	General Compensation	\$200	\$177	\$176	\$26	\$51	\$175	\$175	\$1,750



# SEWER FUND EXPENDITURES

## FUND #08

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
	430.112	Wages: Maintenance Crew	\$17,061	\$19,020	\$19,429	\$9,871	\$19,742	\$19,092	\$19,758	\$197,280
	430.115	Wages: Summer Help	\$1,187	\$0	\$0	\$0	\$0	\$1,100	\$0	\$0
	430.180	OT Wages: Maintenance	\$0	\$0	\$0	\$0	\$0	\$490	\$504	\$6,390
	430.220	Operating Supplies	\$664	\$643	\$684	\$332	\$665	\$800	\$800	\$8,000
	430.222	Chemicals	\$27	\$69	\$30	\$0	\$0	\$127	\$131	\$1,284
	430.231	Operating Fuel	\$1,091	\$1,487	\$835	\$259	\$517	\$1,250	\$1,250	\$12,500
	430.245	Street Materials	\$421	\$672	\$570	\$282	\$564	\$887	\$913	\$8,438
	430.260	Small Tools/Minor Equip	\$762	\$488	\$58	\$11	\$21	\$507	\$522	\$4,500
	430.316	CDL Drug/Alcohol Testin	\$59	\$44	\$28	\$28	\$56	\$101	\$129	\$1,285
	430.324	Cell Phones	\$553	\$490	\$494	\$266	\$531	\$443	\$457	\$3,987
	430.384	Equipment Rental	\$37	\$10	\$9	\$0	\$0	\$255	\$150	\$1,500
	430.386	Uniform Allowance	\$446	\$116	\$148	\$40	\$79	\$150	\$150	\$1,500
	430.420	Dues, Subscrips, Member	\$6	\$4	\$19	\$16	\$32	\$50	\$50	\$500
	430.460	Conferences & Training	\$52	\$158	\$180	\$68	\$136	\$150	\$150	\$1,950
	430.740	Equipment Purchase	\$0	\$7,379	\$0	\$0	\$0	\$0	\$0	
	430.750	Materials Purchase	\$0	\$33	\$291	\$291	\$583	\$500	\$150	\$1,500
		<b>SUBTOTAL</b>	<b>\$31,570</b>	<b>\$38,015</b>	<b>\$29,926</b>	<b>\$14,966</b>	<b>\$29,931</b>	<b>\$33,351</b>	<b>\$32,780</b>	
Equipment	437.251	Vehicle Equipment Parts	\$1,120	\$824	\$621	\$109	\$219	\$0	\$0	\$0
& Truck	437.241	Vehicle Tires	\$141	\$38	\$66	\$22	\$43	\$0	\$0	\$0
Repair	437.451	Contracted Services Vehic	\$588	\$2,802	\$915	\$203	\$406	\$0	\$0	\$0
	437.458	Contracted Services - Othe	\$144	\$8	\$195	\$184	\$367	\$0	\$0	\$0
	437.460	Vehicle Maintenance	\$0	\$0	\$0	\$0	\$0	\$3,100	\$2,500	\$25,000
		<b>SUBTOTAL</b>	<b>\$1,993</b>	<b>\$3,672</b>	<b>\$1,797</b>	<b>\$517</b>	<b>\$1,035</b>	<b>\$3,100</b>	<b>\$2,500</b>	
Sewer	446.455	Storm Sewer Repairs	\$0	\$0	\$0	\$0	\$0	\$0	\$13,000	
Operations	446.456	MS4 Stormwater Mgmt Pr	\$9,907	\$19,225	\$4,205	\$2,205	\$4,410	\$10,000	\$6,000	

# SEWER FUND EXPENDITURES

## FUND #08

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
	446.620	Televising Mains			\$15,200			\$15,500	\$0	
	446.740	Capital Equip Purchases	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$9,907</b>	<b>\$19,225</b>	<b>\$19,405</b>	<b>\$2,205</b>	<b>\$4,410</b>	<b>\$25,500</b>	<b>\$19,000</b>	
Park &	454.459	Lawn Mowing Equip	\$0	\$6	\$0	\$0	\$0	\$200	\$100	\$1,000
Recs			\$0	\$6	\$0	\$0	\$0	\$200	\$100	
Misc. Exps	480.000	Miscellaneous Exp.	\$760	\$848	\$2,293	\$453	\$907	\$2,489	\$4,041	\$49,237
		<b>SUBTOTAL</b>	<b>\$760</b>	<b>\$848</b>	<b>\$2,293</b>	<b>\$453</b>	<b>\$907</b>	<b>\$2,489</b>	<b>\$4,041</b>	
Withholding	481.100	FICA Employer Tax	\$4,198	\$3,471	\$3,574	\$1,881	\$3,763	\$3,293	\$3,392	\$31,027
Items	481.200	Medicare Employer Tax	\$996	\$847	\$989	\$435	\$870	\$747	\$770	\$10,148
	481.300	Unemployment Comp Tax	\$342	\$433	\$399	\$285	\$570	\$570	\$587	\$7,950
		<b>SUBTOTAL</b>	<b>\$5,535</b>	<b>\$4,751</b>	<b>\$4,962</b>	<b>\$2,601</b>	<b>\$5,203</b>	<b>\$4,610</b>	<b>\$4,749</b>	
Pension	483.300	Pension Contribution DC	\$2,000	\$3,117	\$2,356	\$1,276	\$2,552	\$2,360	\$3,050	\$30,500
	483.301	Pension Contribution DB	\$14,414	\$4,897	\$5,850	\$0	\$0	\$3,660	\$6,710	\$67,100
	483.302	Pension Contribution Mgr	\$0	\$0	\$0	\$0	\$0	\$570	\$570	\$5,700
	483.319	Pension Investment Consu	\$470	\$600	\$540	\$540	\$1,080	\$500	\$500	\$5,000
		<b>SUBTOTAL</b>	<b>\$16,884</b>	<b>\$8,614</b>	<b>\$8,746</b>	<b>\$1,816</b>	<b>\$3,632</b>	<b>\$7,090</b>	<b>\$10,830</b>	
	484.000	Worker's Compensation	\$3,952	\$3,988	\$5,190	\$2,437	\$4,874	\$4,500	\$3,600	\$36,000
		<b>SUBTOTAL</b>	<b>\$3,952</b>	<b>\$3,988</b>	<b>\$5,190</b>	<b>\$2,437</b>	<b>\$4,874</b>	<b>\$4,500</b>	<b>\$3,600</b>	
Employee	485.152	Union Life Insurance	\$63	\$63	\$66	\$0	\$0	\$150	\$150	\$1,500
Benefits	485.153	Union Disability Ins - STD	\$800	\$879	\$927	\$464	\$927	\$926	\$954	\$10,522
	485.182	Union Shoe Allowance	\$103	\$70	\$108	\$21	\$42	\$118	\$118	\$1,175

# SEWER FUND EXPENDITURES

## FUND #08

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Total of G-E-S
	485.183	Union Severance Fund	\$545	\$520	\$520	\$250	\$500	\$593	\$611	\$7,392
	485.184	Union Scholarship Fund	\$19	\$19	\$19	\$10	\$20	\$29	\$30	\$337
		<b>SUBTOTAL</b>	<b>\$1,530</b>	<b>\$1,551</b>	<b>\$1,640</b>	<b>\$745</b>	<b>\$1,490</b>	<b>\$1,816</b>	<b>\$1,862</b>	
Insurance	486.100	Property/Liability/Auto In	\$2,697	\$3,516	\$2,297	\$1,252	\$2,504	\$4,889	\$6,100	\$61,000
		<b>SUBTOTAL</b>	<b>\$2,697</b>	<b>\$3,516</b>	<b>\$2,297</b>	<b>\$1,252</b>	<b>\$2,504</b>	<b>\$4,889</b>	<b>\$6,100</b>	
	487.152	Non-Union Dental Reimbu	\$200	\$0	\$38	\$0	\$400	\$200	\$100	\$1,000
	487.156	Non-Union Health Insuran	\$1,250	\$1,192	\$962	\$500	\$1,000	\$1,000	\$1,000	\$10,000
	487.157	Teamsters Health & Welfa	\$16,591	\$18,608	\$20,327	\$10,163	\$16,939	\$21,000	\$20,800	\$208,000
		<b>SUBTOTAL</b>	<b>\$18,041</b>	<b>\$19,800</b>	<b>\$21,326</b>	<b>\$10,663</b>	<b>\$18,339</b>	<b>\$22,200</b>	<b>\$21,900</b>	
Intrfnd Trar	492.180	Transfer to Capital Project	\$0	\$0	\$8,000	\$0	\$0	\$0	\$0	
Intrfnd	492.010	Transfer to General Fund	\$0	\$0	\$0	\$0	\$38,650			
	492.022	Transfer to CP Sinking	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	492.990	Transfer to Investments	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$8,000</b>	<b>\$0</b>	<b>\$38,650</b>	<b>\$0</b>	<b>\$0</b>	
<b>TOTAL EXPENDITURES</b>			<b>\$600,572</b>	<b>\$669,453</b>	<b>\$632,983</b>	<b>\$373,007</b>	<b>\$741,649</b>	<b>\$796,329</b>	<b>\$808,564</b>	
TOTAL REVENUES			\$690,552	\$679,098	\$780,639	\$401,380	\$802,760	\$797,730	\$834,300	
TOTAL EXPENDITURES			\$600,572	\$669,453	\$632,983	\$373,007	\$741,649	\$796,329	\$808,564	
<b>FUND BALANCE</b>			<b>\$89,980</b>	<b>\$9,644</b>	<b>\$147,656</b>	<b>\$28,373</b>	<b>\$61,111</b>	<b>\$1,401</b>	<b>\$25,736</b>	

# CAPITAL PROJECTS REVENUES

## FUND #18

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	
	351.032	Traffic Calming Feasibility	\$2,124	\$0	\$0	\$0	\$0	0	0	
		<b>SUBTOTAL</b>	<b>\$2,124</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
	<b>PA Community Transportation Init</b>									
	354.090	Parking Lot Improvement	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	354.091	Comp Planning	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	354.092	PA H20/PA SW&S Grant					\$1,093,333	\$2,805,000	\$2,900,000	
	354.093	EV Charging Station Grant					\$5,000	\$5,000	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,098,333</b>	<b>\$2,810,000</b>	<b>\$2,900,000</b>	
Misc. Reve	380.000	Miscellaneous Revenue	\$0	\$276,392	\$381	\$9	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$276,392</b>	<b>\$381</b>	<b>\$9</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
Intrfnd Tra	392.010	Transfer from General Fund	\$0	\$0	\$138,000	\$1,281,000	\$127,000	\$140,000	\$125,000	
	392.070	Transfer from Electric Fund	\$150,000	\$10,000	\$386,300	\$121,000	\$130,000	\$180,000	\$100,000	
	393.080	Transfer from Sewer Fund	\$0	\$0	\$8,000	\$0	\$0	\$0	\$0	
	393.100	Transfer from SCR Fund		\$0	\$0	\$0	\$5,800	\$0	\$0	
	392.300	Transfer from Cap. Res. Fd.	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	392.301	Transfer from Invested Fds	\$431,500	\$0	\$0	\$0	\$349,577	\$0	\$366,095	
	392.302	Transfer from Key Bank		\$137,990	\$0		\$366,261	\$0	\$0	
	392.301	Transfer from ARPA Funds	\$0		\$0	\$0	\$348,808	\$348,808	\$0	
Project Loan Repayment										
	393.100	Borrowing Loan	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$581,500</b>	<b>\$147,990</b>	<b>\$532,300</b>	<b>\$1,402,000</b>	<b>\$1,327,446</b>	<b>\$668,808</b>	<b>\$591,095</b>	
<b>TOTAL REVENUES</b>			<b>\$583,624</b>	<b>\$424,382</b>	<b>\$532,681</b>	<b>\$1,402,009</b>	<b>\$2,425,779</b>	<b>\$3,478,808</b>	<b>\$3,491,095</b>	

# CAPITAL PROJECTS EXPENDITURES

## FUND #18

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Notes
	409.373	Jail House Maintenance	\$0	\$0	\$0	\$0	\$0	\$2,500	\$0	
	409.375	PW Pole Barn/Fence	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	409.376	Rent/Renovations	\$3,194	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$3,194</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$2,500</b>	<b>\$0</b>	
<b>PUBLIC SAFETY</b>										
	415.327	Radios for trucks	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	415.328	EV Charging Station		\$0	\$0			\$10,000	\$0	
			<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$10,000</b>	<b>\$0</b>	
<b>TOTAL EXPENDITURES</b>										
	433.372	Battery Backups for signals	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	433.373	CMAQ Closed Loop Signals	\$358	\$0	\$0	\$0	\$0	\$0	\$0	
	433.374	ARLE Grant	\$0	\$64,986	\$0	\$0	\$0	\$0	\$0	
			<b>\$358</b>	<b>\$64,986</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
<b>CROSSWALKS</b>										
	435.372	Handicap Ramps -	\$9,400	\$41,172	\$0	\$0	\$0	\$0	\$0	
	435.373	Curb Ramps/Base Repair	\$0	\$0	\$76,546	\$0		\$68,000	\$0	
	435.374	Crosswalk Grant			\$0			\$0	\$0	
	435.375	CTP Crosswalk			\$0			\$0	\$0	
	435.376	MFT Pedestrian Crossing			\$0			\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$9,400</b>	<b>\$41,172</b>	<b>\$76,546</b>	<b>\$0</b>	<b>\$0</b>	<b>\$68,000</b>	<b>\$0</b>	
	436.372	Storm Sewer/Inlets Various	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
<b>ROAD MAINTENANCE</b>										
	438.372	Lincoln Ave Bridge	\$963,199	\$160,217	\$0	\$0	\$0	\$0	\$0	



# CAPITAL PROJECTS EXPENDITURES

## FUND #18

Category	Acct No.	Description	Acutal 2021	Acutal 2022	Acutal 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Notes
	438.373	Road Mill & Overlay	\$0	\$18,982	\$84,920	\$0	\$0	\$109,905	\$0	
	438.374	Towamencin Ave Reconst	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	438.377	Line Painting Main Roads		\$13,135	\$15,071	\$15,045	\$15,000	\$15,000	\$0	
		<b>SUBTOTAL</b>	<b>\$963,199</b>	<b>\$192,335</b>	<b>\$99,991</b>	<b>\$15,045</b>	<b>\$15,000</b>	<b>\$124,905</b>	<b>\$0</b>	
<b>SEWER MAINTENANCE</b>										
	442.750	DCED Grant	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	442.760	PA Small Water	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	442.770	CDBG Grant Sewer	\$12,097	\$359,770	\$0	\$0	\$0	\$0	\$0	
	442.080	PA H2O/PA SW&S	\$0	\$0	\$0	\$0	\$1,640,500	\$1,640,500	\$2,900,000	
			<b>\$12,097</b>	<b>\$359,770</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,640,500</b>	<b>\$2,900,000</b>	
<b>STORM WATER MGMT &amp; FLOOD CONTROL</b>										
	446.372	Stream cleanup	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	446.373	Inlet/manhole Repair	\$0	\$12,915	\$7,551	\$7,431	\$14,862	\$20,000	\$0	Various
	446.374	Edgewood/Towamen Culvert	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	446.375	Storm Sewer Replacement	\$19,280	\$15,008	\$0	\$0	\$0	\$0	\$0	
	446.376	Contracted Services	\$0	\$14,496	\$10,000	\$0	\$0	\$10,000	\$0	Concrete Work
		<b>SUBTOTAL</b>	<b>\$19,280</b>	<b>\$42,419</b>	<b>\$17,551</b>	<b>\$7,431</b>	<b>\$14,862</b>	<b>\$30,000</b>	<b>\$0</b>	
<b>GRANTS</b>										
	465.371	Small Comm Grants						\$0	\$0	
	465.691	Chestnut St. Trail	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	465.692	Crosswalk	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	465.693	PEC Grant - Feasibility Stud	\$2,000	\$0	\$0	\$0		\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$2,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
	466.610	Park Improvements	\$0	\$0	\$0	\$0	\$10,000	\$0	\$0	

# CAPITAL PROJECTS EXPENDITURES

## FUND #18

Category	Acct No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Notes
	466.611	Liberty Bell Trail Improv	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	466.710	Traffic Calming Implementa	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	466.711	Parking Lot Improvement	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	466.712	Comp Planning	\$0	\$0	\$0	\$0	\$0	\$12,000	\$11,250	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$10,000</b>	<b>\$12,000</b>	<b>\$11,250</b>	
Project Loan Repayment										
	471.100	Borrowing for Capital Purpo	\$255,180	\$477,631	\$0	\$49,725	\$574,849	\$537,874	\$560,000	
		<b>SUBTOTAL</b>	<b>\$255,180</b>	<b>\$477,631</b>	<b>\$0</b>	<b>\$49,725</b>	<b>\$574,849</b>	<b>\$537,874</b>	<b>\$560,000</b>	
<b>TOTAL EXPENDITURES</b>			<b>\$1,264,707</b>	<b>\$1,178,313</b>	<b>\$194,088</b>	<b>\$72,200</b>	<b>\$39,862</b>	<b>\$2,425,779</b>	<b>\$3,471,250</b>	
TOTAL REVENUES			\$583,624	\$424,382	\$532,681	\$1,402,009	\$2,425,779	\$1,332,446	\$3,491,095	
TOTAL EXPENDITURES			\$1,264,707	\$1,178,313	\$194,088	\$72,200	\$39,862	\$2,425,779	\$3,471,250	
<b>FUND BALANCE</b>			<b>(\$681,083)</b>	<b>(\$753,931)</b>	<b>\$338,592</b>	<b>\$1,329,809</b>	<b>\$2,385,917</b>	<b>(\$1,093,333)</b>	<b>\$19,845</b>	

# CAPITAL RESERVE FUND REVENUES

## FUND #30

Category	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Notes
Int Income	340.000	Loss on Investment	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	341.000	Interest Earnings-Investments	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	341.100	Interest Earnings-Conc.Acct.	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	341.101	Investment Interest	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
		Proceeds from loan	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
Interfund	392.010	Transfer from General Fund	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
Transfer	392.070	Transfer from Electric Fund						\$0	\$0	
	392.300	Transfer from Cap Res Fund								
	398.001	Transfer from Invested Fund	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
<b>TOTAL REVENUES</b>			<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	

# CAPITAL RESERVE FUND EXPENDITURES

## FUND #30

Category	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Projected Budget 2024	Projected Budget 2025	Notes
Bldgs & Plan	409.313	Eng/Architrcrtural Svcs	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	409.314	Furniture for Building	\$0	\$0	\$0	\$0		\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
TOTAL EXPEND	430.100	Street Equipment	\$0	\$0	\$14,527	\$0	\$0	\$0	\$0	
	430.700	Street Vehicles	\$0	\$0	\$0	\$0	0	\$0	\$0	
	430.720	Furniture	\$0	\$0	\$0	\$0	\$0			
	430.740	Backhoe Lease	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	430.741	Service Truck Lease	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$14,527</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
	431.740	Capital Purchase Equip	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
Parks	454.610	Electric Plant Park Improv	\$0	\$0	\$0	\$0	\$0	\$0	\$0	With Flagpoles
	454.611	Special Project	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	454.750	Street Decorations	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	454.751	Street Banners	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
Collectn/Trtmnt	429.455	Contracted Services	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
Intrfnd Trans	492.080	Transfer to Sewer Fund	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	492.220	Transfer to CP Sinking Fu	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
<b>TOTAL EXPENDITURES</b>			<b>\$0</b>	<b>\$0</b>	<b>\$14,527</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
TOTAL REVENUES			\$0	\$0	\$0	\$0	\$0	\$0	\$0	
TOTAL EXPENDITURES			\$0	\$0	\$14,527	\$0	\$0	\$0	\$0	
<b>FUND BALANCE</b>			<b>\$0</b>	<b>\$0</b>	<b>(\$14,527)</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	

# SEWER CAPITAL RESERVE FUND REVENUES

## FUND #31

Category	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Notes
Int Earnings	341.000	Interest Income-Investments	\$0	\$0	\$0	\$0	\$5,800	\$5,800	\$5,800	
	341.100	Interest Income-Repo. Acct.								
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$5,800</b>	<b>\$5,800.00</b>	<b>\$5,800.00</b>	
Sanitation	364.110	Sewage Connect/Tapping	\$0	\$0	\$0	\$0	\$0	\$77,700	\$215,100	23 N Main
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$77,700</b>	<b>\$215,100</b>	43 Roosevelt
										1/2 EDU
	398.000	Transfer from Investment	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
<b>TOTAL REVENUES</b>			<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$5,800</b>	<b>\$83,500</b>	<b>\$220,900</b>	



# SEWER CAPITAL RESERVE EXPENDITURES

## FUND #31

Category	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Notes
Transfer	492.080	Transfer to Sinking Fund	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
	492.200	Transfer to Cap Proj Sink			\$0		\$5,800	\$5,800	\$0	
	492.990	Transfer to Sewer Fund	\$0	\$0	\$0	\$0	\$77,700	\$77,700	\$107,550	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$83,500</b>	<b>\$83,500</b>	<b>\$107,550</b>	
			<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$83,500</b>	<b>\$83,500</b>	<b>\$107,550</b>	
TOTAL REVENUES			\$0	\$5,800	\$5,800	\$0	\$5,800	\$83,500	\$220,900	
TOTAL EXPENSES			\$0	\$0	\$0	\$0	\$83,500	\$83,500	\$107,550	
<b>FUND BALANCE</b>			<b>\$0</b>	<b>\$5,800</b>	<b>\$5,800</b>	<b>\$0</b>	<b>(\$77,700)</b>	<b>\$0</b>	<b>\$113,350</b>	

# LIQUID FUELS FUND REVENUES

## FUND #35

Category	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Notes
Int Income	341.100	Interest Earnings	\$149	\$603	\$2,238	\$2,015	\$4,031	\$100	\$100	
		<b>SUBTOTAL</b>	<b>\$149</b>	<b>\$603</b>	<b>\$2,238</b>	<b>\$2,015</b>	<b>\$4,031</b>	<b>\$100.00</b>	<b>\$100.00</b>	
State Shared	355.050	State Aid-Municipal Liquid Fuel	\$79,870	\$83,476	\$85,330	\$85,508	\$85,330	\$84,323	\$83,824	
Revenue		<b>SUBTOTAL</b>	<b>\$79,870</b>	<b>\$83,476</b>	<b>\$85,330</b>	<b>\$85,508</b>	<b>\$85,330</b>	<b>\$84,323</b>	<b>\$83,824</b>	
<b>TOTAL REVENUES</b>			<b>\$80,019</b>	<b>\$84,079</b>	<b>\$87,568</b>	<b>\$87,524</b>	<b>\$89,361</b>	<b>\$84,423</b>	<b>\$83,924</b>	
*** On Summary Sheet \$102,000 was added to the revenues for the carry over from 2023 actual bank balance										

# LIQUID FUELS FUND EXPENDITURES

## FUND #35

Category	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Notes
Equipment	430.260	Minor Equipment Purchase								
	430.740	Major Equipment Purch	\$14,527	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$14,527</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
Streets	431.371	Cleaning Streets & Gutt	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
Snow/Ice	432.220	Operating Supplies: Sal	\$10,347	\$6,521	\$0	\$0	\$20,000	\$20,000	\$15,000	
		<b>SUBTOTAL</b>	<b>\$10,347</b>	<b>\$6,521</b>	<b>\$0</b>	<b>\$0</b>	<b>\$20,000</b>	<b>\$20,000</b>	<b>\$15,000</b>	
Signs &	433.240	Street Signs & Posts	\$0	\$0	\$0	\$0	\$1,126	\$1,126	\$0	
Traffic	433.241	Signal Supplies/Repairs	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
Control		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,126</b>	<b>\$1,126</b>	<b>\$0</b>	
Street Lighting	434.370	Street Lights	\$0	\$0	\$0	\$0	\$562	\$562	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$562</b>	<b>\$562</b>	<b>\$0</b>	
Storm Sewers &	436.370	Sewer Maintenance	\$0	\$0	\$0	\$0	\$562	\$562	\$0	
Drains		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$562</b>	<b>\$562</b>	<b>\$0</b>	
Repairs of Tools &	437.260	Maintenance of Tools	\$0	\$0	\$0	\$0	\$562	\$562	\$0	
Machinery		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$562</b>	<b>\$562</b>	<b>\$0</b>	
St Maint/	438.245	Maint/Repair Supplies	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
Repair		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
Highway	439.600	Capital Construction	\$195,304	\$183,194	\$10,248	\$0	\$0	\$0	\$100,400	N Main Mill &
		<b>SUBTOTAL</b>	<b>\$195,304</b>	<b>\$183,194</b>	<b>\$10,248</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$100,400</b>	Overlay W Broad

# LIQUID FUELS FUND EXPENDITURES

## FUND #35

Category	Acct. No.	Description	Actual 2021	Actual 2022	Actual 2023	YTD 2024 6/30/24	Projected thru 12/31	Budget 2024	Projected Budget 2025	Notes
Miscellaneous	480.000	Miscellaneous	\$0	\$0	\$0	\$0	\$0	\$0	\$0	
		<b>SUBTOTAL</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	
<b>TOTAL EXPENDITURES</b>			<b>\$220,177</b>	<b>\$189,715</b>	<b>\$10,248</b>	<b>\$0</b>	<b>\$22,812</b>	<b>\$22,812</b>	<b>\$115,400</b>	
TOTAL REVENUES			\$80,019	\$84,079	\$87,568	\$87,524	\$89,361	\$84,423	\$83,924	
TOTAL EXENDITURES			\$220,177	\$189,715	\$10,248	\$0	\$22,812	\$22,812	\$115,400	
<b>FUND BALANCE</b>			<b>(\$140,158)</b>	<b>(\$105,637)</b>	<b>\$77,321</b>	<b>\$87,524</b>	<b>\$66,549</b>	<b>\$61,611</b>	<b>(\$31,476)</b>	

**6. NEW BUSINESS / DISCUSSION ITEMS:**

**B. Ordinance No. 557 Rental Inspection  
Program**



**DRAFT**

**HATFIELD BOROUGH  
MONTGOMERY COUNTY, PA**

**ORDINANCE NO. \_\_\_\_\_**

**AN ORDINANCE AMENDING THE HATFIELD BOROUGH CODE OF ORDINANCE  
CHAPTER 5 CODE ENFORCEMENT TO ADD A NEW PART 4 RESIDENTIAL  
RENTAL PROPERTY INSPECTION PROGRAM AND ADDING THE FOLLOWING  
SECTIONS TO CHAPTER 5 OF THE BOROUGH CODE OF ORDINANCES**

**Section 1.** The following sections are added to the Hatfield Borough Code of ordinances, Chapter 5, Code Enforcement.

**§4- 101 Purpose.**

1. The purpose of this Part and the policy of the Borough of Hatfield shall be to protect and promote the public health, safety and welfare of its citizens, to establish rights and obligations of owners and occupants relating to residential rental units in the Borough and to encourage owners and occupants to maintain and improve the quality of life and quality of rental housing within the community. As a means to these ends, this Part provides for a systematic inspection program, along with the registration requirements of Borough Code § 5-208 for Owners of Residential Rental Properties and All Nonresidentially Used Properties and licensing of residential rental units and penalties.

2. In considering the adoption of this Part, the Borough makes the following findings:

A. There is a growing concern in the community with the appearance and physical condition of many residential rental units.

B. There is a perception and appearance of greater incidence of problems with the maintenance and upkeep of residential properties which are not owner occupied as compared to those that are owner occupied.

C. The Borough recognizes a responsibility to tenants who occupy buildings owned by others responsible for maintenance and upkeep.

**§ 4-102 Annual License Registration.**

1. In accordance with §5-208, each year, every owner, operator, responsible agent, or manager of a residential rental unit located in the Borough of Hatfield shall register the property and apply for a residential rental unit license for each residential rental unit owned by such owner from the Borough Code Enforcement Officer. At the same time,

the owner shall complete a license registration for each residential rental unit and shall pay an annual license registration fee as set forth herein. The license shall be valid and operative during the calendar year in which it is issued.

2. Forms for such license and registration shall be provided by the Borough Code Enforcement Officer. The registration form shall list the name, address and telephone number of the residential unit owner, Designated Contact Person, (An individual residing within 10 miles of the Borough who is responsible for property management when the owner resides outside of this distance) an address for service of notices for inspection and/or violations of this ordinance, the location of the unit, the name, address and telephone number of the manager or rental agent (if applicable), the number of rental units in the residential rental property (if applicable) and the names of the respective tenants.
3. Failure to register a residential rental unit and obtain a license for each residential rental unit from the Code Enforcement Officer annually as required by this section, shall constitute a violation of this ordinance.

#### **§ 4-103. License and Registration Fees.**

An annual license and registration fee for each rental unit in an amount established by resolution of Borough Council shall be timely paid and renewed as established in fee schedule. The license registration fee shall be the sole responsibility of the property owner. Late registrations shall be subject to a monthly late fee as established by resolution of Borough Council.

#### **§ 4-104 No Less Than Triennial Inspections Required.**

1. All residential rental units shall be inspected to assure compliance with the minimum maintenance requirements and standards for such properties as set forth in this ordinance. Such inspection shall occur when the unit first becomes a registered residential rental unit and no less than once every three years thereafter, or, if sooner, at such time as the property undergoes a change of ownership or change in tenant or more often if the Borough needs to ensure compliance with the International Property Maintenance Code and its own maintenance requirements as detailed herein.
2. Residential rental unit” means an apartment, a rooming unit or a dwelling unit of any kind which is leased, or held out or otherwise available for lease, for living and sleeping purposes or any and all other residential units not owner-occupied as a primary residence by the owner of record. A residential rental unit shall include residential properties under lease purchase agreements.

3. "Change of ownership" means the transfer of legal or equitable title to the unit or property by deed or other written instrument, whether or not recorded of record. The definition also includes an agreement of sale that provides for the transfer of title after a certain number of installment payments by the tenant.
4. The minimum maintenance requirements and standards for residential rental units shall meet the provisions of the current Borough adopted International Property Maintenance Code and the following requirements:

(1) Exterior:

- (a) Gutters and downspouts.
- (b) Sidewalks (no trip hazards or broken curbs).
- (c) No broken windows.
- (d) Interior and exterior guardrails for stairs and porches over 30 inches above ground.
  - [1] Guardrails shall have balusters spaced such that a four-inch sphere cannot pass through.
  - [2] Guardrails must be 36 inches high on open porches and 34 inches high on stairs.
- (e) No tall grass and weeds.
- (f) No accumulation of trash.
- (g) Swimming pools.
  - [1] Aboveground pools must have four-foot-high approved barrier.
  - [2] In-ground pools must have at least a four-foot-high fence or approved barrier with a self-closing, self-latching and locking gate with a maximum fence/gate spacing of four inches.

(2) Electrical:

- (a) Proper grounding of panel box and outlets.
- (b) No exposed or dangerous wiring.
- (c) Missing covers on receptacle, switches, and junction boxes.
- (d) Labeling of breakers at panel.
- (e) Open slots at panel box (sealed or capped).
- (f) Switched lighting at stairways, top and bottom, except basements.
- (g) Receptacles in all habitable rooms.
- (h) Vent above stove.
- (i) All 125-volt, single-phase, 15- and 20-ampere receptacles installed in bathrooms shall have ground-fault circuit-interrupter protection for personnel.
- (j) All 125-volt, single-phase, 15- or 20-ampere receptacles installed in garages and grade-level portions of unfinished accessory buildings used for

storage or work areas shall have ground-fault circuit-interrupter protection for personnel.

[1] Exceptions:

[a] Receptacles that are not readily accessible.

[b] A single receptacle or a duplex receptacle for two appliances located within dedicated space for each appliance that in normal use is not easily moved from one place to another and that is cord- and plug-connected.

(k) All 125-volt, single-phase, 15- and 20-ampere receptacles installed outdoors shall have ground-fault circuit-interrupter protection for personnel.

(l) Where a crawl space is at or below grade level, all 125-volt, single-phase, 15- and 20-ampere receptacles installed in such spaces shall have ground-fault circuit-interrupter protection for personnel.

(m) All 125-volt, single-phase, 15- and 20-ampere receptacles installed in unfinished basements shall have ground-fault circuit-interrupter protection for personnel. For purposes of this section, "unfinished basements" are defined as portions or areas of the basement not intended as habitable rooms and limited to storage areas, work areas, and the like.

[1] Exceptions:

[a] Receptacles that are not readily accessible.

[b] A single receptacle or duplex receptacle for two appliances located within dedicated space for each appliance that in normal use is not easily moved from one place to another and that is cord- and plug-connected.

(n) All 125-volt, single-phase, 15- and 20-ampere receptacles that serve countertop surfaces shall have ground-fault circuit-interrupter protection for personnel.

(o) All 125-volt, single-phase, 15- and 20-ampere receptacles that serve a countertop surface and are located within six feet (1,829 mm) of the outside edge of a wet-bar sink shall have ground-fault circuit-interrupter protection for personnel. Receptacle outlets shall not be installed in a face-up position in the work surfaces or countertops.

(p) The outlet(s) that supplies a self-contained spa or hot tub, or a packaged spa or hot tub equipment assembly, or a field-assembled spa or hot tub with a heater load of 50 amperes or less, shall be protected by a ground-fault circuit-interrupter.

(q) All pools shall have ground-fault protection.

(r) Notwithstanding the above, when the existing wiring is incompatible with installation of ground-fault circuit-interruption outlets, the existing

wiring shall be certified with a notation on the certificate that substandard wiring is present.

(3) Fire protection and safety:

(a) Smoke detectors.

[1] Battery type is proper in existing structures.

[2] Located in basement.

[3] Located outside bedrooms vicinities and one in each bedroom and on each level of the unit including basements.

(b) Fire separation between building and garage (i.e., one-half-inch drywall).

(c) Egress from all bedrooms (window size requirements should not apply here; check window operation).

(d) Thumb latch dead bolts for doors where installed (keyed type not permitted).

(e) Continuous railing system at all staircases in house.

(4) Plumbing and heating:

(a) Drip let on water heater relief valve (maximum of six inches from floor with one-inch air gap off floor, rigid pipe only).

(b) Fix leaks in plumbing at faucets and taps.

(c) Properly functioning toilets.

(d) Heat to all habitable rooms.

(e) Proper ventilation for bathrooms (window or fan).

(f) Proper ventilation for dryers:

[1] Flexible plastic duct shall not be concealed in walls.

[2] Must vent directly to outside.

(g) Gutters, downspouts, sump pumps, floor drains, or any other sources of stormwater inflow and infiltration may not be connected to the public sewer system.

5. The registration and licensing provisions of this ordinance shall not apply to rental units which offer or provide medical or nursing services, including, without limitation, hospitals, nursing homes, assisted living homes and group homes, or other rental units used for human habitation which offer or provide medical or nursing services, and wherein all operations of such facilities are subject to county, state or federal licensing or regulations concerning the health and safety of the users, patients or tenants. The registration and licensing provisions of this ordinance also shall not apply to a fraternity or hotel or motel units.



6. Failure and/or refusal by the residential rental unit owner or designated agent to provide access for inspection upon reasonable notice shall be deemed a violation of this ordinance.
7. Fees for inspection and reinspection shall be in accordance with the current adopted fee schedule. The Borough Council may, from time to time, by duly adopted resolution, modify the amount of the re-inspection fee.

#### **§ 4-105 Liability of Borough.**

1. By conducting the inspections pursuant to this ordinance the Borough does not warrant or guarantee the complete safety or suitability of residential rental units.
2. For all inspections conducted pursuant to this ordinance the Code Enforcement Officer shall not be responsible for violations that occur between the inspection and rental period.

#### **§ 4-106 Violations and Penalties.**

1. It shall be a violation of this Part 4 for any person or entity to own or operate a residential rental unit in the Borough of Hatfield Borough without completing an annual license registration and obtaining a satisfactory inspection of the unit as provided in this ordinance. All violations shall be issued by the Code Enforcement Officer on a per-unit basis, and each unit in violation of this ordinance shall constitute a separate violation.
2. Any person or entity who violates this ordinance shall, upon conviction, be sentenced to pay a fine of \$300 plus costs of prosecution and reimbursement of any attorney fees expended by the Borough. However, upon receipt of a citation a person or entity who violates this section may, within five business days of the time when such citation was received, apply in full for a rental unit license and/or make arrangements for an inspection and pay a \$50 fine via cash, check or money order to the Borough of Hatfield Borough, at the Hatfield Borough, borough hall during normal Borough business hours, as a penalty for and as full satisfaction of such violation, in which event no prosecution in the manner set forth in this Section shall be brought unless further violations ensue. In the event more than five business days have elapsed without full satisfaction of such violation paid to the Borough of Hatfield, the person or entity in violation shall have up to 14 business days after the date of the citation to apply in full for a rental unit license and/or make arrangements for a triennial inspection and pay a fine in the amount of \$75 via cash, check or money order to the Borough of Hatfield, at the Hatfield

Borough Hall during normal Borough business hours, as a penalty for and as full satisfaction of a such violation, in which event no prosecution in the manner set forth in this Section shall be brought unless further violations ensue. Thereafter, the citation shall be turned over to the court for prosecution of the violation. Owner/operator shall be responsible for court costs and reimbursement of any attorney's fees expended by Borough plus the applicable fine if found guilty upon conviction.

3. Each day that a violation continues after due notice has been served shall be deemed a separate offense.
4. **The Borough Can Make Repairs.** In case the owner of premises shall neglect, fail or refuse to comply with any notice from the Borough or its Code Enforcement Officer to correct a violation relating to maintenance and repair of the premises under any code within the period of time stated in such notice, the Borough may, but is not required to, cause the violation to be corrected. There shall be imposed upon the owner a charge of the actual costs involved, plus 10% of said costs for each time the Borough shall cause a violation to be corrected and the owner of the premises shall be billed after same has been completed. Any such bill which remains unpaid and outstanding after the time specified therein for payment shall be grounds for the imposition of a municipal lien upon the premises as provided by law. Such a lien may be reduced to judgment and enforced and collected as provided by law, together with interest at the legal rate and court costs. The remedies provided by this subsection are not exclusive and the Town and its Code Enforcement Officer may invoke such other remedies available under this Part or the applicable codes, ordinances or statutes, including, where appropriate, condemnation proceedings or declaration of premises as unfit for habitation; or suspension, revocation or nonrenewal of the license issued hereunder.

#### **§4-107 Additional Remedies.**

1. The penalties and remedies set forth in this ordinance shall not be exclusive, and the Borough of Hatfield shall have the right to avail itself of any other remedy at law or in equity which it may deem to be appropriate.

**Section 2.     Severability.** The provisions of this Ordinance are severable. If any part of this Ordinance is declared to be un-constitutional, illegal, or invalid, the validity of the remaining provisions shall be unaffected.

**Section 3.     Repealer.** All Ordinances and parts of Ordinances heretofore adopted, to the extent that the same are inconsistent herewith, are hereby repealed, while all Ordinances not inconsistent herewith remain valid and in force.

**Section 4. Effective Date.**  
January 1, 2025.

The effective date of this Ordinance shall be

**ORDAINED AND ENACTED** by the Borough Council of HATFIELD BOROUGH on  
this \_\_\_\_\_ day of \_\_\_\_\_, 2024 with \_\_\_\_\_ Council Members  
\_\_\_\_\_ voting “aye” and \_\_\_\_\_ voting “nay.”

Attest:

**HATFIELD BOROUGH**

\_\_\_\_\_  
Jaime E. Snyder, Manager, Borough Secretary

By: \_\_\_\_\_  
Jason Ferguson, Council President

Approved this \_\_\_\_\_ day of \_\_\_\_\_, 2024.

\_\_\_\_\_  
Mayor Mary Anne Girard

## **6. NEW BUSINESS / DISCUSSION ITEMS:**

### **C. Updating the Consolidated Fee Schedule**

**DRAFT**

**BOROUGH OF HATFIELD  
RESOLUTION NO. 2024-  
CONSOLIDATED FEE SCHEDULE**

**WHEREAS**, the Borough of Hatfield charges fees for various services provided and applications made to the Borough; and

**WHEREAS**, Borough Council wishes to adopt a consolidated schedule of fees for these services.

**NOW THEREFORE, BE IT RESOLVED** by the Borough Council of the Borough of Hatfield that the following consolidated schedule of fees is adopted, reflecting the fees as of the adoption of this resolution.

**TABLE OF CONTENTS**

1. Amusement Devices
2. Building and Construction Inspections and Permits
3. Plumbing Licenses and Permits
4. Mechanical Permits
5. Electrician Licenses and Permits
6. Electric Utility
7. Sewer Utility
8. Highway Occupancy/Street Opening Permits
9. Fire Prevention Inspections and Permits
10. Property Registrations and Inspections
11. Residential Property Transfer Certification
12. Building Certifications
13. Subdivision and Land Development
14. Conditional Use Hearings
15. Zoning Permits and Zoning Application and Hearings
16. Core Commercial District Parking
17. False Alarm
18. Copying Fees
19. Document Fees
20. Sale of Fireworks
21. Emergency Call Out
22. Other Miscellaneous Fees
23. Exemptions
24. Legal Fees



**FEES**

1. **Amusement Devices (Ch. 13, §103)**

License Fee: \$100.00 per machine, annually

2. **Building and Construction Inspections and Permits (Ch. 5, Part 1, §108; Ch. 8, Part 2, §209)**

Residential:

New Construction  
(incl. additions outside existing structure) \$400.00 plus \$.25 per square foot

Apartment/Condo/Townhouse  
(in common deed) \$400.00 plus \$.25 per square foot

Alterations/Remodeling \$200.00 plus \$.25 per square foot

**Alterations and/or Renovations where floor area does not apply:**

Alterations, renovations or modifications of existing buildings or structures where floor area does not apply (i.e. doors, windows, roofs, structural openings or beams, etc.); \$200 plus \$34 for each \$1,000 of estimated construction value of alterations, renovations or modification certified by the permit applicant

**Accessory Structures over 200 square feet: \$140 plus 30¢ per sq/ft of area**

Utility Shed

200 square feet or less \$50.00

Replacement of existing utility shed within  
existing footprint \$50.00

Deck \$140.00 plus \$.30 per square foot

Fence \$100.00 plus \$.25 per linear foot

Home Occupation Permit \$100.00 (one-time fee)

Driveway Permit \$200.00 plus \$.25 per square foot

Roofing \$140.00 per structure

Non-residential:

New Construction (incl. additions outside existing structure)	\$400.00 plus \$.30 per square foot
Alterations/Remodeling	\$300.00 plus \$.25 per square foot

**Alterations and/or Renovations where floor area does not apply:**

Alterations, renovations or modifications of existing buildings or structures where floor area does not apply (i.e. doors, windows, roofs, structural openings or beams, etc.); \$200 plus \$34 for each \$1,000 of estimated construction value of alterations, renovations or modification certified by the permit applicant

Roofing	\$250.00 per structure
PA UCC Training Fee	\$4.50 charge added to each building permit
Missed Inspection/Reinspection	\$55.00
Demolition	Structures without a foundation: <b>\$140</b> Structures with a foundation: <b>\$200</b> Commercial structures: <b>\$300 /1,000sf</b> of building area

Swimming Pools (Ch. 23, Part 1, §109)

Above Ground	\$140 plus \$.35 per square foot of surface area
In Ground	\$300.00 plus \$.35 per square foot of surface area
Building/Construction Permit Extension Fee	\$50.00/one-year extension
Zoning Fee	\$50.00/residential and non-residential new zoning permit

11/6/2024

Signs (Ch. 27, Part 11, §1106)

Permanent	\$75.00 plus \$1.00 per square foot
Temporary	\$75.00 plus \$50.00 escrow

Storage Pods	\$50.00/ residential and non-residential (30 days)
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Work Completed Without a Permit	Fees are Doubled
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Certified Mail Fee	Actual cost
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3. **A. Plumbing Licenses**

Master	\$50.00 per year
Journeyman	\$40.00 per year

**B. Plumbing Permits**

New	\$140.00 plus \$10.00 fixture unit
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Minor repairs to plumbing system:	\$140
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Water Service and/or Sewer Lateral Repair/ Replacement:	\$140 each
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4. **Mechanical Permit**

Residential	\$140 per unit
Commercial	\$250.00 per unit

5. **A. Electrician Licenses**

Master	\$50.00 per year
Journeyman	\$40.00 per year
Apprentice	\$10.00 per year

**B. Electrical Permits**

**Electrical plan review and inspections must be completed by a Hatfield Borough approved Third Party Agency**

Administrative electrical permit fee:	\$75.00
Meter Deposit	\$150.00

6. **Electric Utility (Resolution 2009-21, amending Ch. 1, Part 5, Section 505 Fee Schedule; Ch. 9, Part 1, §§103, 104, 105, 106, 108, 111 and 115)**

New Residential Customer Deposit	\$300.00
Residential Customer Deposit	
Customer late on two or more bills	\$300.00
New Commercial Customer Deposit	\$300.00
New Industrial Customer Deposit	\$300.00
Application Fee	\$25.00
Transfer of Service Fee	\$25.00
Disconnection Fee	\$50.00
Reconnection Fee	\$50.00
Priority Reconnection Fee	\$75.00
New Connection Fee	\$25.00
Temporary Construction Fee	\$150.00
Temporary Seasonal Construction Fee	\$100.00
Meter Tampering Fee	\$100.00 + cost of meter
Cut Seal Fee	\$50.00
Returned Check Fee	\$25.00
Complaint Testing Fee	\$350.00 – reimbursable if meter is found to be defective
Certified Mail Fee	Actual cost
Penalty Applied to Unpaid Balance for Bills Paid after Due Date	1.5% per month
Attorneys Fee for Collection of Delinquencies/Ordinance Enforcement	\$140.00 per hour plus court costs incurred
Late Fee	\$7.50 per month
Administrative Fee	\$5.00 per account

7. **Sewer Utility (Ch. 18, Part 2, §202.3)**

Transfer of Service Fee	\$25.00
Disconnection/Reconnection Fee	\$75.00
Inspection Fee	\$35.00 per connection
Administrative Fee	\$5.00 per account

8. **Highway Occupancy/Street Opening Permits (Ch. 21, Part 1, §105)**

Highway Occupancy	\$50.00
Street Opening	\$75.00 plus \$.25 per linear foot
Curbing	\$100.00 plus \$.25 per linear foot

**9. Fire Prevention Inspection and Permits (Ch. 5, Part 3, §305)**

Inspection Fee \$85.00/hour  
 Special Event Permits:

Exhibit and Trade Shows (in  
 Buildings)

1-5,000 square feet	\$200.00
5,001-50,000 square feet	\$250.00
50,000+ square feet	\$350.00

Display/Operation of Fueled  
 Vehicles in an Assembly Occupancy

1-5,000 square feet	\$200.00
5,001-50,000 square feet	\$250.00
50,000+ square feet	\$350.00

Carnivals and Fairs

Base Fee	\$200.00
Each Food Vendor (Cooking On site)	\$40.00

Public Assembly of 50 or More  
 Persons for a Special Event

Indoor	\$140.00
Outdoor	\$100.00

Temporary Membrane Structures, Tents and Canopies (each event)

Tents/ Membrane Structures	
>200 square feet	\$60.00
Canopies > 400 square feet	\$60.00

Use and Occupancy Permits--Non Residential

Low Hazard Occupancy (Business and  
 Mercantile)

1-2,000 square feet	\$140.00
2,001-5,000 square feet	\$180.00
5,001-10,000 square feet	\$230.00



10,001-100,000 square feet	\$280.00
100,000+	\$330.00

Moderate/High Hazard Occupancy  
(All other use groups)

1-2,000 square feet	\$200.00
2,001-5,000 square feet	\$250.00
5,001-10,000 square feet	\$300.00
10,001-100,000 square feet	\$400.00
100,000+	\$500.00

Fire Protection Permits--Construction

Residential--1 and 2 Family Dwellings (NFPA 13D Sprinkler System)

New Sprinkler System	\$300.00 each dwelling
Existing Sprinkler System	
Repairs, Modifications, etc	\$40.00 each dwelling
New Fire Alarm System	\$60.00 each dwelling
Existing Fire Alarm System	
Repairs, Modifications, etc	\$25.00 each dwelling

Residential--Multi Family, Hotel, Motel (NFPA 13R Sprinkler System)  
and Non Residential (NFPA 13 Sprinkler System)

New Sprinkler System	
Base Fee	\$450.00
Each Sprinkler Head	\$4.00

Existing Sprinkler System--Repairs, Modifications, etc  
for <20 Additional Heads

Base Fee	\$150.00
Each Sprinkler Head	\$3.00

Existing Sprinkler System--Repairs, Modifications, etc  
for 20 or more Additional Heads

Use New System Fees

New Fire Alarm System

Base Fee	\$120.00
Each 1,000 square feet or fraction thereof	\$20.00

## Existing Fire Alarm System-- Repairs, Modifications, etc

Base Fee	\$60.00
Each 1,000 square feet or fraction thereof	\$10.00

## Fire Protection/Suppression System (All Use Groups)

## Fire Pumps

One Pump	\$180.00
Each Additional Pump	\$60.00

## Standpipe Systems

Base Fee	\$120.00
Each Hose Outlet	\$20.00

## Clean Agent/Foam/ Specialized Fire Suppression System

Each System Up To 2000 square feet	\$180.00
Each Additional 2,000 square feet or fraction thereof	\$60.00

## Commercial Hood Suppression System

Each System	\$160.00
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## Underground Fires Service Mains

First 50 Feet	\$120.00
Each Foot Over 50	\$.25 per foot

Private Fire Hydrants	\$30.00 per hydrant
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Fire Protection/Life Safety System  
Not Listed

1% of total cost with  
minimum fee of \$120.00

Hazardous Activity of Process Equipment (All Use Groups)  
Installation, Modification, Repair or Alteration

## Battery Systems (Lead Acid

Exceeding 50 gallons)	1% of total cost with minimum fee of \$60.00
Blasting	\$60.00
Compressed Gas Systems	1% of total cost with minimum fee of \$60.00
Flammable and Combustible Liquids Production, Transportation or Storage Facilities or Equipment	1% of total cost with minimum fee of \$60.00
Hazardous Materials Storage Facilities	1% of total cost with minimum fee of \$60.00
Industrial Ovens	1% of total cost with minimum fee of \$60.00
Liquified Petroleum Gases (LPG) Systems	1% of total cost with minimum fee of \$60.00
Spraying Rooms, Booths or Dipping Tanks	1% of total cost with minimum fee of \$60.00
Storage Tanks (Hazardous Materials) Installation - First 1,000 gallons or fraction thereof	\$150.00
Each Additional 1,000 gallons or fraction thereof	\$25.00
Dispensing Pump (Each pump)	\$25.00
Removal/Abandon in Place (Each Tank)	\$60.00

#### Fire Protection Permits --Moderate/High Hazard Operations/Storage

##### Fee Based on Total Square Footage of Facility

1-2,000 square feet	\$200.00
2,001-5,000 square feet	\$250.00
5,001-10,000 square feet	\$300.00

10,001-100,000 square feet	\$400.00
100,001 sq. feet or greater	\$500.00

Copy of Fire Report	\$20.00
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10. **Fire Safety & Residential Rental Program Registrations and Inspections (Ch 5, Part 2, §§207, 208)**

Fire Safety Residential Rental Unit Registration (annual)	\$65.00
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Residential Rental Program Application Fee	\$65.00
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Residential Rental Unit Inspection (change in tenant, three-year inspection, property resale)	\$85.00 per unit
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Non-residential Unit up to and including 5,000 square feet in area (annual)	\$140.00
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Non-residential Unit over 5,000 square feet (annual)	\$140.00/year + \$85.00/hour incurred in inspection over first hour
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Missed Inspection	\$50.00
Reinspection Fee (allowed 1)	\$50.00 per time
Late Fee	\$10.00 per unit for every 30 days thereafter

Certified Mail Fee	Actual cost
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11. **Residential Property Transfer Certification (Chapter 5, Part 4, §407)**

Application prior to 10 days before settlement:	\$85.00
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Application within 10 days of settlement:	\$140.00
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12. **Building Certifications**

Floodplain	\$95.00
Zoning	\$55.00
Use and Occupancy, Construction	\$55.00

13. **Subdivision and Land Development (Chapter 22, Part 7, §703)**

Category I -- Residential Subdivisions and Land Developments: applies to all kinds of residential projects for sale, condominium or rental; any structural type; mobile homes and either as a subdivision or single tract land development.

<u>Number of Lots or Units*</u>	<u>General Fee Plus Fee per Each Lot/Unit</u>	<u>Escrow</u>
0-2	\$350.00 + \$100.00	\$1,500.00
3-15	\$350.00 + \$100.00	\$2,000.00
16-25	\$350.00 + \$100.00	\$2,500.00
26-50	\$350.00 + \$100.00	\$3,000.00
51+	\$350.00 + \$100.00	\$4,000.00

(\* whichever is greater)

Category II -- Non-Residential Land Developments: fees apply to all projects or sections of mixed projects, which are for non-residential use of any kind for sale, rental, lease or condominium in any type of building on a single tract of land.

<u>Building Gross Square Feet</u>	<u>General Fee Plus Fee per Gross Square Foot</u>	<u>Escrow</u>
1-2,500	\$500.00 + \$.10 per square foot	\$5,000.00
2,501-10,000	\$750.00 + \$.10 per square foot	\$5,000.00
10,001-25,000	\$1,000.00 + \$.10 per square foot	\$5,000.00
25,001-50,000	\$1,500.00 + \$.10 per square foot	\$5,000.00
50,001+	\$2,000.00 + \$.10 per square foot	\$5,000.00

Category III -- Non-Residential Subdivisions: fees apply to applications subdividing and conveying land for non-residential uses.

<u>Number of Lots</u>	<u>General Fee Plus Fee per Each Lot</u>	<u>Escrow</u>
1-2	\$500.00 + \$200.00 per lot	\$5,000.00
3+	\$750.00 + \$200.00 per lot	\$5,000.00

The escrow amounts funds cover expenses including engineering, technical and solicitor's services. Escrow funds shall not be reduced to an amount less than \$500 and shall be replenished when it reaches that level. An administrative processing fee of four (4) % shall be charged to the escrow fund for each payment made from it.

#### 14. **Conditional Use Hearings**



Single Family	\$500.00 plus \$150.00 for each additional hearing beyond one
Multi-Family	\$1,200.00 plus \$150.00 for each additional hearing beyond one
Non-Residential	\$1,400.00 plus \$150.00 for each additional hearing beyond one

**15. Zoning Applications and Hearings (Ch. 27, Part 3 §308; Part 4, §405; Part 5, §§503, 506; Part 6, §605)**

**A. Zoning Hearing Board Applications and Hearings:**

Single Family	\$500.00 plus \$150.00 for each additional hearing beyond one
Multi-Family	\$1,200.00 plus \$150.00 for each additional hearing beyond one
Non-Residential	\$1,400.00 plus \$150.00 for each additional hearing beyond one

**B. Rezoning Applications and Hearings** \$3,500.00 plus \$500.00 for each additional hearing beyond one

**C. Curative Amendment Application and Hearing** \$3,500.00 plus \$500.00 for each additional hearing beyond one

**16. Core Commercial District Parking (Ch. 27, Part 21, §2107(4)(A)(2))**

Fee in lieu of Off-Street Parking \$5,000.00 per space

**17. False Alarm (Chapter 10, Part 3)**

First false alarm per rolling twelve months warning issued

Second false alarm per rolling twelve months warning issued

Third and Fourth false alarms per rolling twelve months \$200.00 per false alarm

Fifth and Sixth false alarms per rolling twelve months \$300.00 per false alarm

Seventh and subsequent false alarms per rolling twelve months \$500.00 per false alarm

**18. Copying Fees**

Per Page Fee	\$ .25
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**19. Document Fees**

Zoning Ordinance	\$40.00/copy
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Subdivision Ordinance	\$40.00/copy
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Tax Certification	\$30 first year / \$5 each additional yr. \$5.00 per fulfillment of each request
-------------------	---

**20. Display of Fireworks**

Temporary Sale of Fireworks	\$75.00
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Permanent Sale of Fireworks (annual fee)	\$100.00
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Plus, cost of Conditional Use  
Application

**21. Emergency Call Outs**

Pick Up Truck	\$45.00/ hour plus the cost of labor
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Small Dump Truck	\$55.00/ hour plus the cost of labor
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10 Ton Dump Truck	\$65.00/ hour plus the cost of labor
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Bucket Truck	\$75.00/ hour plus the cost of labor
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Back Hoe	\$75.00/ hour plus the cost of labor
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Chipper	\$30.00/ hour plus the cost of labor
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Air Compressor	\$30.00 / hour plus the cost of labor
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Chain Saw	\$20.00 / hour plus the cost of labor
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Blower	\$20.00 / hour plus the cost of labor
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Pumps	\$20.00 / hour plus the cost of labor
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String Trimmer	\$20.00 / hour plus the cost of labor
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**22. Other Miscellaneous Fees**

Lawnmower	\$30.00/ hour plus the cost of labor
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String Trimmer	\$20.00/ hour plus the cost of labor
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Compact Utility Tractor	\$30.00/ hour plus the cost of labor
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Solicitation and Peddling Fee	\$100.00 for first 30 days \$100.00 for every additional 30 days
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Certified Mail Fee	Actual cost
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Attorneys' Fees for collection actions on delinquent accounts	Actual cost
Beekeepers Registration Fee (annual)	\$25.00
Open Burning Permit	\$25.00
Chicken Permit	\$25.00

### **23. Exemptions**

Permits must be obtained; however, permit issuance fees and general permit inspection fees are not payable, by any of the following:

- A. Commonwealth of Pennsylvania
- B. Political Subdivisions of the Commonwealth
- C. Governmental Authorities organized under the laws of the Commonwealth
- D. The Federal Government

### **24. Legal Fees**

To the extent that any state statute or local ordinance allows the Borough to collect or be reimbursed for attorney's fees, they are charged at the Borough Solicitor's usual rate of \$140 per hour.

BE IT FURTHER RESOLVED, that this Consolidated Fee Schedule supersedes and replaces all prior fees and Fee Schedules adopted by the Council of the Borough of Hatfield.

NOW APPROVED and adopted by Borough Council at a duly advertised public meeting held this 15<sup>th</sup> day of May, 2019 with \_\_\_\_ Council Members Voting "Aye" and \_\_\_\_ Council Members voting "Nay".

Attest:

Borough of Hatfield

---

Jaime E. Snyder  
Borough Manager/Secretary

---

Jason Ferguson  
Borough Council President

---

Mary Anne Girard, Mayor

5/15/19

**BOROUGH OF HATFIELD  
RESOLUTION NO. ~~2024~~  
CONSOLIDATED FEE SCHEDULE**

Deleted: 2019-09

**WHEREAS**, the Borough of Hatfield charges fees for various services provided and applications made to the Borough; and

**WHEREAS**, Borough Council wishes to adopt a consolidated schedule of fees for these services.

**NOW THEREFORE, BE IT RESOLVED** by the Borough Council of the Borough of Hatfield that the following consolidated schedule of fees is adopted, reflecting the fees as of the adoption of this resolution.

**TABLE OF CONTENTS**

1. Amusement Devices
2. Building and Construction Inspections and Permits
3. Plumbing Licenses and Permits
4. Mechanical Permits
5. Electrician Licenses and Permits
6. Electric Utility
7. Sewer Utility
8. Highway Occupancy/Street Opening Permits
9. Fire Prevention Inspections and Permits
10. Property Registrations and Inspections
11. Residential Property Transfer Certification
12. Building Certifications
13. Subdivision and Land Development
14. Conditional Use Hearings
15. Zoning Permits and Zoning Application and Hearings
16. Core Commercial District Parking
17. False Alarm
18. Copying Fees
19. Document Fees
20. Sale of Fireworks
21. Emergency Call Out
22. Other Miscellaneous Fees
23. Exemptions
24. Legal Fees

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## FEES

### 1. Amusement Devices (Ch. 13, §103)

License Fee: \$100.00 per machine, annually

### 2. Building and Construction Inspections and Permits (Ch. 5, Part 1, §108; Ch. 8, Part 2, §209)

#### Residential:

New Construction  
(incl. additions outside existing structure) \$400.00 plus \$.25 per square foot

Apartment/Condo/Townhouse  
(in common deed) \$400.00 plus \$.25 per square foot

Alterations/Remodeling ~~\$200.00~~ plus \$.25 per square foot

Alterations and/or Renovations where floor area does not apply:  
Alterations, renovations or modifications of existing buildings or structures where floor area does not apply (i.e. doors, windows, roofs, structural openings or beams, etc.); \$200 plus \$34 for each \$1,000 of estimated construction value of alterations, renovations or modification certified by the permit applicant

Accessory Structures over 200 square feet: \$140 plus 30¢ per sq/ft of area

Utility Shed  
200 square feet or less \$50.00

Replacement of existing utility shed within existing footprint \$50.00

Deck \$140.00 plus ~~\$~~30 per square foot  
Fence \$100.00 plus \$.25 per linear foot

Home Occupation Permit \$100.00 (one-time fee)

Driveway Permit \$200.00 plus \$.25 per square foot

Roofing \$140.00 per structure

#### Non-residential:

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New Construction  
(incl. additions outside existing structure) \$400.00 plus \$.30 per square foot

Alterations/Remodeling \$300.00 plus \$.25 per square foot

Alterations and/or Renovations where floor area does not apply:  
Alterations, renovations or modifications of existing buildings or structures where  
floor area does not apply (i.e. doors, windows, roofs, structural openings or  
beams, etc.); \$200 plus \$34 for each \$1,000 of estimated construction value of  
alterations, renovations or modification certified by the permit applicant

Roofing

\$250.00 per structure

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PA UCC Training Fee

\$4.50 charge added to each building permit

Missed Inspection/Reinspection

\$55.00

Demolition

Structures without a foundation: \$140

Structures with a foundation: \$200

Commercial structures: \$300 /1,000sf of building area

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Swimming Pools (Ch. 23, Part 1, §109)

Above Ground

\$140 plus \$.35 per square foot of surface area

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In Ground

\$300.00 plus \$.35 per square foot of surface area

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Building/Construction Permit Extension Fee

\$50.00/one-year extension

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Zoning Fee

\$50.00/residential and non-residential new zoning permit

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Signs (Ch. 27, Part 11, §1106)

5/15/19

	Permanent	<del>\$75.00</del> plus \$1.00 per square foot	Deleted: 50
	Temporary	<del>\$75.00</del> plus \$50.00 escrow	Deleted: 50
	Storage Pods	<del>\$50.00/</del> residential and non-residential (30 days)	Deleted: 25
	Work Completed Without a Permit	Fees are Doubled	
	Certified Mail Fee	Actual cost	
3.	<b>A. Plumbing Licenses</b>		
	Master	\$50.00 per year	
	Journeyman	\$40.00 per year	
	<b>B. Plumbing Permits</b>		
	New	<del>\$140.00</del> plus \$10.00 fixture unit	Deleted: 50
	<u>Minor repairs to plumbing system:</u>	<del>\$140</del>	Deleted: Replacement \$30.00 plus \$8.00 fixture unit
	<u>Water Service and/or Sewer Lateral Repair/ Replacement: \$140 each</u>		Formatted: Underline
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4.	<b>Mechanical Permit</b>		
	Residential	<del>\$140</del> per unit	Deleted: \$100.00
	Commercial	\$250.00 per unit	Deleted: / Fire Place
			Deleted: / \$75.00
			Deleted: Rough & Final \$60.00
5.	<b>A. Electrician Licenses</b>		
	Master	\$50.00 per year	
	Journeyman	\$40.00 per year	
	Apprentice	\$10.00 per year	
	<b>B. <u>Electrical Permits</u></b>		
	<u>Electrical plan review and inspections must be completed by a Hatfield Borough approved Third Party Agency</u>		Deleted: Electrician
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	<u>Administrative electrical permit fee:</u>	<del>\$75.00</del>	Deleted: New and Replacement
	Meter Deposit	\$150.00	Deleted: 50
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			Deleted: Devices .50 per fixture / rough and final

5/15/19

6. **Electric Utility (Resolution 2009-21, amending Ch. 1, Part 5, Section 505 Fee Schedule; Ch. 9, Part 1, §§103, 104, 105, 106, 108, 111 and 115)**

New Residential Customer Deposit	\$300.00
Residential Customer Deposit	
Customer late on two or more bills	\$300.00
New Commercial Customer Deposit	\$300.00
New Industrial Customer Deposit	\$300.00
Application Fee	\$25.00
Transfer of Service Fee	\$25.00
Disconnection Fee	\$50.00
Reconnection Fee	\$50.00
Priority Reconnection Fee	\$75.00
New Connection Fee	\$25.00
Temporary Construction Fee	\$150.00
Temporary Seasonal Construction Fee	\$100.00
Meter Tampering Fee	\$100.00 + cost of meter
Cut Seal Fee	\$50.00
Returned Check Fee	\$25.00
Complaint Testing Fee	\$350.00 – reimbursable if meter is found to be defective
Certified Mail Fee	Actual cost
Penalty Applied to Unpaid Balance for Bills Paid after Due Date	1.5% per month
Attorneys Fee for Collection of Delinquencies/Ordinance Enforcement	\$140.00 per hour plus court costs incurred
Late Fee	\$7.50 per month
Administrative Fee	\$5.00 per account

7. **Sewer Utility (Ch. 18, Part 2, §202.3)**

Transfer of Service Fee	\$25.00
Disconnection/Reconnection Fee	\$75.00
Inspection Fee	\$35.00 per connection
Administrative Fee	\$5.00 per account

8. **Highway Occupancy/Street Opening Permits (Ch. 21, Part 1, §105)**

Highway Occupancy	\$50.00
Street Opening	\$75.00 plus \$.25 per linear foot
Curbing	\$100.00 plus \$.25 per linear foot

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9. **Fire Prevention Inspection and Permits (Ch. 5, Part 3, §305)**

Inspection Fee ~~\$85.00~~/hour  
Special Event Permits:

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Exhibit and Trade Shows (in Buildings)

1-5,000 square feet ~~\$200.00~~  
5,001-50,000 square feet ~~\$250.00~~  
50,000+ square feet ~~\$350.00~~

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Display/Operation of Fueled Vehicles in an Assembly Occupancy

1-5,000 square feet ~~\$200.00~~  
5,001-50,000 square feet ~~\$250.00~~  
50,000+ square feet ~~\$350.00~~

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Carnivals and Fairs

Base Fee ~~\$200.00~~  
Each Food Vendor (Cooking On site) ~~\$40.00~~

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Public Assembly of 50 or More Persons for a Special Event

Indoor ~~\$140.00~~  
Outdoor ~~\$100.00~~

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Temporary Membrane Structures, Tents and Canopies (each event)

Tents/ Membrane Structures  
>200 square feet ~~\$60.00~~  
Canopies > 400 square feet ~~\$60.00~~

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Use and Occupancy Permits--Non Residential

Low Hazard Occupancy (Business and Mercantile)

1-2,000 square feet ~~\$140.00~~  
2,001-5,000 square feet ~~\$180.00~~  
5,001-10,000 square feet ~~\$230.00~~  
10,001-100,000 square feet ~~\$280.00~~

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100,000+ \$330.00

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Moderate/High Hazard Occupancy  
(All other use groups)

1-2,000 square feet	\$200.00
2,001-5,000 square feet	\$250.00
5,001-10,000 square feet	\$300.00
10,001-100,000 square feet	\$400.00
100,000+	\$500.00

Fire Protection Permits--Construction

Residential--1 and 2 Family Dwellings (NFPA 13D Sprinkler System)

New Sprinkler System	\$300.00 each dwelling
Existing Sprinkler System	
Repairs, Modifications, etc	\$40.00 each dwelling
New Fire Alarm System	\$60.00 each dwelling
Existing Fire Alarm System	
Repairs, Modifications, etc	\$25.00 each dwelling

Residential--Multi Family, Hotel, Motel (NFPA 13R Sprinkler System)  
and Non Residential (NFPA 13 Sprinkler System)

New Sprinkler System	
Base Fee	\$450.00
Each Sprinkler Head	\$4.00

Existing Sprinkler System--Repairs, Modifications, etc  
for <20 Additional Heads

Base Fee	\$150.00
Each Sprinkler Head	\$3.00

Existing Sprinkler System--Repairs, Modifications, etc  
for 20 or more Additional Heads

Use New System Fees

New Fire Alarm System

Base Fee	\$120.00
Each 1,000 square feet or fraction thereof	\$20.00



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Existing Fire Alarm System-- Repairs, Modifications, etc

Base Fee	\$60.00
Each 1,000 square feet or fraction thereof	\$10.00

Fire Protection/Suppression System (All Use Groups)

Fire Pumps

One Pump	\$180.00
Each Additional Pump	\$60.00

Standpipe Systems

Base Fee	\$120.00
Each Hose Outlet	\$20.00

Clean Agent/Foam/ Specialized Fire Suppression System

Each System Up To 2000 square feet	\$180.00
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Each Additional 2,000 square feet or fraction thereof	\$60.00
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Commercial Hood Suppression System

Each System	\$160.00
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Underground Fires Service Mains

First 50 Feet	\$120.00
Each Foot Over 50	\$.25 per foot

Private Fire Hydrants	\$30.00 per hydrant
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Fire Protection/Life Safety System Not Listed	1% of total cost with minimum fee of \$120.00
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Hazardous Activity of Process Equipment (All Use Groups)  
Installation, Modification, Repair or Alteration

Battery Systems (Lead Acid

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Exceeding 50 gallons)	1% of total cost with minimum fee of \$60.00
Blasting	\$60.00
Compressed Gas Systems	1% of total cost with minimum fee of \$60.00
Flammable and Combustible Liquids Production, Transportation or Storage Facilities or Equipment	1% of total cost with minimum fee of \$60.00
Hazardous Materials Storage Facilities	1% of total cost with minimum fee of \$60.00
Industrial Ovens	1% of total cost with minimum fee of \$60.00
Liquefied Petroleum Gases (LPG) Systems	1% of total cost with minimum fee of \$60.00
Spraying Rooms, Booths or Dipping Tanks	1% of total cost with minimum fee of \$60.00
Storage Tanks (Hazardous Materials) Installation - First 1,000 gallons or fraction thereof	\$150.00
Each Additional 1,000 gallons or fraction thereof	\$25.00
Dispensing Pump (Each pump)	\$25.00
Removal/Abandon in Place (Each Tank)	\$60.00

Fire Protection Permits --Moderate/High Hazard Operations/Storage

Fee Based on Total Square Footage of Facility

1-2,000 square feet	\$200.00
2,001-5,000 square feet	\$250.00
5,001-10,000 square feet	\$300.00

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10,001-100,000 square feet	\$400.00
100,001 sq. feet or greater	\$500.00

Copy of Fire Report	\$20.00
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10. **Property Registrations and Inspections (Ch 5, Part 2, §§207, 208)**

Residential Rental Unit <u>Registration</u> (annual)	<u>\$65.00</u>
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<u>Residential Rental Unit Inspection</u> <u>(change in tenant, three-year inspection, property resale)</u>	<u>\$75.00 per unit</u>
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Non-residential Unit up to and including 5,000 square feet in area (annual)	<u>\$140.00</u>
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Non-residential Unit over 5,000 square feet (annual)	<u>\$140.00/year + \$85.00/hour</u> incurred in inspection over first hour
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Missed Inspection	\$50.00
Reinspection Fee (allowed 1)	\$50.00 per time
Late Fee	\$10.00 per unit for every 30 days thereafter

Certified Mail Fee	Actual cost
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11. **Residential Property Transfer Certification (Chapter 5, Part 4, §407)**

Application prior to 10 days before settlement:	<u>\$85.00</u>
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Application within 10 days of settlement:	<u>\$140.00</u>
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12. **Building Certifications**

Floodplain	\$95.00
Zoning	\$55.00
Use and Occupancy, Construction	\$55.00

13. **Subdivision and Land Development (Chapter 22, Part 7, §703)**

Category I -- Residential Subdivisions and Land Developments: applies to all kinds of residential projects for sale, condominium or rental; any structural type; mobile homes and either as a subdivision or single tract land development.

Number of Lots	General Fee
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<u>or Units*</u>	<u>Plus Fee per Each Lot/Unit</u>	<u>Escrow</u>
0-2	\$350.00 + \$100.00	\$1,500.00
3-15	\$350.00 + \$100.00	\$2,000.00
16-25	\$350.00 + \$100.00	\$2,500.00
26-50	\$350.00 + \$100.00	\$3,000.00
51+	\$350.00 + \$100.00	\$4,000.00

(\* whichever is greater)

Category II -- Non-Residential Land Developments: fees apply to all projects or sections of mixed projects, which are for non-residential use of any kind for sale, rental, lease or condominium in any type of building on a single tract of land.

<u>Building Gross Square Feet</u>	<u>General Fee Plus Fee per Gross Square Foot</u>	<u>Escrow</u>
1-2,500	\$500.00 + \$.10 per square foot	\$5,000.00
2,501-10,000	\$750.00 + \$.10 per square foot	\$5,000.00
10,001-25,000	\$1,000.00 + \$.10 per square foot	\$5,000.00
25,001-50,000	\$1,500.00 + \$.10 per square foot	\$5,000.00
50,001+	\$2,000.00 + \$.10 per square foot	\$5,000.00

Category III -- Non-Residential Subdivisions: fees apply to applications subdividing and conveying land for non-residential uses.

<u>Number of Lots</u>	<u>General Fee Plus Fee per Each Lot</u>	<u>Escrow</u>
1-2	\$500.00 + \$200.00 per lot	\$5,000.00
3+	\$750.00 + \$200.00 per lot	\$5,000.00

The escrow amounts funds cover expenses including engineering, technical and solicitor's services. Escrow funds shall not be reduced to an amount less than \$500 and shall be replenished when it reaches that level. An administrative processing fee of four (4) % shall be charged to the escrow fund for each payment made from it.

#### 14. Conditional Use Hearings

Single Family	\$500.00 plus \$150.00 for each additional hearing beyond one
Multi-Family	\$1,200.00 plus \$150.00 for each additional hearing beyond one
Non-Residential	\$1,400.00 plus \$150.00 for each additional hearing beyond one

5/15/19

15. **Zoning Applications and Hearings (Ch. 27, Part 3 §308; Part 4, §405; Part 5, §§503, 506; Part 6, §605)**

Deleted: Zoning Permits and

A. Zoning Hearing Board Applications and Hearings:

Single Family	\$500.00 plus \$150.00 for each additional hearing beyond one
Multi-Family	\$1,200.00 plus \$150.00 for each additional hearing beyond one
Non-Residential	\$1,400.00 plus \$150.00 for each additional hearing beyond one

B. Rezoning Applications and Hearings \$3,500.00 plus \$500.00 for each additional hearing beyond one

C. Curative Amendment Application and Hearing \$3,500.00 plus \$500.00 for each additional hearing beyond one

16. **Core Commercial District Parking (Ch. 27, Part 21, §2107(4)(A)(2))**

Fee in lieu of Off-Street Parking \$5,000.00 per space

17. **False Alarm (Chapter 10, Part 3)**

First false alarm per rolling twelve months	warning issued
Second false alarm per rolling twelve months	warning issued
Third and Fourth false alarms per rolling twelve months	\$200.00 per false alarm
Fifth and Sixth false alarms per rolling twelve months	\$300.00 per false alarm
Seventh and subsequent false alarms per rolling twelve months	\$500.00 per false alarm

18. **Copying Fees**

Per Page Fee \$.25

19. **Document Fees**

Zoning Ordinance \$40.00/copy



5/15/19

	Subdivision Ordinance	\$40.00/copy
	Tax Certification	\$30 first year / \$5 each additional yr. \$5.00 per fulfillment of each request
20.	<b>Display of Fireworks</b>	
	Temporary Sale of Fireworks	\$75.00
	Permanent Sale of Fireworks (annual fee)	\$100.00 Plus, cost of Conditional Use Application
21.	<b>Emergency Call Outs</b>	
	Pick Up Truck	\$45.00 per hour
	Small Dump Truck	\$55.00 per hour
	10 Ton Dump Truck	\$65.00 per hour
	Bucket Truck	\$75.00 per hour
	Back Hoe	\$75.00 per hour
	Chipper	\$30.00 per hour
	Air Compressor	\$30.00 per hour
	Chain Saw	\$20.00 per hour
	Blower	\$20.00 per hour
	Pumps	\$20.00 per hour
	String Trimmer	\$20.00 per hour
		Plus, the cost of labor
22.	<b>Other Miscellaneous Fees</b>	
	Solicitation and Peddling Fee	\$100.00 for first 30 days \$100.00 for every additional 30 days
	Certified Mail Fee	Actual cost
	Attorneys' Fees for collection actions on delinquent accounts	Actual cost
	Beekeepers Registration Fee (annual)	\$25.00
	Open Burning Permit	\$25.00
23.	<b>Exemptions</b>	
	Permits must be obtained; however, permit issuance fees and general permit inspection fees are not payable, by any of the following:	
	A.	Commonwealth of Pennsylvania
	B.	Political Subdivisions of the Commonwealth
	C.	Governmental Authorities organized under the laws of the Commonwealth

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D. The Federal Government

24. **Legal Fees**

To the extent that any state statute or local ordinance allows the Borough to collect or be reimbursed for attorney's fees, they are charged at the Borough Solicitor's usual rate of \$140 per hour.

BE IT FURTHER RESOLVED, that this Consolidated Fee Schedule supercedes and replaces all prior fees and Fee Schedules adopted by the Council of the Borough of Hatfield.

NOW APPROVED and adopted by Borough Council at a duly advertised public meeting held this 15<sup>th</sup> day of May, 2019 with \_\_\_\_ Council Members Voting "Aye" and \_\_\_\_ Council Members voting "Nay".

Attest:

Borough of Hatfield

\_\_\_\_\_  
Michael DeFinis  
Borough Manager/Secretary

\_\_\_\_\_  
John H. Weierman  
Borough Council President

\_\_\_\_\_  
Robert L. Kaler III, Mayor

5/15/19

**BOROUGH OF HATFIELD  
RESOLUTION NO. ,  
CONSOLIDATED FEE SCHEDULE**

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**WHEREAS**, the Borough of Hatfield charges fees for various services provided and applications made to the Borough; and

**WHEREAS**, Borough Council wishes to adopt a consolidated schedule of fees for these services.

**NOW THEREFORE, BE IT RESOLVED** by the Borough Council of the Borough of Hatfield that the following consolidated schedule of fees is adopted, reflecting the fees as of the adoption of this resolution.

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10. Property Registrations and Inspections
11. Residential Property Transfer Certification
12. Building Certifications
13. Subdivision and Land Development
14. Conditional Use Hearings
15. Zoning Permits and Zoning Application and Hearings
16. Core Commercial District Parking
17. False Alarm
18. Copying Fees
19. Document Fees
20. Sale of Fireworks
21. Emergency Call Out
22. Other Miscellaneous Fees
23. Exemptions
24. Legal Fees

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## FEES

### 1. Amusement Devices (Ch. 13, §103)

License Fee: \$100.00 per machine, annually

### 2. Building and Construction Inspections and Permits (Ch. 5, Part 1, §108; Ch. 8, Part 2, §209)

#### Residential:

New Construction  
(incl. additions outside existing structure) \$400.00 plus \$.25 per square foot

Apartment/Condo/Townhouse  
(in common deed) \$400.00 plus \$.25 per square foot

Alterations/Remodeling ~~\$200.00~~ plus \$.25 per square foot

Alterations and/or Renovations where floor area does not apply:  
Alterations, renovations or modifications of existing buildings or structures where floor area does not apply (i.e. doors, windows, roofs, structural openings or beams, etc.); \$200 plus \$34 for each \$1,000 of estimated construction value of alterations, renovations or modification certified by the permit applicant

Accessory Structures over 200 square feet: \$140 plus 30¢ per sq/ft of area

Utility Shed  
~~200 square feet or less~~ \$50.00

Replacement of existing utility shed within  
existing footprint \$50.00

Deck \$140.00 plus \$.30 per square foot  
Fence \$100.00 plus \$.25 per linear foot

Home Occupation Permit \$100.00 (one-time fee)

Driveway Permit \$200.00 plus \$.25 per square foot

Roofing \$140.00 per structure

#### Non-residential:

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Deleted: Unattached Garage \$100.00 plus \$.25 per square foot  
1

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5/15/19

New Construction  
(incl. additions outside existing structure) \$400.00 plus \$.30 per square foot

Alterations/Remodeling \$300.00 plus \$.25 per square foot

Alterations and/or Renovations where floor area does not apply:  
Alterations, renovations or modifications of existing buildings or structures where  
floor area does not apply (i.e. doors, windows, roofs, structural openings or  
beams, etc.): \$200 plus \$34 for each \$1,000 of estimated construction value of  
alterations, renovations or modification certified by the permit applicant

Roofing

\$250.00 per structure

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PA UCC Training Fee

\$4.50 charge added to each building permit

Missed Inspection/Reinspection

\$55.00

Demolition

Structures without a foundation: \$140

Structures with a foundation: \$200

Commercial structures: \$300 /1,000sf of building area

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Swimming Pools (Ch. 23, Part 1, §109)

Above Ground

\$140 plus \$.35 per square foot of surface area

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In Ground

\$300.00 plus \$.35 per square foot of surface area

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Building/Construction Permit Extension Fee

\$50.00/one-year extension

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Zoning Fee

\$50.00/residential and non-residential new zoning permit

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Signs (Ch. 27, Part 11, §1106)



5/15/19

	Permanent	\$75.00 plus \$1.00 per square foot	Deleted: 50
	Temporary	\$75.00 plus \$50.00 escrow	Deleted: 50
	Storage Pods	\$50.00/ residential and non-residential (30 days)	Deleted: 25
	Work Completed Without a Permit	Fees are Doubled	
	Certified Mail Fee	Actual cost	
3.	<b>A. Plumbing Licenses</b>		
	Master	\$50.00 per year	
	Journeyman	\$40.00 per year	
	<b>B. Plumbing Permits</b>		
	New	\$140.00 plus \$10.00 fixture unit	Deleted: 50
	<u>Minor repairs to plumbing system: \$140</u>		Deleted: Replacement \$30.00 plus \$8.00 fixture unit
	<u>Water Service and/or Sewer Lateral Repair/ Replacement: \$140 each</u>		Formatted: Underline
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			Deleted: Rough & Final \$60.00
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4.	<b>Mechanical Permit</b>		
	Residential	\$140 per unit	Deleted: \$100.00
	Commercial	\$250.00 per unit	Deleted: / Fire Place
			Deleted: / \$75.00
			Deleted: Rough & Final \$60.00
5.	<b>A. Electrician Licenses</b>		
	Master	\$50.00 per year	
	Journeyman	\$40.00 per year	
	Apprentice	\$10.00 per year	
	<b>B. Electrical Permits</b>		
	<u>Electrical plan review and inspections must be completed by a Hatfield Borough approved Third Party Agency</u>		Deleted: Electrician
			Formatted: Indent: Left: 0.5"
	<u>Administrative electrical permit fee:</u>	\$75.00	Deleted: New and Replacement
	Meter Deposit	\$150.00	Deleted: 50
			Deleted: plus \$25.00 per 100 amps of service
			Deleted: Devices 50 per fixture / rough and final

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6. **Electric Utility (Resolution 2009-21, amending Ch. 1, Part 5, Section 505 Fee Schedule; Ch. 9, Part 1, §§103, 104, 105, 106, 108, 111 and 115)**

New Residential Customer Deposit	\$300.00
Residential Customer Deposit	
Customer late on two or more bills	\$300.00
New Commercial Customer Deposit	\$300.00
New Industrial Customer Deposit	\$300.00
Application Fee	\$25.00
Transfer of Service Fee	\$25.00
Disconnection Fee	\$50.00
Reconnection Fee	\$50.00
Priority Reconnection Fee	\$75.00
New Connection Fee	\$25.00
Temporary Construction Fee	\$150.00
Temporary Seasonal Construction Fee	\$100.00
Meter Tampering Fee	\$100.00 + cost of meter
Cut Seal Fee	\$50.00
Returned Check Fee	\$25.00
Complaint Testing Fee	\$350.00 – reimbursable if meter is found to be defective
Certified Mail Fee	Actual cost
Penalty Applied to Unpaid Balance for Bills Paid after Due Date	1.5% per month
Attorneys Fee for Collection of Delinquencies/Ordinance Enforcement	\$140.00 per hour plus court costs incurred
Late Fee	\$7.50 per month
Administrative Fee	\$5.00 per account

7. **Sewer Utility (Ch. 18, Part 2, §202.3)**

Transfer of Service Fee	\$25.00
Disconnection/Reconnection Fee	\$75.00
Inspection Fee	\$35.00 per connection
Administrative Fee	\$5.00 per account

8. **Highway Occupancy/Street Opening Permits (Ch. 21, Part 1, §105)**

Highway Occupancy	\$50.00
Street Opening	\$75.00 plus \$.25 per linear foot
Curbing	\$100.00 plus \$.25 per linear foot

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9. **Fire Prevention Inspection and Permits (Ch. 5, Part 3, §305)**

Inspection Fee \$85.00/hour  
Special Event Permits:

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Exhibit and Trade Shows (in Buildings)

1-5,000 square feet \$200.00  
5,001-50,000 square feet \$250.00  
50,000+ square feet \$350.00

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Display/Operation of Fueled Vehicles in an Assembly Occupancy

1-5,000 square feet \$200.00  
5,001-50,000 square feet \$250.00  
50,000+ square feet \$350.00

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Carnivals and Fairs

Base Fee \$200.00  
Each Food Vendor (Cooking On site) \$40.00

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Public Assembly of 50 or More Persons for a Special Event

Indoor \$140.00  
Outdoor \$100.00

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Temporary Membrane Structures, Tents and Canopies (each event)

Tents/ Membrane Structures  
>200 square feet \$60.00  
Canopies > 400 square feet \$60.00

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Use and Occupancy Permits--Non Residential

Low Hazard Occupancy (Business and Mercantile)

1-2,000 square feet \$140.00  
2,001-5,000 square feet \$180.00  
5,001-10,000 square feet \$230.00  
10,001-100,000 square feet \$280.00

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100,000+

\$330.00

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Moderate/High Hazard Occupancy  
(All other use groups)

1-2,000 square feet	\$200.00
2,001-5,000 square feet	\$250.00
5,001-10,000 square feet	\$300.00
10,001-100,000 square feet	\$400.00
100,000+	\$500.00

#### Fire Protection Permits--Construction

##### Residential--1 and 2 Family Dwellings (NFPA 13D Sprinkler System)

New Sprinkler System	\$300.00 each dwelling
Existing Sprinkler System	
Repairs, Modifications, etc	\$40.00 each dwelling
New Fire Alarm System	\$60.00 each dwelling
Existing Fire Alarm System	
Repairs, Modifications, etc	\$25.00 each dwelling

##### Residential--Multi Family, Hotel, Motel (NFPA 13R Sprinkler System) and Non Residential (NFPA 13 Sprinkler System)

New Sprinkler System	
Base Fee	\$450.00
Each Sprinkler Head	\$4.00

Existing Sprinkler System--Repairs, Modifications, etc  
for <20 Additional Heads

Base Fee	\$150.00
Each Sprinkler Head	\$3.00

Existing Sprinkler System--Repairs, Modifications, etc  
for 20 or more Additional Heads

Use New System Fees

##### New Fire Alarm System

Base Fee	\$120.00
Each 1,000 square feet or fraction thereof	\$20.00

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Existing Fire Alarm System-- Repairs, Modifications, etc

Base Fee	\$60.00
Each 1,000 square feet or fraction thereof	\$10.00

Fire Protection/Suppression System (All Use Groups)

Fire Pumps

One Pump	\$180.00
Each Additional Pump	\$60.00

Standpipe Systems

Base Fee	\$120.00
Each Hose Outlet	\$20.00

Clean Agent/Foam/ Specialized Fire Suppression System

Each System Up To 2000 square feet	\$180.00
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Each Additional 2,000 square feet or fraction thereof	\$60.00
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Commercial Hood Suppression System

Each System	\$160.00
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Underground Fires Service Mains

First 50 Feet	\$120.00
Each Foot Over 50	\$.25 per foot

Private Fire Hydrants	\$30.00 per hydrant
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Fire Protection/Life Safety System Not Listed	1% of total cost with minimum fee of \$120.00
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Hazardous Activity of Process Equipment (All Use Groups)  
Installation, Modification, Repair or Alteration

Battery Systems (Lead Acid

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Exceeding 50 gallons)	1% of total cost with minimum fee of \$60.00
Blasting	\$60.00
Compressed Gas Systems	1% of total cost with minimum fee of \$60.00
Flammable and Combustible Liquids Production, Transportation or Storage Facilities or Equipment	1% of total cost with minimum fee of \$60.00
Hazardous Materials Storage Facilities	1% of total cost with minimum fee of \$60.00
Industrial Ovens	1% of total cost with minimum fee of \$60.00
Liquefied Petroleum Gases (LPG) Systems	1% of total cost with minimum fee of \$60.00
Spraying Rooms, Booths or Dipping Tanks	1% of total cost with minimum fee of \$60.00
Storage Tanks (Hazardous Materials) Installation - First 1,000 gallons or fraction thereof	\$150.00
Each Additional 1,000 gallons or fraction thereof	\$25.00
Dispensing Pump (Each pump)	\$25.00
Removal/Abandon in Place (Each Tank)	\$60.00

Fire Protection Permits --Moderate/High Hazard Operations/Storage

Fee Based on Total Square Footage of Facility

1-2,000 square feet	\$200.00
2,001-5,000 square feet	\$250.00
5,001-10,000 square feet	\$300.00



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10,001-100,000 square feet	\$400.00
100,001 sq. feet or greater	\$500.00

Copy of Fire Report	\$20.00
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10. **Property Registrations and Inspections (Ch 5, Part 2, §§207, 208)**

Residential Rental Unit <u>Registration</u> (annual)	<u>\$65.00</u>
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<u>Residential Rental Unit Inspection</u> <u>(change in tenant, three-year inspection, property resale)</u>	<u>\$75.00 per unit</u>
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Non-residential Unit up to and including 5,000 square feet in area (annual)	<u>\$140.00</u>
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Non-residential Unit over 5,000 square feet (annual)	<u>\$140.00/year + \$85.00/hour</u> incurred in inspection over first hour
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Missed Inspection	\$50.00
Reinspection Fee (allowed 1)	\$50.00 per time
Late Fee	\$10.00 per unit for every 30 days thereafter

Certified Mail Fee	Actual cost
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11. **Residential Property Transfer Certification (Chapter 5, Part 4, §407)**

Application prior to 10 days before settlement:	<u>\$85.00</u>
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Application within 10 days of settlement:	<u>\$140.00</u>
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12. **Building Certifications**

Floodplain	\$95.00
Zoning	\$55.00
Use and Occupancy, Construction	\$55.00

13. **Subdivision and Land Development (Chapter 22, Part 7, §703)**

Category I -- Residential Subdivisions and Land Developments: applies to all kinds of residential projects for sale, condominium or rental; any structural type; mobile homes and either as a subdivision or single tract land development.

Number of Lots	General Fee
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<u>or Units*</u>	<u>Plus Fee per Each Lot/Unit</u>	<u>Escrow</u>
0-2	\$350.00 + \$100.00	\$1,500.00
3-15	\$350.00 + \$100.00	\$2,000.00
16-25	\$350.00 + \$100.00	\$2,500.00
26-50	\$350.00 + \$100.00	\$3,000.00
51+	\$350.00 + \$100.00	\$4,000.00

(\* whichever is greater)

Category II -- Non-Residential Land Developments: fees apply to all projects or sections of mixed projects, which are for non-residential use of any kind for sale, rental, lease or condominium in any type of building on a single tract of land.

<u>Building Gross Square Feet</u>	<u>General Fee Plus Fee per Gross Square Foot</u>	<u>Escrow</u>
1-2,500	\$500.00 + \$.10 per square foot	\$5,000.00
2,501-10,000	\$750.00 + \$.10 per square foot	\$5,000.00
10,001-25,000	\$1,000.00 + \$.10 per square foot	\$5,000.00
25,001-50,000	\$1,500.00 + \$.10 per square foot	\$5,000.00
50,001+	\$2,000.00 + \$.10 per square foot	\$5,000.00

Category III -- Non-Residential Subdivisions: fees apply to applications subdividing and conveying land for non-residential uses.

<u>Number of Lots</u>	<u>General Fee Plus Fee per Each Lot</u>	<u>Escrow</u>
1-2	\$500.00 + \$200.00 per lot	\$5,000.00
3+	\$750.00 + \$200.00 per lot	\$5,000.00

The escrow amounts funds cover expenses including engineering, technical and solicitor's services. Escrow funds shall not be reduced to an amount less than \$500 and shall be replenished when it reaches that level. An administrative processing fee of four (4) % shall be charged to the escrow fund for each payment made from it.

#### 14. Conditional Use Hearings

Single Family	\$500.00 plus \$150.00 for each additional hearing beyond one
Multi-Family	\$1,200.00 plus \$150.00 for each additional hearing beyond one
Non-Residential	\$1,400.00 plus \$150.00 for each additional hearing beyond one

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15. **Zoning Applications and Hearings (Ch. 27, Part 3 §308; Part 4, §405; Part 5, §§503, 506; Part 6, §605)**

Deleted: Zoning Permits and

A. Zoning Hearing Board Applications and Hearings:

Single Family	\$500.00 plus \$150.00 for each additional hearing beyond one
Multi-Family	\$1,200.00 plus \$150.00 for each additional hearing beyond one
Non-Residential	\$1,400.00 plus \$150.00 for each additional hearing beyond one

B. Rezoning Applications and Hearings \$3,500.00 plus \$500.00 for each additional hearing beyond one

C. Curative Amendment Application and Hearing \$3,500.00 plus \$500.00 for each additional hearing beyond one

16. **Core Commercial District Parking (Ch. 27, Part 21, §2107(4)(A)(2))**

Fee in lieu of Off-Street Parking	\$5,000.00 per space
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17. **False Alarm (Chapter 10, Part 3)**

First false alarm per rolling twelve months	warning issued
Second false alarm per rolling twelve months	warning issued
Third and Fourth false alarms per rolling twelve months	\$200.00 per false alarm
Fifth and Sixth false alarms per rolling twelve months	\$300.00 per false alarm
Seventh and subsequent false alarms per rolling twelve months	\$500.00 per false alarm

18. **Copying Fees**

Per Page Fee	\$.25
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19. **Document Fees**

Zoning Ordinance	\$40.00/copy
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	Subdivision Ordinance	\$40.00/copy
	Tax Certification	\$30 first year / \$5 each additional yr. \$5.00 per fulfillment of each request
20.	<b>Display of Fireworks</b>	
	Temporary Sale of Fireworks	\$75.00
	Permanent Sale of Fireworks (annual fee)	\$100.00 Plus, cost of Conditional Use Application
21.	<b>Emergency Call Outs</b>	
	Pick Up Truck	\$45.00 per hour
	Small Dump Truck	\$55.00 per hour
	10 Ton Dump Truck	\$65.00 per hour
	Bucket Truck	\$75.00 per hour
	Back Hoe	\$75.00 per hour
	Chipper	\$30.00 per hour
	Air Compressor	\$30.00 per hour
	Chain Saw	\$20.00 per hour
	Blower	\$20.00 per hour
	Pumps	\$20.00 per hour
	String Trimmer	\$20.00 per hour
		Plus, the cost of labor
22.	<b>Other Miscellaneous Fees</b>	
	Solicitation and Peddling Fee	\$100.00 for first 30 days \$100.00 for every additional 30 days
	Certified Mail Fee	Actual cost
	Attorneys' Fees for collection actions on delinquent accounts	Actual cost
	Beekeepers Registration Fee (annual)	\$25.00
	Open Burning Permit	\$25.00
23.	<b>Exemptions</b>	
	Permits must be obtained; however, permit issuance fees and general permit inspection fees are not payable, by any of the following:	
	A.	Commonwealth of Pennsylvania
	B.	Political Subdivisions of the Commonwealth
	C.	Governmental Authorities organized under the laws of the Commonwealth

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D. The Federal Government

24. **Legal Fees**

To the extent that any state statute or local ordinance allows the Borough to collect or be reimbursed for attorney's fees, they are charged at the Borough Solicitor's usual rate of \$140 per hour.

BE IT FURTHER RESOLVED, that this Consolidated Fee Schedule supercedes and replaces all prior fees and Fee Schedules adopted by the Council of the Borough of Hatfield.

NOW APPROVED and adopted by Borough Council at a duly advertised public meeting held this 15<sup>th</sup> day of May, 2019 with \_\_\_\_ Council Members Voting "Aye" and \_\_\_\_ Council Members voting "Nay".

Attest:

Borough of Hatfield

\_\_\_\_\_  
Michael DeFinis  
Borough Manager/Secretary

\_\_\_\_\_  
John H. Weierman  
Borough Council President

\_\_\_\_\_  
Robert L. Kaler III, Mayor

**6. NEW BUSINESS / DISCUSSION ITEMS:**

**D. Payment Request No. 1 Utility  
Replacement Project**



November 13, 2024

Jaime E. Snyder  
Borough Manager  
Hatfield Borough  
401 South Main Street  
Hatfield PA 19440

RE: **Broad St. and N. Main St. Utility Replacement Project**  
Contract No. HAT 24-01  
**Application for Payment 1**  
Bursich Project No: HAT-01/147492

Dear Jaime:

We have reviewed the Contractor's Application and Certificate for Payment No. 1, submitted by KBC Construction LLC dated November 11, 2024, for the above referenced contract. Application No. 1, in the amount of \$291,685.50, includes a request for payment for work associated with the water utility replacement in W. Broad St., as listed on the enclosed application, less 10% retainage.

Since the pay items are for work related to the water infrastructure, we have coordinated with the NPWA to evaluate work performed by the contractor. The NPWA has reviewed the application and is satisfied with the quantities submitted for payment.

Based on the work completed, **we recommend payment of \$291,685.50**, to KBC Construction LLC for work completed through November 2, 2024. Upon payment, the Remaining Contract Cost will be \$2,851,169.75 with \$2,883,579.25 Remaining to be Paid including retainage held.

Should you have any questions or need further information, please feel free to contact me at 484-941-0418 or ccamburn@vancleefengineering.com.

Very Truly Yours,  
Van Cleef Engineering Associates, LLC



Chad E. Camburn, P.E.  
Senior Technical Manager

Enclosures: Application and Certificate for Payment No. 1, dated 11/11/2024 (21 pages)  
Payment Summary, dated November 13, 2024 (4 pages)  
Certified Payrolls, 10/13/24 – 11/2/24 (10 pages)

Pc: Steve Fickert, Borough Public Works Director (w/ encl.; via email)  
Kate Harper, Borough Solicitor (w/ encl.; via email)  
Katie Vlahos, Assistant to the Manager (w/ encl.; via email)

F:\Projects\HAT-01\147492\_Broad St Storm Sewer Replacement\03\_ENG\01\_Contract Admin\Payments\Pay 1\Recommendation\2024-11-13\_Broad & Main Util Replace-Pay 1 Recommend Ltr to Hat.docx

**OFFICE LOCATIONS**

[www.vancleefengineering.com](http://www.vancleefengineering.com)

Hillsborough, NJ  
908-359-8291

Mt. Arlington, NJ  
862-284-1100

Phillipsburg, NJ  
908-454-3080

Doylestown, PA  
215-345-1876

Pottstown, PA  
610-323-4040

Hamilton, NJ  
609-689-1100

Toms River, NJ  
732-573-0490

Freehold, NJ  
732-303-8700

Bethlehem, PA  
610-332-1772

# APPLICATION AND CERTIFICATE FOR PAYMENT

AIA DOCUMENT G702

PAGE ONE OF 2

PAGES

**TO (OWNER):**

Hatfield Borough  
401 S Main St. Hatfield, PA 19440

**PROJECT:**

Broad &amp; Main Utility Replacement Project

**APPLICATION NO:**

1

**DISTRIBUTION TO:**☐ OWNER☐ ARCHITECT ENGINEER☐ CONTRACTOR

#

**ATTENTION:**

Chad Cambum  
Vancleef Engineering

**CONTRACTOR**

KBC CONSTRUCTION LLC  
1475 Hampton Lane  
Warminster, PA 18974

**ARCHITECT'S****PROJECT NO:**

HAT 24-01

**CONTRACT DATE:**

7/18/24

**CONTRACTOR'S APPLICATION FOR PAYMENT**

Application is made for Payment, as shown below, in connection with the Contract.  
Continuation Sheet, AIA Document G703, is attached.

The present status of the account for this Contract is as follows:

ORIGINAL CONTRACT SUM .....	\$	3,175,264.75
Net change by Change Order .....	\$	-
CONTRACT SUM TO DATE .....	\$	3,175,264.75
TOTAL COMPLETED & STORED TO DATE .....	\$	324,095.00
(Column G on G703)		
RETAINAGE <span style="border: 1px solid black; padding: 2px;">10%</span> .....	\$	32,409.50
or total in Column I on G703		
TOTAL EARNED LESS RETAINAGE .....	\$	291,685.50
LESS PREVIOUS CERTIFICATES FOR PAYMENT	\$	-
CURRENT PAYMENT DUE .....	\$	291,685.50

CHANGE ORDER SUMMARY		
Change Orders approved in previous months by Owner	ADDITIONS	DEDUCTIONS
TOTAL	\$ -	\$ -
Approved this Month		
Number	Date Approved	
TOTALS	\$ -	\$ -
Net change by Change Orders	\$ -	\$ -

The undersigned Contractor certifies that to the best of his knowledge, information and belief the Work covered by this Application for Payment has been completed in accordance with the Contract documents, that all amounts have been paid by him for Work for which previous Certificates for Payment were issued and payments received from the Owner, and that current payments shown herein are now due.

**CONTRACTOR:**

By:  11/11/24

**ARCHITECT'S CERTIFICATE FOR PAYMENT  
ENGINEER'S**

In accordance with the Contract Documents, based on on-the-site observations and the data comprising the above application, the Architect certifies to the Owner that the Work has progressed to the point indicated; that to the best of his knowledge, information and belief, the quality of the work is in accordance with the Contract Documents; and the Contractor is entitled to payment of the AMOUNT CERTIFIED.

**AMOUNT CERTIFIED .....**

(Attach explanation if amount certified differs from the amount applied for.)

**ARCHITECT ENGINEER**

By:



Date: 11/13/24

This Certificate is not negotiable. The AMOUNT CERTIFIED is payable only to the Contractor named herein. Issuance, Payment and acceptance of payment are without prejudice to any rights of the Owner or Contractor under this Contract.

State of: PennsylvaniaCounty of: Bucks

Subscribed and sworn to before me this

11<sup>th</sup>

day of

November 2024

Notary Public:

My Commission expires

Florence R. Spurlin  
June 29, 2026

Commonwealth of Pennsylvania - Notary Seal  
Florence R. Spurlin, Notary Public  
Bucks County  
My Commission Expires June 29, 2026  
Commission Number 1225646

**Jaime Snyder**

---

**From:** [REDACTED]  
**Sent:** Friday, November 8, 2024 10:45 AM  
**To:** Chad Camburn  
**Cc:** [REDACTED]  
**Subject:** RE: Pay App 1 Draft and Certified Payroll

Good morning Chad,

After reviewing the first pay application from KBC, NPWA agrees and is satisfied with the quantities submitted for payment. Please forward us a notarized copy of the pay application when received.

Thank you,

**Steve Fretz**  
Engineering & Operations Support  
North Penn Water Authority  
300 Forty Foot Road  
Lansdale, PA 19446

[REDACTED]

---

**From:** [REDACTED]  
**Sent:** Friday, November 8, 2024 7:11 AM  
**To:** [REDACTED]  
**Subject:** FW: Pay App 1 Draft and Certified Payroll

Hi Steve,

Not sure if Dan already passed this along but just in case.

Julie

---

**From:** [REDACTED]  
**Sent:** Wednesday, November 6, 2024 4:11 PM  
**To:** [REDACTED]

**Subject:** [PDF] FW: Pay App 1 Draft and Certified Payroll

Dan,

**6. NEW BUSINESS / DISCUSSION ITEMS:**

**E. SEPTA Sub-License Agreement  
Bard & Jester Brewery**

DRAFT

AGREEMENT BY AND AMONG  
SOUTHEASTERN PENNSYLVANIA TRANSPORTATION AUTHORITY (SEPTA),  
HATFIELD BOROUGH, AND BARD & JESTER BREWERY  
FOR THE LICENSING AND SUBLICENSING OF  
SEPTA'S HATFIELD RAIL STATION

SEPTA REGISTRY NO. \_\_\_\_\_  
CORPORATE CD# 4610

THIS AGREEMENT (herein "Agreement"), which includes the granting of a license and a sublicense, is made and entered into on this \_\_\_\_\_ day of November, 2024 ("Effective Date"), by and among the Southeastern Pennsylvania Transportation Authority (Licensor; herein "SEPTA"), a body corporate and politic which exercises the public powers of the Commonwealth of Pennsylvania as an agency and instrumentality thereof with its principal office located at 1234 Market Street, 10<sup>th</sup> Floor, Philadelphia, PA 19107-3780; and the Borough of Hatfield (Licensee; herein "Hatfield"), a Pennsylvania municipal government with its principal office located at 401 South Main Street, P.O. Box 190, Hatfield, Pennsylvania 19440; and Bard & Jester Brewery, (Sublicensee; herein "Bard & Jester") with a mailing address of \_\_\_\_\_ . Herein may be referred to each individually as a "Party" and collectively, the "Parties."

RECITALS

WHEREAS, SEPTA owns a certain out-of-service train station known as the Hatfield Station (herein the "Station"), which is situated upon the Bethlehem Branch by the intersection of E. Broad Street and N. Market St. in Hatfield, Pennsylvania;

WHEREAS, the Borough of Hatfield (herein "Hatfield") desires that SEPTA approves a ten-year license for the Station premises, given that Hatfield intends to sublicense the Station premises to Bard & Jester Brewery (herein "Bard & Jester") for its occupancy and use thereof for the operation of a retail establishment; and

WHEREAS, SEPTA has agreed to grant Hatfield: an exclusive license ("License") for various elements of the Station premises (specifically herein referred to collectively as the "Premises") as described, delineated and depicted herein; and the right to grant an exclusive sublicense ("Sublicense") to Bard & Jester' for its occupancy and use of the Premises; with both the License and Sublicense made subject to the terms and conditions set forth herein.

NOW, THEREFORE, in consideration of the mutual covenants and agreements herein contained, and other good and valuable consideration as described herein, the Parties, intending to be legally bound, hereby agree and commit as follows:

## AGREEMENT

1. Incorporation of Recitals. The recitals above are hereby incorporated into the body of the Agreement.
2. Premises. The Premises, which are hereby made subject to the License and the Sublicense, include the various elements of the Station, such as the 2500 square foot Station Building, the 1500 square foot pole barn, the area around the Station Building, the nonexclusive use of the parking lot, the driveway area., and other surrounding property; except that the Premises shall not include the track area, the platform area or any railroad operation facilities that remain in place. The Premises total approximately 1.7 acres and are shown and depicted on the drawing dated May 21<sup>st</sup>, 2018 that is presented in the attached and herein incorporated Exhibit A.
3. Granting of License and Sublicense. SEPTA does hereby grant to Hatfield, and Hatfield accepts from SEPTA, a License for the Premises for the Term (defined and specified in Section 4) and for the Licensed Use (defined and specified in Section 5) thereof, subject to the terms and conditions set forth herein.

Hatfield does hereby grant to Bard & Jester, with SEPTA's permission and concurrence, a Sublicense allowing for the Sublicensed Use (defined and specified in Section 5) of the Premises for the Term (see Section 4) subject to the terms and conditions set forth herein.

Bard & Jester does hereby accept the Sublicense granted by Hatfield and commits to occupy and use the Premises in accordance with the terms and conditions set forth in this Agreement.

4. Term and Termination. The term ("Term") of the Sublicense shall be ten (10) years, and shall commence on December 1, 2024 ("Commencement Date"), and shall terminate on December 1, 2034 ("Termination Date"), unless renewed as stated hereunder. Provided Tenant gives six months' notice of its intention to renew the sublicense before the Termination Date, and SEPTA has not invoked its right to terminate the License or Sublicense, the Term shall be extended for five years from the Termination date, with an option for one additional four year Term to expire on June 30, 2043.

Each Party has the right to terminate their respective License and/or Sublicense Agreement during the Term for any reason or no reason, by giving the other Parties thirty (30) days' prior written notice. If the either the License or Sublicense is terminated early, the other shall also terminate upon the same date, therefore any notice of early termination must be provided to all Parties simultaneously.

5. Use. Under and for the Term of the License, Hatfield is permitted to sublicense the Premises to Bard & Jester, to collect rental ("Sublicense Fee"; see Section 6 ) from Bard & Jester for its Sublicensed Use of the Premises;, and to have those other rights and obligations as are described herein (collectively "Licensed Use").



Under and for the Term of this Agreement, Bard & Jester is permitted to occupy and use the Premises for the operation of a licensed brewery that makes and sells beer and alcoholic and nonalcoholic to the public with a licensed brewery use including all ancillary uses related thereto (“Sublicensed Use”), subject to, as limited by and in accordance with the terms and conditions set forth herein.

Both the License and Sublicense include the nonexclusive right of access into, out of and across the existing driveway and parking lot for the purpose of ingress and egress between the Station Property and N. Market Street, for Bard & Jester and Hatfield, and their employees, agents and the public in general.

6. Rental. Except as otherwise agreed between Hatfield and SEPTA in the 2023 Lease between the Borough and SEPTA, for Hatfield’s Licensed Use of the Premises, Hatfield shall pay to SEPTA a License Fee” each month. The first monthly payment is due on the Commencement Date and all subsequent monthly payments of the License Fee shall be paid to SEPTA on or before the 1<sup>st</sup> day of each preceding month.

For Bard & Jester’ Sublicensed Use of the Premises, Bard & Jester shall pay to Hatfield a Sublicense Fee of \$ 2500 per month for the Sublicense for the first ten years of the Sublicense. Thereafter, if Bard & Jester renews the Agreement for the five year term and the subsequent four year term, the Sublicense Fee shall be \$3750 per month. The first monthly payment is due on the Commencement Date and all subsequent monthly payments of the Sublicense Fee shall be paid to Hatfield on or before the 1<sup>st</sup> day of each preceding month.

Hatfield Borough has agreed to grant Bard & Jester a credit against license fees in the amount of \$2500 per month for 36 months, provided Bard & Jester promptly improves the Premises for its use as a brewery and tavern and is open for business by

7. Cleaning and Maintenance. Bard & Jester shall have the primary responsibility for the cleaning, maintenance, upkeep (including lawn-cutting, shrub and tree maintenance, and removal of ice and snow) and repair (collectively “Maintenance”) of the Premises; which shall include generally the interior of the warehouse and Station Building, the exterior surface of the warehouse and Station Building, the parking lot and grounds, sidewalks, and fixtures both inside and outside of the Station Building. The Borough agrees it will be responsible for the sidewalks outside the Premises. However, in the event that any Maintenance is not properly performed by Bard & Jester, Hatfield will be ultimately responsible for the condition of the Premises with regard to Maintenance. The Premises must be maintained in a condition that is safe and proper for the Sublicensed Use.
8. Fixtures. Bard & Jester shall be responsible for the cost, installation, placement, care, maintenance, repair, safe use, protection and removal of all “Fixtures” that it places or installs in or on the Premises. Under the License and Sublicense, Fixtures shall include all equipment, furniture, appliances, décor, lighting, window treatments, or other

temporary items, materials, additions, etc., that can be easily put in place and can be removed. The result(s) of any lapse by Bard & Jester in exercising responsibility for any and all Fixtures, shall become the responsibility and liability of Hatfield. Under no condition shall SEPTA be responsible or liable for any Fixtures or any conditions that arise from the use thereof.

9. Signage. Both the License and Sublicense permit the installation of signage as necessary and/or useful for the Sublicensed Use of the Premises and in accordance with the Borough's zoning code. Signage shall not be installed in a manner that makes permanent changes to any part of the Premises. The costs associated with the signage installation shall be borne by the Party who desires it.
10. Improvements. In the event that Bard & Jester wants to make any changes, modifications, additions, renovations, improvements, etc. (collectively "Improvement" or "Improvements"), which are not merely Fixtures, to any aspect of the Premises for its Licensed Use thereof, Bard & Jester shall first obtain written approval from Hatfield, and then if approval is given, the Borough shall seek SEPTA's written approval prior to initiating any work on an Improvement. Hatfield's and SEPTA's approvals will not be unreasonably withheld, conditioned or delayed.
11. Taxes: Bard & Jester shall be primarily responsible for paying all taxes, including real estate taxes, that are owed for the Term period, regarding the Premises and Sublicensee's Use thereof. In the event that Bard & Jester does not pay any tax that is owed regarding the Premises for the Term period, Hatfield will be ultimately responsible for such payment.
12. Surrender of Premises. Bard & Jester shall have the primary responsibility for vacating and surrendering the Premises upon the Termination Date and for surrendering the Premises in safe and good order, ordinary wear and tear excepted, and clean condition. Bard & Jester shall also remove all removeable fixtures, personal property, and signage that it had installed on all parts of the Premises. All permanent improvements shall remain and will become the property of SEPTA.

In the event that Bard & Jester does not timely and properly surrender the Premises on or before the Termination Date, Hatfield will be ultimately responsible for removing Bard & Jester and for ensuring that the Premises upon Bard & Jester's departure are in safe and good order and clean condition, and that all personal property has been removed (unless SEPTA agrees to allow certain property to remain), at Hatfield's costs. In no way shall SEPTA be responsible for any actions or costs needed to vacate and put in proper condition the Premises upon or after the Termination Date.

13. Indemnification. In the event that a claim or action is brought against SEPTA or Hatfield, or both, as a result of Hatfield's License, Bard & Jester Sublicense, or the operation, use or occupancy of the Premises, Bard & Jester will release and hold harmless, and will indemnify and defend, SEPTA and Hatfield, their Board, officers, directors, employees, and agents, for any and all loss, liability, damage or expense, including reasonable attorney's fees, associated with bodily injury, sickness, property damage, or any other harm. Nothing contained in or implied by this Agreement shall

constitute or be construed to be a waiver by SEPTA of any immunity, exemption, protection or defense available to it under Pennsylvania's Sovereign Immunity Act of 1980, October 5, P.L. 639, No. 142, § 221(1) (42 Pa.C.S. §§ 8501 *et seq.*), or any other law or statute.

14. Notices. All notices, statements and/or communications required or referenced under this Agreement shall be in writing and sent by US Postal Service registered or certified mail, addressed to the Party or Parties as follows, which information may be changed by providing written notice to the other two Parties:

Hatfield:                    Hatfield Borough  
                                  Attn: Ms. Jaime Snyder  
                                  401 South Main Street, P.O. Box 190,  
                                  Hatfield, Pennsylvania 19440

Bard & Jester:            Bard & Jester Brewery  
                                  Attn: \_\_\_\_\_  
                                  \_\_\_\_\_, Pennsylvania 19\_\_\_\_

SEPTA:                    SEPTA  
                                  Attn: Director, Real Estate Department  
                                  1234 Market Street, 10<sup>th</sup> Floor  
                                  Philadelphia, PA 19107-3780

With copy to:            SEPTA  
                                  Attn: Deputy General Counsel – Corporate  
                                  1234 Market Street, 5<sup>th</sup> Floor  
                                  Philadelphia, PA 19107-3780

15. Assignment and Subletting. Hatfield is not permitted to sublicense the Premises to an entity other than Bard & Jester under the License granted by this Agreement.

Bard & Jester is not permitted to assign the Sublicense granted under this Agreement.

16. Future Interest in Premises. The Parties recognize and acknowledge that it is SEPTA's intention to seek and engage in good faith negotiations for the transaction of a long-term arrangement for rights to use, occupy and operate some or all of the Premises and potentially other property at and near the Station. Notwithstanding that, this Agreement shall be binding on SEPTA's successors and assigns.

17. Entirety of Agreement. The Agreement contains the complete understanding among the Parties and sets forth all representations and commitments between and among the Parties regarding the License and Sublicense of the Premises for the Term stated herein. Any and all previous understandings, arrangements, commitments, and promises regarding the

Premises, the License and/or the Sublicense, whether verbal or in writing, are superseded by this Agreement.

18. Modifications to Agreement. No modification or change of or to any term or provision herein shall be effective unless it is in writing and signed by the proper representative of each of the three Parties.
19. Governing Law, Jurisdiction and Venue. This Agreement is governed by and is to be interpreted and enforced in accordance with the laws of the Commonwealth of Pennsylvania. All matters, disputes, claims, litigation, or any other proceedings, in connection with this Agreement, shall be brought and resolved, in the state or federal courts located in the City of Philadelphia, Pennsylvania, except for enforcement of this Sublease with Brad & Jester Brewery which may be brought in the Montgomery County Courts, irrespective of any procedural rules or laws related to venue and forum non conveniens. The Parties expressly consent to such jurisdiction and venue, and waive any objection to such jurisdiction or venue and all claims of inconvenience or lack of personal jurisdiction. The Parties represent and acknowledge that their position on jurisdiction and venue described above is reasonable and has been freely and voluntarily made.
20. Partial Invalidity. If any term of the Agreement shall be held to be invalid or unenforceable, the remaining terms and provisions hereof shall not be effected thereby, and each such remaining term and provision of the Agreement shall be valid and duly considered in full force and effect.
21. Recording Prohibited. Recording, in any office of a recorder of deeds, of this this Agreement, or any other document purporting to represent it, is prohibited.
22. Counterparts. This Agreement may be executed in counterparts, each of which shall be deemed an original, and when put together the, counterparts, even if a copy, shall constitute one and the same valid instrument. A facsimile or email pdf file signature page shall be deemed an original.

[The remainder of page is intentionally blank.]

IN WITNESS WHEREOF, the Parties, intending to be legally bound and by signing below, do hereby make this Agreement effective as of the Effective Date specified on the first page.

HATFIELD BOROUGH - Licensee

Witness:

By: \_\_\_\_\_  
Name: \_\_\_\_\_  
Title: \_\_\_\_\_

By: \_\_\_\_\_  
Name: \_\_\_\_\_  
Title: \_\_\_\_\_

BARD & JESTER FIRPLACES - Sublicensee

Witness:

By: \_\_\_\_\_  
Name: \_\_\_\_\_  
Title: \_\_\_\_\_

By: \_\_\_\_\_  
Name: \_\_\_\_\_  
Title: \_\_\_\_\_

SOUTHEASTERN PENNSYLVANIA TRANSPORTATION  
AUTHORITY (SEPTA) - Licensor

Witness:

By: \_\_\_\_\_  
Jeffrey D. Knueppel  
General Manager

By: \_\_\_\_\_  
Name:: \_\_\_\_\_  
Title: \_\_\_\_\_

Approved as to form

\_\_\_\_\_  
Office of General Counsel, SEPTA

EXHIBIT A  
DIAGRAM OF PREMISES



SEPTA  
Real Estate Dept.  
D. Doler 03.19.2018

Hatfield Station Lease Premises



## **7. OLD BUSINESS:**

**A. Ordinance No. 556 Updating the  
International Property Maintenance  
Code from 2003 to 2021**

## NOTICE

The following Ordinance will be considered for adoption by Hatfield Borough Council on Wednesday, December 4, 2024 at 7:00 p.m. at the Hatfield Borough Hall, 401 S. Main Street. Hatfield, PA.

### **ORDINANCE NUMBER \_\_\_\_\_**

**AN ORDINANCE OF HATFIELD BOROUGH, MONTGOMERY COUNTY, PA, ADOPTING THE 2021 INTERNATIONAL PROPERTY MAINTENANCE CODE PUBLISHED BY THE INTERNATIONAL CODE COUNCIL, AS AMENDED FROM TIME TO TIME; PROVIDING FOR THE MAINTENANCE OF EXISTING RESIDENTIAL AND NONRESIDENTIAL STRUCTURES AND PREMISES WITHIN THE BOROUGH OF HATFIELD AS THE BOROUGH PROPERTY MAINTENANCE CODE; PROVIDING FOR THE ISSUANCE OF PERMITS, COLLECTION OF FEES, MAKING OF INSPECTIONS AND PRESCRIBING PENALTIES FOR THE VIOLATION THEREOF; PROVIDING FOR APPEALS; ADDING CERTAIN LOCAL REGULATIONS RELATING TO INSECT SCREENS AND HEATING FOR RESIDENTIAL BUILDINGS; REPEALING THOSE PROVISIONS OF CHAPTER FIVE OF THE HATFIELD BOROUGH CODE OF ORDINANCES THAT ARE INCONSISTENT WITH THE INTERNATIONAL PROPERTY MAINTENANCE CODE AND THIS ORDINANCE AND PROVIDING AN EFFECTIVE DATE**

A copy of the full text of the proposed ordinance is on file and available to the public during business hours at the Hatfield Borough Hall, 401 S. Main St., Hatfield, PA 19440, as well as the offices of The Reporter, 307 Derstine Avenue, Lansdale, PA 19446. The Ordinance adopts, by reference, the 2021 International Property Maintenance Code . A copy of the 2021 International Property Maintenance Code is available and may be examined during normal business hours at no charge. The public is invited and encouraged to attend the Public Meeting. Persons requiring special assistance or accommodations are requested to contact the Borough Manager, Jaime Snyder at the Borough Hall prior to the scheduled meeting date.

**Catherine M. Harper, Esq. Timoney Knox LLP, Hatfield Borough Solicitor**

DRAFT

**BOROUGH OF HATFIELD  
MONTGOMERY COUNTY, PENNSYLVANIA  
ORDINANCE NO. \_\_\_\_**

**AN ORDINANCE AMENDING HATFIELD BOROUGH AMENDING PART II  
INTERNATIONAL PROPERTY MAINTENANCE CODE OF CHAPTER 5 CODE  
ENFORCEMENT, TO ADOPT THE 2021 EDITION OF THE INTERNATIONAL  
PROPERTY MAINTENANCE CODE, REPEALING PRIOR INCONSISTENT  
ORDINANCES OR PARTS OF ORDINANCES; CONTAINING A SEVERABILITY  
CLAUSE AND A CLAUSE ADDRESSING FUTURE REVISIONS; AND PROVIDING  
AN EFFECTIVE DATE.**

**WHEREAS**, the Council of the Borough of Hatfield, Montgomery County, Pennsylvania, is duly empowered by the Borough Code, 8 Pa.C.S. § 101, *et seq.*, to enact certain regulations relating to the public health, safety and welfare of the citizens of the community of the Borough of Hatfield;

**WHEREAS**, pursuant to the Borough Code of the Commonwealth of Pennsylvania including but not limited to Sections 1202(4), 1202(5), 1202(6), 1202(10), 1202(11), and 1202(24), the Council of the Borough of Hatfield is authorized to regulate the conditions and maintenance of all property, buildings, and structures within the Borough of Hatfield;

**WHEREAS**, the Council of the Borough of Hatfield previously adopted provisions of the International Property Maintenance Code as published by the International Code Council as amended from time to time, pursuant to the Pennsylvania Construction Code Act, 35 P.S. 7210.101, *et seq.*; and

**WHEREAS**, the Council of the Borough of Hatfield wishes to amend § 5-201, *et seq.*, (International Property Maintenance Code) of the Borough of Hatfield Code of Ordinances to adopt the 2021 edition of the International Property Maintenance Code as the property maintenance code of the Borough of Hatfield.

**NOW, THEREFORE, BE IT ORDAINED**, by the authority of the Council of the Borough of Hatfield, Montgomery County, Pennsylvania, that the Borough of Hatfield Code of Ordinances, Chapter 5 (Code Enforcement), Part II (International Property Maintenance Code) is hereby amended in accordance with this Ordinance, as follows:

**Section 1.     2021 INTERNATIONAL PROPERTY MAINTENANCE CODE.**

The Code of Ordinances of the Borough of Hatfield, Chapter 5 *Code Enforcement*, Part II INTERNATIONAL PROPERTY MAINTENANCE CODE, is hereby amended as follows:

Sections 201 through 211 of the Code is hereby amended by adding the underlined text and deleting the bold bracketed text:

§ 5-201. Adoption of 2021 International Property Maintenance Code by Reference.

This Borough hereby enacts by reference, as the Property Maintenance Code of Hatfield Borough, the 2021 International Property Maintenance Code as published by the International Code Council as amended from time to time, as fully as though the Code were set forth herein at length.

§ 5-202. Administration and Enforcement Provisions.

Administration and enforcement of the Code within this Borough shall be undertaken as determined by Hatfield Borough Council in accordance with the regulations of the Pennsylvania Construction Code Act, 35 P.S. § 7210.101 *et seq.*, to the extent applicable and the Pennsylvania Borough Code, 8 Pa.C.S.A. § 101 *et seq.*

§ 5-203. Building Code Board of Appeals.

The Building Code Board of Appeals (hereinafter "Appeals Board"), previously established by separate resolution in conformity with the requirements of the Pennsylvania Construction Code Act, Act 45 of 1999, 35 P.S. § 7210.501(c) and 34 Pa. Code § 403.121, shall hear and rule on appeals, requests for variances and requests for extensions of time.

§ 5-204. Amendments Made in Property Maintenance Code.

1. The existing structures code hereby adopted is amended as follows:

- A. "Borough of Hatfield" shall be inserted wherever the words "[Name of Municipality]" appear in brackets therein; whenever the term "legal officer" or "legal representative" is used in this Code, it shall be held to mean the Borough Solicitor.
- B. Section 106.4 is hereby amended to read as follows:

§ 106.4. Penalty:

- (1) Any person, firm or corporation who shall violate any provision of this Part, upon conviction thereof in an action brought before a Magisterial

District Judge in the manner provided for the enforcement of summary offenses under the Pennsylvania Rules of Criminal Procedure, shall be sentenced to pay a fine of not more than \$1,000 plus costs and, in default of payment of said fine and costs, to a term of imprisonment not to exceed 90 days. Each day that a violation of this Part continues or each section of this Part which shall be found to have been violated shall constitute a separate offense.

(2) The application of the above penalty shall not be held to prevent the enforced removal of prohibited conditions nor permit them to continue.

C. Section 304.14 is hereby amended to read as follows:

§ 304.14. Insect Screens. During the period from the first day of spring to the first day of fall every door, window and other outside opening used or required for ventilation purposes serving any building containing habitable rooms, food preparation areas, food service areas, or any areas where products used in food for human consumption are processed, manufactured, packaged or stored, shall be supplied with approved tightly fitting screens of not less than 16 mesh per inch and every swing door shall have a self-closing device in good working condition.

Exception. Screen doors shall not be required for out-swinging doors or other types of openings which make screening impractical, provided other approved means, such as air curtains or insect repellent fans are employed.

D. Section 602.3, "Heat Supply," is hereby amended to read as follows:

§ 602.3. *Heating for Residential Buildings.*

(1) Every dwelling shall be provided with heating facilities capable of maintaining a room temperature of 65° F. (18° C.) at a level of three feet (914 mm) above the floor and a distance of three feet (914 mm) from the exterior walls in all habitable rooms, bathrooms and toilet rooms based on the outside design temperature required for the locality by the mechanical code listed in Appendix A.

(2) Every owner and operator of any building who rents, leases or lets one or more dwelling unit, rooming unit, dormitory or guest room on terms, either express or implied, to furnish heat to the occupants thereof shall supply sufficient heat during the period from the first day of fall to the first day of spring to maintain a room temperature of not less than 65° F. (18° C.) in all habitable rooms, bathrooms and toilet rooms during the hours between 6:30 a.m. and 10:30 p.m. of each day

and not less than 60° F. (16° C.) during other hours. The temperature shall be measured at a point three feet (914 mm) above the floor and three feet (914 mm) from the exterior walls. When the outdoor temperature is below the outdoor design temperature required for the locality by the mechanical code listed in Appendix A, the owner or operator shall not be required to maintain the minimum room temperatures, provided the heating system is operating at full capacity, with supply valves and dampers in a fully open position.

E. Section 602.4, "Occupiable Work Spaces," is hereby amended to read as follows:

*§ 602.4. Occupiable Work Spaces.*

(1) Every enclosed occupied work space shall be supplied with sufficient heat during the period from the first day of fall to the first day of spring to maintain a temperature of not less than 65° F. (18° C.) during all working hours. The temperature shall be measured at a point three feet (914 mm) above the floor and three feet (914 mm) from the exterior walls.

*(2) Exceptions.*

(a) Processing, storage and operation areas that require cooling or special temperature conditions.

(b) Areas in which persons are primarily engaged in vigorous physical activities.

§ 5-205. State Law and Regulations.

In all matters that are regulated by the laws of the Commonwealth of Pennsylvania or by regulations of departments or agencies of the commonwealth promulgated by authority of law, such laws or regulations, as the case may be, shall control where the requirements thereof are the same as or in excess of the provisions of this Part. The Code shall control in all cases where the state requirements are not as strict as those contained in this Part.

§ 5-206. Provisions to be Continuation of Existing Regulations.

The provisions of this Part so far as they are the same as those of ordinances and/or codes in force immediately prior to the enactment of this Part, are intended as a continuation of such ordinances and codes and not as new enactments. The provisions of this Part shall not affect any act done or liability incurred, nor shall



they affect any suit or prosecution pending or to be instituted to enforce any right or penalty or punish any offense under the authority of any of the repealed ordinances.

§ 5-207. Inspection Fees.

Whenever the Code Official, who shall be appointed from time to time by the Borough Council of the Borough of Hatfield, performs an inspection permitted or required by the Code, there shall be charged to the property owner a fee for said inspection initially set at the rate as established, from time to time, by resolution of Borough Council, of inspection, reinspection, investigation and report time. The minimum fee shall be in an amount as established from time to time by resolution of Borough Council. Borough Council may from time to time, by resolution, change such fee and the fee in force at the time of the inspection shall be applied.

**Section 2. Severability.** The provisions of this Ordinance are severable. If any part of this Ordinance is declared to be un-constitutional, illegal, or invalid, the validity of the remaining provisions shall be unaffected.

**Section 3. Repealer.** All Ordinances and parts of Ordinances heretofore adopted, to the extent that the same are inconsistent herewith, are hereby repealed, while all Ordinances not inconsistent herewith remain valid and in force.

**Section 4. Effective Date.** The effective date of this Ordinance shall be January 1, 2025.

**ORDAINED AND ENACTED** by the Borough Council of HATFIELD BOROUGH on this \_\_\_\_\_ day of \_\_\_\_\_, 2024 with \_\_\_\_\_ Council Members \_\_\_\_\_ voting “aye” and \_\_\_\_\_ voting “nay.”.

Attest:

**HATFIELD BOROUGH**

\_\_\_\_\_  
Jaime E. Snyder, Manager / Secretary

By: \_\_\_\_\_  
Jason Ferguson, Council President

Approved this \_\_\_\_\_ day of \_\_\_\_\_, 2024.

\_\_\_\_\_  
Mayor Mary Anne Girard

**7. OLD BUSINESS:**

**B. Sitework Escrow Release  
No. 1 Lennar (Bennetts Court)**

October 30, 2024

Jaime E. Snyder  
Borough Manager  
Hatfield Borough  
401 South Main Street  
Hatfield PA 19440

RE: **Bennetts Court Townhomes**  
**Sitework Escrow Release 1**  
Bursich Project No.: HAT-01/187965



Dear Jaime:

We have reviewed the construction status for the Bennetts Court Townhomes development based on the applicant's request for escrow release. As part of our review, we visited the site with the Borough's Public Works Director on October 29, 2024 to observe the site conditions.

Based on the construction status we recommend the following release:

**Sitework Escrow:                      \$ 814,128.77**

Upon release, the remaining escrow held will be \$274,309.20. A detailed list of the items and costs recommended for release is enclosed.

Should you have any questions or need further information, please feel free to contact me at 484-941-0418 or [ccamburn@vancleefengineering.com](mailto:ccamburn@vancleefengineering.com).

Very Truly Yours,  
Van Cleef Engineering Associates, LLC



Chad E. Camburn, P.E.  
Senior Technical Manager

Enclosure: COST ESTIMATE - BENNETTS COURT TOWNHOMES (SITE WORK) – Release No. 1,  
dated October 30, 2024

pc: Katie Vlahos, Assistant to the Borough Manager (w/ encl.; via email)  
Kate Harper, Borough Solicitor (w/ encl.; via email)  
Steve Fickert, Borough Public Works Director (w/ encl.; via email)  
Ario Rivera, Lennar Land Development Manager (w/ encl.; via email)

F:\Projects\HAT-01\187965 Bennett's Court (Prestige Property Partners)\Townhouse LD\Escrow\Releases\Release 1\2024-10-30 Bennetts Court-Sitework Escrow Release 1.docx

**OFFICE LOCATIONS**

[www.vancleefengineering.com](http://www.vancleefengineering.com)

Hillsborough, NJ  
908-359-8291

Mt. Arlington, NJ  
862-284-1100

Phillipsburg, NJ  
908-454-3080

Doylestown, PA  
215-345-1876

Pottstown, PA  
610-323-4040

Hamilton, NJ  
609-689-1100

Toms River, NJ  
732-573-0490

Freehold, NJ  
732-303-8700

Bethlehem, PA  
610-332-1772

## COST ESTIMATE - BENNETTS COURT TOWNHOMES (SITE WORK)

ADDRESS: MAPLE AVE., HATFIELD BOROUGH

BURSICH No.: HAT-01/187965

ORIGINAL ESTIMATE PREPARED: NOVEMBER 4, 2022

VAN CLEEF ENGINEERING, INC.

2129 EAST HIGH STREET

POTTSTOWN, PA 19464

RELEASE NO.: 1

DATE: 10/30/2024

ORIGINAL ESTIMATE					TOTAL PAST RELEASES		CURRENT RELEASE		TOTAL RELEASE TO DATE		REMAINING	
DESCRIPTION	QTY	UNITS	UNIT COST	TOTAL	QTY	TOTAL	QTY	TOTAL	QTY	TOTAL	QTY	TOTAL
A. EROSION & SEDIMENTATION CONTROLS												
1 ROCK CONSTRUCTION ENTRANCE	340	SY	\$16.60	\$5,644.00	0	\$0.00	340	\$5,644.00	340	\$5,644.00	0	\$0.00
2 CONSTRUCTION PROTECTION FENCING	1,541	LF	\$2.40	\$3,698.40	0	\$0.00	1,541	\$3,698.40	1,541	\$3,698.40	0	\$0.00
3 18" FILTER SOCK	1,234	LF	\$5.80	\$7,157.20	0	\$0.00	1,234	\$7,157.20	1,234	\$7,157.20	0	\$0.00
4 18" SILT FENCE	195	LF	\$2.30	\$448.50	0	\$0.00	195	\$448.50	195	\$448.50	0	\$0.00
5 SLOPE LINING - NAG S75 BASIN 1	480	SY	\$1.50	\$720.00	0	\$0.00	0	\$0.00	0	\$0.00	(480)	(\$720.00)
6 CONCRETE WASHOUT	2	EA	\$714.00	\$1,428.00	0	\$0.00	2	\$1,428.00	2	\$1,428.00	0	\$0.00
7 STRIP TOPSOIL - BASIN 1	360	CY	\$5.10	\$1,836.00	0	\$0.00	360	\$1,836.00	360	\$1,836.00	0	\$0.00
8 CUT-BASIN 1	1,500	CY	\$4.10	\$6,150.00	0	\$0.00	0	\$0.00	0	\$0.00	(1,500)	(\$6,150.00)
9 RETURN TOPSOIL - BASIN 1	120	CY	\$5.10	\$612.00	0	\$0.00	120	\$612.00	120	\$612.00	0	\$0.00
10 RAKE AND SEED BERM - BASIN 1	5,000	SF	\$0.15	\$750.00	0	\$0.00	5,000	\$750.00	5,000	\$750.00	0	\$0.00
11 INLET PROTECTION	15	EA	\$150.00	\$2,250.00	0	\$0.00	15	\$2,250.00	15	\$2,250.00	0	\$0.00
12 STAKEOUT BASIN	1	LS	\$750.00	\$750.00	0	\$0.00	0	\$0.00	0	\$0.00	(1)	(\$750.00)
13 E&S MAINTENANCE AND REMOVAL	1	LS	\$3,000.00	\$3,000.00	0	\$0.00	0	\$0.00	0	\$0.00	(1)	(\$3,000.00)
SUBTOTAL ITEM A.				\$34,444.10	\$0.00		\$23,824.10		\$23,824.10		(\$10,620.00)	
B. STORMWATER MANAGEMENT												
1 8-INCH ROOF DRAIN PIPES	460	LF	\$25.00	\$11,500.00	0	\$0.00	0	\$0.00	0	\$0.00	(460)	(\$11,500.00)
2 15-INCH HDPE PIPE	603	LF	\$51.00	\$30,753.00	0	\$0.00	603	\$30,753.00	603	\$30,753.00	0	\$0.00
3 18-INCH HDPE	202	LF	\$59.00	\$11,918.00	0	\$0.00	202	\$11,918.00	202	\$11,918.00	0	\$0.00
4 24-INCH HDPE	205	LF	\$77.00	\$15,785.00	0	\$0.00	205	\$15,785.00	205	\$15,785.00	0	\$0.00
5 BASIN OUTLET CONTROL STRUCTURES	3	EA	\$4,319.00	\$12,957.00	0	\$0.00	0	\$0.00	0	\$0.00	(3)	(\$12,957.00)
6 STORM MANHOLE	1	EA	\$3,000.00	\$3,000.00	0	\$0.00	1	\$3,000.00	1	\$3,000.00	0	\$0.00
7 STORM INLETS	14	EA	\$2,647.00	\$37,058.00	0	\$0.00	14	\$37,058.00	14	\$37,058.00	0	\$0.00
8 YARD DRAINS	4	EA	\$1,823.00	\$7,292.00	0	\$0.00	4	\$7,292.00	4	\$7,292.00	0	\$0.00
9 8-INCH CLEANOUT	5	EA	\$368.00	\$1,840.00	0	\$0.00	5	\$1,840.00	5	\$1,840.00	0	\$0.00
10 ENDWALL R4 STONE	22	TONS	\$39.00	\$858.00	0	\$0.00	0	\$0.00	0	\$0.00	(22)	(\$858.00)
11 TIE INTO EXISTING NEW INLET	1	LS	\$14,527.00	\$14,527.00	0	\$0.00	1	\$14,527.00	1	\$14,527.00	0	\$0.00
12 SNOUTS	4	EA	\$500.00	\$2,000.00	0	\$0.00	4	\$2,000.00	4	\$2,000.00	0	\$0.00
13 UNDERGROUND BASIN #2 COMPLETE	1	EA	\$39,974.00	\$39,974.00	0	\$0.00	0	\$0.00	0	\$0.00	(1)	(\$39,974.00)
14 RAIN GARDEN, COMPLETE	1	EA	\$5,000.00	\$5,000.00	0	\$0.00	0	\$0.00	0	\$0.00	(1)	(\$5,000.00)
15 ENDWALLS	2	EA	\$1,500.00	\$3,000.00	0	\$0.00	0	\$0.00	0	\$0.00	(2)	(\$3,000.00)
SUBTOTAL ITEM B.				\$197,462.00	\$0.00		\$124,173.00		\$124,173.00		(\$73,289.00)	
C. LANDSCAPING & SIGNAGE												
1 LARGE DECIDUOUS TREE	37	EA	\$300.00	\$11,100.00	0	\$0.00	25	\$7,500.00	25	\$7,500.00	(12)	(\$3,600.00)
2 SMALL SHADE TREE	24	EA	\$300.00	\$7,200.00	0	\$0.00	19	\$5,700.00	19	\$5,700.00	(5)	(\$1,500.00)
3 EVERGREEN TREES	29	EA	\$300.00	\$8,700.00	0	\$0.00	14	\$4,200.00	14	\$4,200.00	(15)	(\$4,500.00)
4 SHRUBS	227	EA	\$150.00	\$34,050.00	0	\$0.00	220	\$33,000.00	220	\$33,000.00	(7)	(\$1,050.00)
5 RESPREAD TOPSOIL	12,400	SF	\$0.05	\$620.00	0	\$0.00	12,400	\$620.00	12,400	\$620.00	0	\$0.00
6 PERMANENT SEED AND MULCH	104,000	SF	\$0.05	\$5,200.00	0	\$0.00	52,000	\$2,600.00	52,000	\$2,600.00	(52,000)	(\$2,600.00)
7 TRAFFIC SIGNS	15	EA	\$202.00	\$3,030.00	0	\$0.00	15	\$3,030.00	15	\$3,030.00	0	\$0.00
SUBTOTAL ITEM C.				\$69,900.00	\$0.00		\$56,650.00		\$56,650.00		(\$13,250.00)	
D. CLEARING AND DEMOLITION												
1 REMOVE INLET AND PIPE	25	LF	\$33.00	\$825.00	0	\$0.00	25	\$825.00	25	\$825.00	0	\$0.00
2 FULL STONE BACKFILL	10	TON	\$20.00	\$200.00	0	\$0.00	10	\$200.00	10	\$200.00	0	\$0.00
3 TEMPORARY PAVING	20	SY	\$87.00	\$1,740.00	0	\$0.00	20	\$1,740.00	20	\$1,740.00	0	\$0.00
4 REMOVE BUILDING AND HORROCKS SHED/GARAGE	1	LS	\$15,000.00	\$15,000.00	0	\$0.00	1	\$15,000.00	1	\$15,000.00	0	\$0.00
5 CLEARING	1	LS	\$31,200.00	\$31,200.00	0	\$0.00	1	\$31,200.00	1	\$31,200.00	0	\$0.00
SUBTOTAL ITEM D.				\$48,965.00	\$0.00		\$48,965.00		\$48,965.00		\$0.00	



**COST ESTIMATE - BENNETTS COURT TOWNHOMES (SITE WORK)**  
**ADDRESS: MAPLE AVE., HATFIELD BOROUGH**  
**BURSICH No.: HAT-01/187965**  
**ORIGINAL ESTIMATE PREPARED: NOVEMBER 4, 2022**

**VAN CLEEF ENGINEERING, INC.**  
**2129 EAST HIGH STREET**  
**POTTSTOWN, PA 19464**

**RELEASE NO.: 1**

**DATE: 10/30/2024**

ORIGINAL ESTIMATE					TOTAL PAST RELEASES		CURRENT RELEASE		TOTAL RELEASE TO DATE		REMAINING	
DESCRIPTION	QTY	UNITS	UNIT COST	TOTAL	QTY	TOTAL	QTY	TOTAL	QTY	TOTAL	QTY	TOTAL
E. EARTHWORK												
1 STRIP TOPSOIL (10" ASSUMED) AND STOCKPILE	4,240	CY	\$5.10	\$21,624.00	0	\$0.00	4,240	\$21,624.00	4,240	\$21,624.00	0	\$0.00
2 EARTHWORK-CUT	340	CY	\$5.10	\$1,734.00	0	\$0.00	340	\$1,734.00	340	\$1,734.00	0	\$0.00
3 EARTHWORK-FILL	3,740	CY	\$2.80	\$10,472.00	0	\$0.00	3,740	\$10,472.00	3,740	\$10,472.00	0	\$0.00
4 ROUGH GRADE	131,700	SF	\$0.05	\$6,585.00	0	\$0.00	131,700	\$6,585.00	131,700	\$6,585.00	0	\$0.00
SUBTOTAL ITEM E.				\$40,415.00	\$0.00		\$40,415.00		\$40,415.00		\$0.00	
F. SANITARY SEWER												
1 8-INCH SDR 26 PVC	548	LF	\$53.00	\$29,044.00	0	\$0.00	0	\$0.00	0	\$0.00	(548)	(\$29,044.00)
2 6-INCH SDR 26 PVC	364	LF	\$43.00	\$15,652.00	0	\$0.00	0	\$0.00	0	\$0.00	(364)	(\$15,652.00)
3 SANITARY MANHOLE	2	EA	\$3,500.00	\$7,000.00	0	\$0.00	0	\$0.00	0	\$0.00	(2)	(\$7,000.00)
4 CLEANOUTS	18	EA	\$500.00	\$9,000.00	0	\$0.00	0	\$0.00	0	\$0.00	(18)	(\$9,000.00)
5 LATERAL CONNECTIONS	18	EA	\$281.00	\$5,058.00	0	\$0.00	0	\$0.00	0	\$0.00	(18)	(\$5,058.00)
6 TIE INTO EXISTING WITH NEW MANHOLE	1	LS	\$29,877.00	\$29,877.00	0	\$0.00	0	\$0.00	0	\$0.00	(1)	(\$29,877.00)
SUBTOTAL ITEM F.				\$95,631.00	\$0.00		\$0.00		\$0.00		(\$95,631.00)	
G ASPHALT PAVING												
1 FINE GRADE PAVING	29,836	SF	\$0.09	\$2,685.24	0	\$0.00	29,836	\$2,685.24	29,836	\$2,685.24	0	\$0.00
2 5-INCH 2A STONE	3,315	SY	\$15.00	\$49,725.00	0	\$0.00	3,315	\$49,725.00	3,315	\$49,725.00	0	\$0.00
3 5-INCH 25 MM BASE COURSE	3,315	SY	\$40.00	\$132,600.00	0	\$0.00	3,315	\$132,600.00	3,315	\$132,600.00	0	\$0.00
4 TACK COAT	3,315	SY	\$1.00	\$3,315.00	0	\$0.00	3,315	\$3,315.00	3,315	\$3,315.00	0	\$0.00
5 1.5-INCH 9.5 MM WEARING COURSE	3,315	SY	\$10.00	\$33,150.00	0	\$0.00	2,800	\$28,000.00	2,800	\$28,000.00	(515)	(\$5,150.00)
6 SEALER CURB AND JOINT	1,600	LF	\$1.40	\$2,240.00	0	\$0.00	1,600	\$2,240.00	1,600	\$2,240.00	0	\$0.00
7 MILL AND OVERLAY MAPLE AVENUE	80	SY	\$56.00	\$4,480.00	0	\$0.00	80	\$4,480.00	80	\$4,480.00	0	\$0.00
SUBTOTAL ITEM G.				\$228,195.24	\$0.00		\$223,045.24		\$223,045.24		(\$5,150.00)	
H ADDITIONAL SITE IMPROVEMENTS												
1 FINE GRADE, PROVIDE, AND BACKFILL CONCRE	1,557	LF	\$21.50	\$33,475.50	0	\$0.00	1,557	\$33,475.50	1,557	\$33,475.50	0	\$0.00
2 MEADOWS 1600 CURE SEAL	1,557	LF	\$0.40	\$622.80	0	\$0.00	1,557	\$622.80	1,557	\$622.80	0	\$0.00
3 CONCRETE WHEEL STOPS	2	EA	\$78.00	\$156.00	0	\$0.00	0	\$0.00	0	\$0.00	(2)	(\$156.00)
4 PARKING LOT STRIPING	1	LS	\$540.00	\$540.00	0	\$0.00	1	\$540.00	1	\$540.00	0	\$0.00
5 CONSTRUCTION AHEAD SIGN	1	EA	\$1,400.00	\$1,400.00	0	\$0.00	1	\$1,400.00	1	\$1,400.00	0	\$0.00
6 DRIVEWAY APRONS	2,027	SF	\$25.00	\$50,675.00	0	\$0.00	2,027	\$50,675.00	2,027	\$50,675.00	0	\$0.00
7 CONCRETE SIDEWALK	1,473	SF	\$15.00	\$22,095.00	0	\$0.00	1,200	\$18,000.00	1,200	\$18,000.00	(273)	(\$4,095.00)
8 TRUNCATED DOMES	10	EA	\$250.00	\$2,500.00	0	\$0.00	0	\$0.00	0	\$0.00	(10)	(\$2,500.00)
9 CONCRETE MONUMENTS	6	EA	\$150.00	\$900.00	0	\$0.00	6	\$900.00	6	\$900.00	0	\$0.00
10 IRON PINS	60	EA	\$100.00	\$6,000.00	0	\$0.00	60	\$6,000.00	60	\$6,000.00	0	\$0.00
11 SPLIT RAIL FENCE	590	LF	\$35.00	\$20,650.00	0	\$0.00	550	\$19,250.00	550	\$19,250.00	(40)	(\$1,400.00)
12 VINYL 6-INCH FENCE	443	LF	\$35.00	\$15,505.00	0	\$0.00	443	\$15,505.00	443	\$15,505.00	0	\$0.00
13 SINGLE FIXTURE POLE MOUNTED LIGHTS	6	EA	\$2,500.00	\$15,000.00	0	\$0.00	6	\$15,000.00	6	\$15,000.00	0	\$0.00
14 HANDICAP RAMPS AT MAPLE AVE.	3	EA	\$7,500.00	\$22,500.00	0	\$0.00	0	\$0.00	0	\$0.00	(3)	(\$22,500.00)
SUBTOTAL ITEM H.				\$192,019.30	\$0.00		\$161,368.30		\$161,368.30		(\$30,651.00)	
SUBTOTAL ITEMS A. THRU H.				\$907,031.64	\$0.00		\$678,440.64		\$678,440.64		(\$228,591.00)	
10% CONTINGENCY				\$90,703.16	\$0.00		\$67,844.06		\$67,844.06		(\$22,859.10)	
TOTAL CONSTRUCTION ESCROW				\$997,734.80	\$0.00		\$746,284.70		\$746,284.70		(\$251,450.10)	
ENGINEERING and LEGAL FEES (10%)				\$90,703.16	\$0.00		\$67,844.06		\$67,844.06		(\$22,859.10)	
TOTAL COST ESTIMATE				\$1,088,437.97	\$0.00		\$814,128.77		\$814,128.77		(\$274,309.20)	

2024-10-30 Bennetts Court Sitework Esc-Rel 1.xls



**COST ESTIMATE - BENNETTS COURT TOWNHOMES (SITE WORK)**  
**ADDRESS: MAPLE AVE., HATFIELD BOROUGH**  
**BURSICH No.: HAT-01/187965**  
**ORIGINAL ESTIMATE PREPARED: NOVEMBER 4, 2022**

**VAN CLEEF ENGINEERING, INC.**  
**2129 EAST HIGH STREET**  
**POTTSTOWN, PA 19464**

**RELEASE NO.: 1**

**DATE: 10/30/2024**

DESCRIPTION	QTY	ORIGINAL ESTIMATE			TOTAL	TOTAL PAST RELEASES		CURRENT RELEASE		TOTAL RELEASE TO DATE		REMAINING	
		UNITS	UNIT COST			QTY	TOTAL	QTY	TOTAL	QTY	TOTAL	QTY	TOTAL

**ESCROW RELEASES:**

NO.	VALUE	DATE
1	\$814,128.77	10/30/2024
<b>Total</b>	<b>\$814,128.77</b>	

SUBMITTED:

\_\_\_\_\_  
 APPLICANT

\_\_\_\_\_  
 DATE

RECOMMENDED FOR RELEASE:

  
 VAN CLEEF ENGINEERING, INC.

10/30/2024  
 DATE

APPROVED:

\_\_\_\_\_  
 HATFIELD BOROUGH

\_\_\_\_\_  
 DATE

ENGINEERING OBSERVATION REQUIRED AFTER E&S CONTROLS ARE INSTALLED AND PRIOR TO ANY GRUBBING OR EARTHMOVING, FOR CONSTRUCTION OF ALL ESCROWED IMPROVEMENTS, AND PRIOR TO REMOVAL OF E&S CONTROLS. A MINIMUM OF 48 HOURS NOTICE SHALL BE PROVIDED IN ORDER TO SCHEDULE FIELD STAFF.

ENGINEERING AND OBSERVATION CHARGES SHALL BE BILLED ON AN HOURLY BASIS FOR ACTUAL TIME REQUIRED. HOURLY RATES WILL BE BILLED AT 150% OF STANDARD RATES FOR WORK PERFORMED BEFORE 7:30 AM OR AFTER 5:30 PM ON WEEKDAYS, AND ANY TIME DURING HOLIDAYS AND WEEKENDS. ENGINEERING SERVICES INCLUDE, BUT ARE NOT LIMITED TO, COMMUNICATIONS, OBSERVATIONS OF INSTALLED IMPROVEMENTS, TRAVEL, PROCESSING OF REQUESTS FOR ECROW RELEASE, FINAL PUNCHLIST AND CLOSEOUT OF THE PROJECT.

**7. OLD BUSINESS:**

**C. 2025 Proposed Meeting Dates**

PUBLIC NOTICE

**The Borough of Hatfield Council will hold its meetings for the year 2025 on the following dates:  
WORKSHOP / REGULAR MEETING. Meetings begin at 7:00 PM**

DRAFT

Borough Council Dates:

January ?, 2025  
January 15, 2025  
February 5, 2025  
February 19, 2025  
March 5, 2025  
March 19, 2025  
April 2, 2025  
April 16, 2025  
May 7, 2025  
May 14, 2025 Town Hall Meeting  
May 21, 2025  
June 18, 2025  
July 16, 2025  
August 20, 2025  
September 10, 2025 at 6:00 PM Strategic Planning Meeting  
September 17, 2025  
October 1, 2025  
October 15, 2025  
November 5, 2025  
November 12, 2025 Budget Presentation Meeting  
November 19, 2025  
December 3, 2025  
December 17, 2025

**The Borough of Hatfield Planning Commission will hold its meetings for the year 2025 on the following dates. Meetings begin at 6:00 PM**

Planning Commission Dates:

January 27, 2025  
February 24, 2025  
March 24, 2025  
April 28, 2025  
May 19, 2025  
June 23, 2025  
July 28, 2025  
August 25, 2025  
September 22, 2025  
October 27, 2025  
November 17, 2025  
December 15, 2025

**The Borough of Hatfield HEROC Committee will hold its meetings for the year 2025 on the following dates. Meetings begin at 8:00 AM**

HEROC Meeting Dates:

January 22, 2025  
February 26, 2025  
March 26, 2025

April 23, 2025  
May 28, 2025  
June 25, 2025  
July 23, 2025  
August 27, 2025  
September 24, 2025  
October 22, 2025  
November 19, 2025  
December 17, 2025

The Borough of Hatfield Zoning Hearing Board convenes on a case-by-case basis. Sufficient public notice will be provided when applications for a hearing are submitted. **All meetings will be held at the Hatfield Borough Municipal Complex 401 South Main Street Hatfield, PA 19440.** The public is invited and encouraged to attend. The Municipal Complex is wheel chair accessible. Any person that requires a special accommodation should contact the Borough offices at 215-855-0781 at least three days in advance of the meeting.

Jaime E. Snyder  
Borough Manager/Secretary

# 2025

## January

Su	Mo	Tu	We	Th	Fr	Sa
			1	2	3	4
5	6	7	8	9	10	11
12	13	14	<b>15</b>	16	17	18
19	20	21	<b>22</b>	23	24	25
26	<b>27</b>	28	29	30	31	

## February

Su	Mo	Tu	We	Th	Fr	Sa
						1
2	3	4	<b>5</b>	6	7	8
9	10	11	12	13	14	15
16	17	18	<b>19</b>	20	21	22
23	<b>24</b>	25	<b>26</b>	27	28	

## March

Su	Mo	Tu	We	Th	Fr	Sa
						1
2	3	4	<b>5</b>	6	7	8
9	10	11	12	13	14	15
16	17	18	<b>19</b>	20	21	22
23	<b>24</b>	25	<b>26</b>	27	28	29
30	31					

## April

Su	Mo	Tu	We	Th	Fr	Sa
		1	<b>2</b>	3	4	5
6	7	8	9	10	11	12
13	14	15	<b>16</b>	17	18	19
20	21	22	<b>23</b>	24	25	26
27	<b>28</b>	29	30			

## May

Su	Mo	Tu	We	Th	Fr	Sa
				1	2	3
4	5	6	<b>7</b>	8	9	10
11	12	13	<b>14</b>	15	16	17
18	<b>19</b>	20	<b>21</b>	22	23	24
25	26	27	<b>28</b>	29	30	31

## June

Su	Mo	Tu	We	Th	Fr	Sa
1	2	3	4	5	6	7
8	9	10	11	12	13	14
15	16	17	<b>18</b>	19	20	21
22	<b>23</b>	24	<b>25</b>	26	27	28
29	30					

## July

Su	Mo	Tu	We	Th	Fr	Sa
		1	2	3	4	5
6	7	8	9	10	11	12
13	14	15	<b>16</b>	17	18	19
20	21	22	<b>23</b>	24	25	26
27	<b>28</b>	29	30	31		

## August

Su	Mo	Tu	We	Th	Fr	Sa
					1	2
3	4	5	6	7	8	9
10	11	12	13	14	15	16
17	18	19	<b>20</b>	21	22	23
24	<b>25</b>	26	<b>27</b>	28	29	30
31						

## September

Su	Mo	Tu	We	Th	Fr	Sa
	1	2	3	4	5	6
7	8	9	<b>10</b>	11	12	13
14	15	16	<b>17</b>	18	19	20
21	<b>22</b>	23	<b>24</b>	25	26	27
28	29	30				

## October

Su	Mo	Tu	We	Th	Fr	Sa
			<b>1</b>	2	3	4
5	6	7	8	9	10	11
12	13	14	<b>15</b>	16	17	18
19	20	21	<b>22</b>	23	24	25
26	<b>27</b>	28	29	30	31	

## November

Su	Mo	Tu	We	Th	Fr	Sa
						1
2	3	4	<b>5</b>	6	7	8
9	10	11	12	13	14	15
16	<b>17</b>	18	<b>19</b>	20	21	22
23	24	25	26	27	28	29
30						

## December

Su	Mo	Tu	We	Th	Fr	Sa
	1	2	<b>3</b>	4	5	6
7	8	9	10	11	12	13
14	<b>15</b>	16	<b>17</b>	18	19	20
21	22	23	24	25	26	27
28	29	30	31			

## **8. ACTION ITEMS:**

**A. Motion to Consider Sitework Escrow Release No. 1 Lennar (Bennetts Court) in the Amount of \$814,128.77 (eight hundred fourteen thousand one hundred twenty-eight dollars and seventy-seven cents)**



## **8. ACTION ITEMS:**

**B. Motion to Consider Advertising  
Ordinance No. 557 Enacting a Rental  
Inspection Program in Hatfield Borough  
for a Public Hearing to be held on  
December 4, 2024 at 7:00PM  
in Council Chambers**

## **8. ACTION ITEMS:**

**C. Motion to Consider Payment Request No. 1 for the Utility Replacement Project to KBC Construction Inc. in the Amount of \$291,685.50 (two hundred ninety-one thousand six hundred eighty-five dollars and fifty cents)**

## **9. Motion to Approve Payment of the Bills**

Column1	Column2	Column3	Column4	Column5	Column6
NOVEMBER 2024 ACCOUNTS PAYABLE BILL LIST					
VENDOR BILL LIST					
	ITEM DESCRIPTION	AMOUNT PAID	DATE PROCESSED	TOTAL PAID	CHECK NO.
TD BANK					
AT&T	PW & MGR CELL PHONES	\$531.54	11/14/2024	\$531.54	28542
COMCAST CABLE	401 S MAIN ST INTERNET SERVICE	\$116.85	11/14/2024	\$116.85	28543
HATFIELD BOROUGH ELECT	ELECTRIC FOR 615 DAIN AVE	\$62.28	11/14/2024	\$62.28	28544
LOWE'S	VARIOUS ITEMS	\$413.84	11/14/2024	\$413.84	28545
NORTH PENN WATER AUTHORITY	615 DAIN AVE WATER SERVICES	\$36.23	11/14/2024	\$36.23	28546
PITNEY BOWES	POSTAGE FOR POSTAGE MACHINE	\$3,000.00	11/14/2024	\$3,000.00	28547
VERIZON	INTERNET SERVICE FOR 401 S MAIN ST	\$245.89	11/14/2024	\$245.89	28548
WELLS FARGO	SERIES 2020 AND 2021 A AND B NOTES	\$2,619.82	10/25/2024	\$2,619.82	ACH
21ST CENTURY MEDIA	LEGAL ADVERTISING	\$932.71			
ALLEGHENY ELECTRIC COOP	AUGUST MONTHLY ELECTRIC SALES	\$2,926.00			
ALWAYS INTEGRITY	JANITORIAL SERVICES	\$620.00			
AMP INC.	OCTOBER PMPM/VERIZON CHARGES	\$1,504.16			
AMP OHIO	OCTOBER ELECTRIC PURCHASE	\$145,038.65			
ARMOUR & SONS	REPLACE PEDESTRIAN POLE	\$1,557.65			
ARMOUR & SONS	REPAIR TRAFFIC SIGNAL MAIN/BROAD	\$400.28			
ASSOCIATION OF MAYORS OF BOROS	MEMEBERSHIP DUES	\$60.00			
AT&T	PW & MGR CELL PHONES	\$531.30			
DL BEARDSLEY	BELT FOR SAW	\$71.00			
BEE BERGVALL & CO	CONSULTING SERVICES	\$245.00			
BERGEYS	RELAY FOR SMALL DUMP	\$41.89			
BOWMAN	ENGINEERING - ARBOR GROVE 23 N MAIN	\$990.00			
BOWMAN	ENGINEERING - BROAD & MAIN UTILITY	\$82.50			
BOROUGH OF HATFIELD ELECTRIC	REPAYMENT OF BORROWING	\$8,287.43			
CANON COPIER	COPIER LEASE	\$685.25			
CARR & DUFF	41 E BROAD CONDUIT	\$4,148.44			
CLARKE'S LANDSCAPING	FALL CURBSIDE CHIPPING	\$1,275.00			
CLEMENS UNIFORMS	MATS FOR HALLWAYS	\$77.30			
COMMONWEALTH OF PA	PESTICIDE LICENSE RENEWAL	\$35.00			
CODE INSPECTIONS	BLDG CODE FIRE & ZONING SERVICES	\$6,346.00			
COMCAST	16 CHERRY ST INTERNET	\$121.23			
DELAWARE VALLEY HEALTH INS	HEALTH INSURANCE FOR EMPLOYEES	\$16,194.79			
DIDDENS GREENHOUSE	FLOWERS FOR CHRISTMAS	\$423.00			
DISCHELL BARTLE DOOLEY	LEGAL SERVICES - VINNYS PIZZA	\$1,615.00			
DISCHELL BARTLE DOOLEY	LEGAL SERVICES - VINNYS PIZZA	\$204.00			
EAS WATER	WATER FOR OFFICES	\$123.78			
EDDIES ELECTRIC	ST LIGHT REPAIRS	\$238.00			
EDDIES ELECTRIC	ELECTRIC SERVICE FOR 33 W BROAD	\$238.00			
EJ USA, INC.	INLET & MANHOLE FRAMES	\$5,289.76			
EJ USA, INC.	MANHOLE RISERS	\$423.86			
ESTABLISHED TRAFFIC CONTROL	STREET SIGNS	\$318.00			
GILL QUARRIES, INC.	DUMP FEE	\$150.00			
GLASGOW, INC.	BLACKTOP	\$666.60			
GLASGOW, INC.	BLACKTOP	\$132.00			
GUARDIAN	COUNCIL LIFE INSURANCE	\$33.95			
THE HARTFORD	AD&D LIFE STD & LTD INSURANCE	\$799.11			
HATFIELD TOWNSHIP	OCTOBER POLICE SERVICES	\$82,917.00			
H&K MATERIALS	BLACKTOP	\$390.25			
H&K MATERIALS	BLACKTOP	\$282.45			
H&K MATERIALS	BLACKTOP	\$264.82			
HAJOCA LANSDALE	SEWER FITTINGS	\$426.55			
HTMA	WASTEWATER DISPOSAL TREATMENT	\$172,195.84			
JEFF'S WINDOW CLEANING	WINDOW CLEANING	\$658.00			
KBC CONSTRUCTION	PAYMENT #1 BROAD/MAIN ST	\$291,685.50			
MAILLIE	2023 AUDIT	\$4,000.00			
NETWORK CONCEPTS	MANAGED IT SERVICES	\$515.00			
PA ONE CALL	MONTHLY ACTIVITY	\$33.51			
PA STATE ASSOCIATION OF BOROS	MEMEBERSHIP DUES	\$532.00			
PA STATE ASSOCIATION OF BOROS	SUBSCRIPTION	\$170.00			
PA STATE MAYORS ASSOCIATION	MEMEBERSHIP DUES	\$70.00			
PITNEY BOWES	POSTAGE MACHINE LEASE	\$438.00			





**10. MOTION to ADJOURN:  
EXECUTIVE SESSION**